

AGENDA
CITY OF DAYTON, MINNESOTA
12260 S. Diamond Lake Road, Dayton, MN 55327
Tuesday, May 26, 2026

Worksession with Parks Commission - 5:30 P.M.
REGULAR MEETING OF THE CITY COUNCIL - 6:30 P.M.

The invite for Zoom for this meeting can be found on the City's website community calendar

- 6:30 **CALL TO ORDER**
- 6:30 **PLEDGE OF ALLEGIANCE**
- 6:35 **APPROVAL OF AGENDA**
- 6:40 **CONSENT AGENDA** *These routine or previously discussed items are enacted with one motion. Any questions on items should have those items removed from consent agenda and approved separately.*
- A. Approval of Council Meeting Minutes of May 12, 2026
- B. Approval of Council Worksession Meeting Minutes of May 12, 2026
- C. Approval of Temporary Liquor License Dates for Lions Club of Dayton; Dates 06/05/2026, 06/26/2026, 07/10/2026, 07/24/2026, 08/07/2026, 08/21/2026
- D. Approval of Resolution 23-2026; Municipal State Aid Advance Request for Dayton Parkway Traffic Signals Funding
- E. Approval of Resolution 21-2026: Waiving of City Code 1002.08, Subd 6 (Proof of Recording), French Lake Industrial Center Six
- F. Approval of Seasonal Public Works Employees
- G. Approval of Change Order 12 for the Well Head Treatment
- H. Administrators Memo on Spending
- I. MOU
- J. Approval of Election Timeframe Change for Early Voting
- 6:45 **OPEN FORUM** *Is limited to Three minutes for non-agenda items; state your name and address; No Council Action will be taken and items will be referred back to staff*
- 6:50 **STAFF, CONSULTANT AND COUNCIL UPDATES**
- COUNCIL BUSINESS**
- New Business**
- K. Accept (Draft) NW Water Tower Preliminary Design Report
- Action Items**
- L. Approval of Payment of Claims for May 26, 2026
- M. 2026 Resident Survey Results
- N. Approval of Resolution 22-2026, Variance to increase the maximum front yard buildings setback; and Site Plan Review for Dayton Assisted Living
- O. Approval of Ordinance 2026-09; Amending City Code 1001.35 Regarding Accessory Buildings
- P. Curbside EAW Response
- Q. Letter of Support for Regional Solicitation Grant
- Closed session**
- R. Closed Session. Pursuant to MN Statute 13D.05, Subd 3(c)(3), a closed session shall be conducted to develop or consider offers or counteroffers for the purchase or sale of real or personal property PID: 31-120-22-33-0009
- ADJOURNMENT**

The City of Dayton's mission is to promote a thriving community and to provide residents with a safe and pleasant place to live while preserving our rural character, creating connections to our natural resources, and providing customer service that is efficient, fiscally responsible, and responsive.

CALL TO ORDER

Fisher called the regular meeting of the Dayton City Council to order at 6:30 PM on Tuesday, May 12, 2026.

PRESENT: Dennis Fisher, David Fashant, Stephanie Henderson, Scott Salonek, and Sara Van Asten

ABSENT:

ALSO PRESENT: City Administrator/Finance Director, Zach Doud; Community Development Director, Jon Sevald; Public Works Superintendent, Marty Farrell; Fire Chief, Gary Hendrickson; Police Chief, Paul Enga; Assistant City Administrator/City Clerk, Amy Benting; City Engineer, Jason Quisberg

PLEDGE OF ALLEGIANCE

APPROVAL OF AGENDA

Two modifications to the agenda were noted: the Closed Session Recap was moved to immediately follow approval of the agenda, and an Open Forum Response was added after the Consent Agenda.

Motion to approve the agenda as modified was made by Henderson and seconded by Fashant. The motion carried 5-0.

CLOSED SESSION RECAP

Per Minnesota Statute Section 13.05, Subdivision 3(a), a recap of the closed session held on April 28, 2026 was read into the record. The council conducted a performance evaluation of City Administrator Zach Doud, concluding that he had performed well and directing him to identify professional goals for the coming year. The council approved a 5% salary increase, bringing his total annual compensation to \$192,000.18.

Motion to approve the closed session recap was made by Van Asten and seconded by Fashant. The motion carried 4-1 with nay Henderson.

CONSENT AGENDA

- A.** Approval of Council Meeting Minutes of April 28, 2026
- B.** Approval of Council Worksession Meeting Minutes of April 28, 2026
- C.** Approval of Resolution 20-2026; community grant
- D.** Approval of Temp Liquor St. Johns the Baptist
- E.** Approval of Gambling St. Johns the Baptist
- F.** Approve Pay Request 3 for the 113th Avenue Trunk Sewer Extension Project
- G.** Approve Change Order 2 for the Dayton HMPG Generators Project
- H.** Approve Pay Request 7 (Final) for the Dayton HMPG Generators Project

Van Asten noted typographical corrections to Item G that had already been communicated to staff, with no changes to content. Fashant asked about Item G regarding the CenterPoint Energy gas meter relocation related to the Dayton HMPG Generators Project. Quisberg explained that while the plans were approved by CenterPoint's office staff ahead of time, field staff required the gas connection to be relocated to the exterior of the building rather than run internally, resulting in the change order — a recurring issue experienced with utility companies.

Motion to approve the consent agenda was made by Van Asten and seconded by Salonek. The motion carried 5-0.

OPEN FORUM RESPONSE

Fisher read the council's collective majority responses to questions raised at the April 28, 2026 meeting, covering the following topics:

E-Bikes: State statute governs e-bike use; however, the city has the ability to regulate where bikes may be ridden. Staff was directed to provide further research to the council.

EDA Chair Conflict of Interest: The city attorney advised that Minnesota law does not prohibit a person from simultaneously serving as Dayton EDA Commissioner, Champlin Council Member, and Champlin EDA Commissioner. A conflict of interest, if it exists, would disqualify the individual from participating on specific matters rather than from serving in the role. Council Member Salonek's prior comments were referenced to a discussion held December 9, 2025, available on the city's website and YouTube channel.

EDA Levy: The EDA levy request is a recommendation to the city council. Each year it varies based on the direction of the EDA but the official levy authority is the City Council so this was only a recommendation, not an official levy.

Flock Camera System: A series of questions regarding the Flock license plate reader system were addressed, including camera approval (through the budget process, with council reducing cameras from 10 to 6; PD secured 7 cameras at the price of 6), location criteria (entrances to the city only), public input in placement (not applicable; a staff decision), number anticipated (2 additional needed for full coverage), ongoing costs (budgeted annually as a contracted price), HOA access (city does not currently have access to any HOA Flock systems), data access and retention policy (governed by MN Statute 13.09 and 626.8472, publicly available), and whether policies require updating (no update needed).

OPEN FORUM

No members of the public or online attendees requested to speak. Open forum was closed.

STAFF, CONSULTANT AND COUNCIL UPDATES

Doud reminded attendees of the Public Works open house the following evening beginning at 5:00 PM, noting city hall offices would close at 3:00 PM in preparation. Farrell reported that the roadside cleanup pilot went well, with 15 segments completed, and the program is planned to be repeated twice yearly. The cleanup day resulted in 6 full roll-off boxes of trash and demo, one of scrap metal, and a full appliance truck. The Mother's Day bouquet-making event drew approximately 30 attendees. Three seasonal employees have been hired for the summer. On water quality, Farrell reported significant improvements since the wellhead treatment plant. Hendrickson reported that part-time duty crew training began May 4, with the crew set to begin operations June 1 from 10:00 AM to 10:00 PM. Hendrickson also noted the department would be taking possession of the new ladder truck on Thursday and bringing it to a graphics shop before delivery to the station. Hendrickson provided a heads-up that approximately \$7,500 in costs for diesel exhaust capture system relocations and approximately \$5,800 in graphics costs would be forthcoming. Sevald thanked council members who attended the Graco headquarters groundbreaking. Sevald noted that Met Council will hold a special District 1 meeting on Wednesday, June 17 at the Maple Grove Community Center and offered to send council an invitation. Sevald raised the topic of the upcoming 2050 Comprehensive

Plan, expressing a preference for writing it for Dayton residents rather than primarily for Met Council, and invited council members to submit questions they have about Dayton to help shape the plan's direction.

Quisberg reported that the traffic signal at Dayton Parkway by the Kwik Trip intersection is now operational.

Salonek thanked the 31 residents who participated in the road cleanup. Salonek also raised the idea of the city purchasing a 20x30 ft. tent (estimated at \$3,500–\$4,000) for use at events at Elsie Stephens Park, given 8 events are scheduled there this year. Discussion followed regarding logistics — storage, setup, takedown by public works staff, and potential cost recovery — noting that renting a similar tent currently costs approximately \$650. Staff noted prior council discussion had concluded that renting made more sense to avoid becoming a rental facility operator. Council directed staff to gather more information on the tent option before making a decision.

Van Asten inquired about the status of the curb appeal/housing rehabilitation grant program, funded by approximately \$73,000 in local affordable housing dollars with roughly three years remaining to spend it. Sevald confirmed a program framework still needs to be developed. Van Asten also raised the concept of proactively amending city ordinances to establish stronger standards — such as setbacks, buffering, hours of operation limitations, and lighting — for industrial, commercial, or other non-residential uses when they abut residential districts, citing concerns about the inability to bind future property owners to commitments made by current developers. Council members expressed support, and Sevald indicated the planning commission also has interest.

Fashant recognized the police department for conducting a well-received hunter safety program, noting the course is open to residents age 12 and older.

Henderson echoed appreciation for the Cleanup Day and highway cleanup events.

COUNCIL BUSINESS

New Business

I. Park Commission; Launch Park Passport Program

Farrell presented a Park Passport Program developed by staff in response to a Park Commission initiative to increase park usage. The program uses QR code scanning at park entrance signs, directing users to a Microsoft-hosted page where they can answer questions about the park and log a visit. The program is designed as a family-oriented activity, targeting 8–10 parks in its first year with plans to launch at the May 13 open house and run through October. A rewards component was considered but set aside as impractical to manage at this stage.

Motion to approve the launch of the Park Passport Program was made by Henderson and seconded by Salonek. The motion carried 5-0.

J. Quarterly Report

Doud presented the Q1 2026 financial report, prepared by City Accountant Dena, highlighting the following:

General Fund Cash Balance: The balance is tracking near the center of expectations. A rapid early-year decline is normal as the city operates on the prior year's fund balance while spending against a growing budget. The first tax settlement is expected around July 4.

General Fund Income Statement: Taxes are not received until Q3; intergovernmental reimbursements typically arrive in Q4. A notable revenue increase reflects insurance

reimbursements for several incidents. Higher early expenditures reflect prepaid annual subscriptions and elevated public works overtime from winter plowing through March.

Cash Balances by Fund: Overall balances remain healthy at approximately \$30 million across all funds. EDA cash balances declined following two property purchases.

Water Fund: Generally healthy at approximately \$10 million. A data error in the 2025 current expenditures column was acknowledged; a corrected update will be provided to council. Salaries and benefits will become more precisely allocated as the department develops more specialized staffing over time.

Sewer Fund: Strong, with approximately \$4.6 million on hand. The sewer assessment fund reflects a large 2025 addition due to the booking of assessments for properties in the southwest area of the city related to an \$800,000 pipe installed under the I-94 interchange, assessed to five property owners on a delayed five-year schedule. Met Council treatment charges represent the primary expenditure. Fund is predictable given that the city owns no major sewer infrastructure assets.

Motion to accept the Q1 2026 Quarterly Report was made by Van Asten and seconded by Salonek. The motion carried 5-0.

COUNCIL BUSINESS

Action Items

K. Revisit Appointment to the Water Shed

Farrell brought forward the matter of the current Dayton watershed board representative, who no longer resides in Dayton having moved to Maple Grove. Research had revealed no formal residency requirement for the appointed position. Council discussion reflected broad consensus that the representative should be a Dayton resident, and that regular communication back to the council from any appointee is expected. Council directed staff to post the position to seek a resident interested in serving. In the interim, Alternate Van Asten will attend meetings through the summer months, as she is unavailable to attend during the school year due to her primary employment. The current representative will be informed that he will no longer serve as Dayton's representative.

No formal vote was taken; direction was provided by consensus.

L. Approval of Payment of Claims for May 12, 2026

Van Asten asked about the frequency and value of media production expenditures from C Vision Productions, which appeared three times on the claims roster. Doud explained covering meeting recordings for five commission and council meetings, periodic photography/drone sessions used for city publications and events, and a live burn training video produced for the fire department. Van Asten also raised the question of oversight of utility bills now that they are centralized rather than managed by individual department heads. Doud confirmed that the utility billing coordinator monitors water and sewer usage and reports anomalies. Electricity and gas bills will be more formally monitored going forward. Doud also noted that the city discovered it had been paying approximately \$200 per month to Spectrum for a service that does not exist in city hall and is working to recover a credit for the overpayment.

Fashant requested a breakdown of the fire department repair bill, noting it had been previewed at the prior meeting. Hendrickson confirmed the total was \$9,108.12 — comprising \$3,395.62 in parts (including a \$2,526 Opticon device replacement) and

\$5,617.50 in labor — covering repairs to Engine 21, including a drum brake not releasing properly and an onboard air compressor running continuously.

Motion to approve payment of claims for May 12, 2026 was made by Fashant and seconded by Salonek. The motion carried 5-0.

M. Approval of Legacy Woods Development Agreement

Sevald presented the development agreement for Legacy Woods, a 10-lot single-family subdivision off North Diamond Lake Road northwest of Lake Laura, whose final plat was approved in March 2026. This is the first project utilizing the city's newly drafted development agreement template.

Van Asten provided detailed review comments from having read the entire document, resulting in the following corrections directed by council:

- Section 3.1 (Site Improvements) to reference "city standards" rather than "industry standards."
- Section 4.3 (Access) to include language prohibiting or conditioning construction traffic on Thicket Lane North, and confirming that if used, the developer will be responsible for repairs.
- Section 4.15 to remove the requirement for paved driveways, as the A-3 zoning district allows gravel driveways.
- The homeowners association requirement to be noted as developer-driven and not city-mandated where not strictly necessary (though acknowledged necessary here for maintenance of the monument sign and landscape buffer along North Diamond Lake Road).
- Language to be added requiring cleanup of construction debris, with a provision allowing the city to perform the work and charge the developer if not completed.
- The sentence requiring all mailboxes to be of similar design and color to be stricken, while retaining the requirement for a mailbox at the end of each driveway for public safety and address identification purposes.
- A deed restriction to be recorded against the remaining lot exceeding 20 acres to ensure future owners are bound by A-3 district requirements and cannot subdivide beyond those limits without proper process.

The developer requested a credit of approximately \$28,000 in trail dedication fees in exchange for Outlot A, a parcel intended for future right-of-way connecting to Xanthus Avenue once urban services are extended to the area. Council declined the request, with members indicating that trail dedication fees should be reserved for trail purposes and that this outlot does not constitute trail or park land.

Motion to approve the Legacy Woods Development Agreement with all noted corrections and without crediting trail dedication fees was made by Salonek and seconded by Van Asten. The motion carried 5-0.

N. Award Construction Contract for the River Hills Forcemain Abandonment

Van Asten was shocked the low bid for the River Hills Forcemain Abandonment project was noted to be approximately one-third the cost of the highest bid submitted. Quisberg confirmed that the low bidder is required to meet all city standards and has performed satisfactorily on prior city projects, including the 113th Avenue project.

Fashant asked for clarification on the 4-inch pressure pipe referenced in the project scope; Quisberg explained that sewage from north of Pioneer Parkway is pumped through a force main, which uses pressure to move flow through a smaller-diameter pipe from the lift station.

Motion to award the construction contract for the River Hills Forcemain Abandonment to the low bidder was made by Salonek and seconded by Van Asten. The motion carried 5-0.

ADJOURNMENT

With no objections, Fisher adjourned the meeting at 8:00 PM.

Approved: _____

Attest: Amy Benting

CALL TO ORDER

Mayor Fisher called the work session meeting to order at 5:45 p.m.

PRESENT: Mayor Dennis Fisher, Stephanie Henderson, David Fashant, Scott Salonek, and Sara Van Asten

ABSENT:

ALSO PRESENT: Public Works Superintendent, Marty Farrell; Fire Chief Gary Hendrickson; City Administrator/Finance Director, Zach Doud; Assistant City Administrator/City Clerk, Amy Benting; Police Chief, Paul Enga

FIRE DEPARTMENT RESPONSE LETTER WORKSESSION

Doud started by recapping the Maple Grove and Rogers fire response letters for calls on the south side of Dayton.

Doud had questions from staff and council to lead the discussion from now to the 2031 fire station. Council discussed how the duty crew could have affected the calls from the letter.

Conversation moved to locations of the fire department and being geographically challenged to arrive at all calls before other departments. Response time and alarm changes within meeting the standards needed to respond to calls.

The most critical hours will continue to be analyzed to be staffed appropriately.

Hendrickson stated the aid calls are reciprocal between cities.

Met Council population density was determined as the data to use.

Doud will get more information sent out to council and the draft response letter.

ADJOURNMENT

Fisher declared the meeting adjourned at 6:27 p.m.

Approved: _____

Attest: Amy Benting

ITEM:

Approval of Temporary Liquor License for the Dayton Lions.

PREPARED BY:

Amy Benting, City Clerk

POLICY DECISION / ACTION TO BE CONSIDERED:

Approval of temporary liquor licenses

BACKGROUND:

This is a standard Council approval; the event's are scheduled for 06/05/2026, 06/26/2026, 07/10/2026, 07/24/2026, 08/07/2026, 08/21/2026 these will take place at Elsie Stephens Park 14430 Dayton River Road.

CRITICAL ISSUES:

There are no outstanding issues.

RECOMMENDATION:

Approval of temporary liquor license

ATTACHMENT(S):

N/A

ITEM:

Municipal State Aid Advance Request
Dayton Parkway Traffic Signals Funding

PREPARED BY:

Jason Quisberg, Engineering

POLICY DECISION / ACTION TO BE CONSIDERED:

Approve Resolution 23-2026 Approving Municipal State Aid Street Funds Advance

BACKGROUND:

The Dayton Parkway Traffic Signal project is currently under construction. This project is being funded using an advance of Dayton's future Municipal State Aid (MSA) allotments. This advance was approved by MNDOT's State Aid Department...but only for 2026.

The initial reimbursement request for this project was submitted in December 2025, but not processed until January 2026...after the window for the approved advance. Therefore, only a partial payment was received (in the amount of the balance at the time of processing).

To be reimbursed for the costs not included in that payment, and also those expected to complete the project, a new advance request (for 2026) needs to be submitted. Attached is a Council resolution approving Dayton to initiate the advance. This is required for MNDOT to process the request.

Consistent with the project budget, it is expected that approximately \$550,000 remains to be reimbursed – being either work already completed, but not yet reimbursed, or work yet to be completed, that will then be reimbursed.

RECOMMENDATION:

Approve Resolution 23-2026 Approving Municipal State Aid Street Funds Advance

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ATTACHMENT(S):

Resolution 23-2026 Approving Municipal State Aid Street Funds Advance

Resolution 23-2026
A Resolution Approving Municipal State Aid Street Funds Advance

WHEREAS, the Municipality of Dayton is planning to implement Municipal State Aid Street Project(s) in 2026 which will require State Aid funds in excess of those available in its State Aid Construction Account, and

WHEREAS, said municipality is prepared to proceed with the construction of said project(s) through the use of an advance from the Municipal State Aid Street Fund to supplement the available funds in their State Aid Construction Account, and

WHEREAS, the advance is based on the following determination of estimated expenditures:

Account Balance as of date <u>5/14/2026</u>	\$ <u>0.00</u>
Less estimated disbursements:	
Project # <u>SAP-229-112-005</u>	\$ <u>550,000</u>
Bond Principle (if any)	\$ _____
Project Finals (overruns-if any)	\$ _____
Other _____	\$ _____
Total Estimated Disbursements	\$ <u>550,000</u>
Advance Amount (amount in excess of acct balance)	\$ <u>550,000</u>

WHEREAS, repayment of the funds so advanced will be made in accordance with the provisions of Minnesota Statutes 162.14, Subd. 6 and Minnesota Rules, Chapter 8820.1500, Subp. 10b, and

WHEREAS, the Municipality acknowledges advance funds are released on a first-come-first-serve basis and this resolution does not guarantee the availability of funds.

NOW, THEREFORE, Be It Resolved: That the Commissioner of Transportation be and is hereby requested to approve this advance for financing approved Municipal State Aid Street Project(s) of the Municipality of Dayton in an amount up to \$550,000.00. I hereby authorize repayments from subsequent accruals to the Municipal State Aid Street Construction Account of said Municipality from future year allocations until fully repaid.

I HEREBY CERTIFY that the above is a true and correct copy of a resolution presented to and adopted by the Municipality of Dayton, County of Hennepin, State of Minnesota, at a duly authorized Municipal Council Meeting held in the Municipality of Dayton, Minnesota on the ____ day of _____, 20____, as disclosed by the records of said Municipality on file and of record in the office.

Motion by _____, second by _____
Motion passes _____.

Mayor

Attest:
City of Dayton

City Clerk

ITEM:

Resolution 21-2026, Waving City Code 1002.08, Subd 6 (Proof of Recording) for French Lake Industrial Center Six

APPLICANT/PRESENTER:

Kirsten Mussman, Graco

PREPARED BY:

Jon Sevald, Community Development Director

POLICY DECISION / ACTION TO BE CONSIDERED:

1. Approve the Resolution, waiving Proof of Recording.
2. Deny the Resolution, Finding reasoning does not justify the waiver.

BACKGROUND:

On March 24, 2026, the City Council approved the Final Plat of French Lake Industrial Center Six, including an office building for Graco.¹ Graco submitted the plat to the Hennepin County Surveyor for review. This review takes about a month and is required prior to recording.

City Code 1002.08, Subd 6 states *“The City shall not take any action related to the plat such as issuing building permits or awarding public street contracts on the platted property until the time as the City Clerk has been satisfied that the plat has been recorded with Hennepin County.”*

Staff has completed permit review. Graco is requesting the City Council waive City Code 1002.08, Subd 6, such that the city can issue building permits prior to the plat being recorded. This waiver will allow Graco to start 2-4 weeks earlier.

CRITICAL ISSUES:

Precedence	This plat is a replat of an outlot with no public infrastructure construction. Permitting building construction prior to recording of this plat has minimal risk to the city. Risks include Hennepin County rejecting the plat, causing the plat to be redesigned and shifting property lines and easements such that structures (under construction) are not compliant with setbacks.
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STAFF RECOMMENDATION:

Staff recommends Approval.

RELATIONSHIP TO COUNCIL GOALS:

N/A

BUDGET IMPACT:

N/A

¹ Resolution 10-2026, *Granting Approval of Preliminary Plat Amendment and Final Plat, and Planned Unit Development Amendment and Conditional Use Permit and Site Plan for French Lake Industrial Center Six*

ATTACHMENT(S):

Resolution 21-2026

Applicant's request, May 20, 2026

RESOLUTION 21-2026

**CITY OF DAYTON
COUNTIES OF HENNEPIN AND WRIGHT
STATE OF MINNESOTA**

WAIVING CITY CODE 1002.08, SUBD 6 (PROOF OF RECORDING), ALLOWING THE CITY TO ISSUE BUILDING PERMITS PRIOR TO RECORDING OF THE PLAT OF FRENCH LAKE INDUSTRIAL CENTER SIX

WHEREAS, City Code 1002.08, Subd 6 (Proof of Recording) states; *“The City shall not take any action related to the plat such as issuing building permits or awarding public street contracts on the platted property until the time as the City Clerk has been satisfied that the plat has been recorded with Hennepin County.”* and,

WHEREAS, the City Council approved Resolution 10-2026 *Granting Approval of Preliminary Plat Amendment, and Final Plat, and Planned Unit Development Amendment, and Conditional Use Permit, and Site Plan for French Lake Industrial Center Six*; and,

WHEREAS, Graco has applied for a building permit on Lot 1, Block 1 of French Lake Industrial Center Six for a 96,000 sq ft office building, employing 250 personnel; and,

WHEREAS, Graco has submitted the plat to Hennepin County for recording. The county is experiencing a delay in reviewing and recording plats, anticipating a 2-4 week delay before the city is able to issue building permits; and,

WHEREAS, it is Staff’s opinion that due to the fact that the plat includes no new city infrastructure, there is minimal risk to the city if the city issues building permits prior to recording of the plat.

DECISION

NOW, THEREFORE, BE IT RESOLVED, the City Council of the City of Dayton waives City Code 1002.08, Subd 6 (Proof of Recording), permitting the city to issue permits prior to recording of the plat with the following conditions:

1. McGough Construction and Graco Inc. assume all risk and responsibility if the city issues permits prior to recording of the plat of French Lake Industrial Center Six, and it is found that the recorded plat is inconsistent with the City Council approved plat. The plat shall be recorded prior to the city issuing a Certificate of Occupancy.

Adopted by the City Council of the City of Dayton on this 26th day of May, 2026.

Dennis Fisher, Mayor

ATTEST

Amy Benting, City Clerk

Motion by Councilmember _____. Second by Councilmember _____.

Ayes:

Nays:

Resolution *Approved*

From: [Kirsten A. Mussman](#)
To: [Jon Sevald](#); [Jason Quisberg](#)
Cc: [Pearson, Nate](#)
Subject: Graco Building Permit
Date: Wednesday, May 20, 2026 9:35:38 AM

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hello Jon and Jason,

Per our recent correspondence, our team has been actively coordinating with the City regarding the new Graco Headquarters project and appreciates your continued support throughout the review process. The current project schedule anticipates issuance of the building permit by the end of May, which remains critical to maintaining overall schedule commitments.

As discussed, one of the City's prerequisites for permit issuance is recording the new plat with Hennepin County. We have recently communicated that Hennepin County is experiencing significant delays affecting this project and others across the county. As of this past Monday, we were informed that plat recording will not occur until June 10 at the earliest, and potentially as late as June 17.

In parallel, the construction team has procured structural steel for delivery and erection beginning June 1. This is critical path for our schedule and corporate objective to vacate the Minneapolis property by June 2027.

Accordingly, this correspondence serves as a formal request for the City to waive the plat recording requirement for the purpose of issuing the building permit. Our design team, including licensed engineers and architects, have thoroughly evaluated the situation and are confident in proceeding. Graco is prepared to accept the associated risk should any issues arise during the County's final review and recording process.

We appreciate the City's consideration of this request and remain committed to working collaboratively to keep the project on track while maintaining the integrity of the review process.

Much Appreciated,
Kirsten Mussman

Kirsten Mussman

Graco Inc. | Facilities Services Manager
65 11th Ave NE, Minneapolis 55413
507-390-4686 (m) | kirsten_a_mussman@graco.com

PRESENTER: Marty Farrell

ITEM: Hiring of Public Works Seasonal Positions

PREPARED BY: Marty Farrell

POLICY DECISION / ACTION TO BE CONSIDERED: Approving hiring of 4 Seasonal positions

BACKGROUND: Staff initially budgeted for 3 Seasonal positions, but are now requesting 4 hires, this is due to unforeseen staff absence. One of the Public Works Crew was involved in a no fault accident, which resulted in him requiring surgery, he will be off work for 12 weeks. The additional seasonal would allow us to take staff off the mowing crew to help complete scheduled summer projects.

CRITICAL ISSUES: N/A

BUDGET IMPACT: 3 positions are included in the 2026 budget. The 4th position would be covered from the salary reimbursement being paid to the City by workers compensation (66% of salary) for the injured staff member. The pay rate for 3 of the 4 hires is \$20 per hour, the 4th is a returning seasonal who will receive \$22 Per hour.

RECOMMENDATION: Approve hiring of 4 Public Works Seasonals

ATTACHMENT(S): None.

PRESENTER: Marty Farrell

ITEM: Approval of Well Head treatment Work Change Directive #12 from Magney Construction for \$34,079.69

PREPARED BY: Marty Farrell

POLICY DECISION/ACTION TO BE CONSIDERED: Approval of Well Head Treatment Plant Work Change Directive #12 from Magney Construction.

BACKGROUND: Work Change Directive #12 addresses the plumbing changes required by Minnesota Department of Health before they would approve the plant to go online. The work is primarily plumbing related to discharge into the City Sanitary Sewer, and overflow system to manage any potential backflow from the City Sanitary Sewer into the Well Head Treatment plant. There are also upgrades to 2 pumps that are required due to these changes.

BUDGET IMPACT:

The total Magney Contract budget excluding Change Orders	\$7,256,700.00
Total Magney Contract including Change Orders and WCDs	\$7,583,636.05
Total remaining Magney Contract budget including Change Orders	\$608,357.62

Project funded from \$4,000,000 Federal Grant, \$1,750,000 and Water Enterprise Fund 601.

RECOMMENDATION: Approve WCD #12 from Magney Construction for \$34,079.69.

ATTACHMENT(S): WCD#12, budget spreadsheets

Project Financial Breakdown		Magney	Magney Change Orders	Magney Contract Balance
Date	Pay Application/Change Order	Magney Pay App Amount		
	Starting Balance			7,256,700.00
	Pay Application 1 (Magney)	\$ 220,114.44		\$ (20,114.44)
	Pay Application 2 (Magney)	\$ 121,201.36		\$ (121,201.36)
	Pay Application 3 (Magney)	\$ 325,410.38		\$ (325,410.38)
	CO#1		\$ 33,873.28	\$ 33,873.28
2/5/2024	Pay Application 4 (Magney)	\$ 475,489.08		\$ (475,489.08)
3/1/2024	Pay Application 5 (Magney)	\$ 238,137.78		\$ (238,137.78)
4/2/2024	Pay Application 6	\$ 75,375.85		\$ (75,375.85)
5/2/2024	Pay Application 7	\$ 237,614.95		\$ (237,614.95)
6/3/2024	Pay Application 8	\$ 740,678.00		\$ (740,678.00)
7/23/2024	Pay Application 9	\$ 27,835.00		\$ (27,835.00)
7/17/2024	CO#2		\$ 45,000.00	\$ 45,000.00
12/2/2024	Pay Application 10	\$ 505,636.31		\$ (505,636.31)
2/19/2025	CO#3		\$ 172,182.38	\$ 172,182.38
3/31/2025	Pay Application 11	\$ 976,450.47		\$ (976,450.47)
5/1/2025	Pay Application 12	\$ 280,900.97		\$ (280,900.97)
5/30/2025	Pay Application 13	\$ 239,382.06		\$ (239,382.06)
5/20/2025	WCD 03 Non Standard paint cost \$231.00 included on CO#4		\$ -	\$ -
6/13/2025	WCD 04 Plumbing changes \$3806.76 included on CO#4		\$ -	\$ -
7/16/2025	WCD05 Chemical Feed Changes (MDH Directed) COI#4	\$ -	\$ -	\$ -
7/1/2025	Pay Application 14	\$ 191,897.08		\$ (191,897.08)
7/17/2025	CO#4 Includes WCD 3,4,5,		\$ 12,990.80	\$ 12,990.80
9/26/2025	Pay Application 15	\$ 477,884.16		\$ (477,884.16)
10/3/2025	WCD06 Remove Blower piping not required per manufacturer		\$ (1,797.57)	\$ (1,797.57)
10/3/2025	Pay Application 16	\$ 278,823.62		\$ (278,823.62)
10/13/2025	WCD07 Add Gate Valve for WHTP isolation		\$ 6,918.03	\$ 6,918.03
10/13/2025	Pay Application 17	\$ 217,322.67		\$ (217,322.67)
11/4/2025	Pay Application 18	\$ 277,098.03		\$ (277,098.03)
10/25/2025	WCD 08 Watermain pipe reducer piping		\$ 2,929.57	\$ 2,929.57
11/10/2025	WCD09 Sanitary Line extension		\$ 11,903.44	\$ 11,903.44
12/1/2025	Pay Application 19	\$ 172,755.31		\$ (172,755.31)
1/5/2026	Pay Application 20	\$ 333,339.80		\$ (333,339.80)
2/18/2026	WCD 10 (will be refunded by B&V)		\$ 4,471.99	\$ 4,471.99
2/18/2026	Pay Application 21	\$ 292,194.34		\$ (292,194.34)
2/27/2026	Pay Application 22	\$ 241,414.00		\$ (241,414.00)
4/6/2026	Pay Application 23	\$ 28,322.77		\$ (28,322.77)
3/13/2026	WCD 11 Filter Effluent Air Release Valve		\$ 4,384.44	\$ 4,384.44
5/15/2026	WCD 12 MDH Backflow prevention		\$ 34,079.69	\$ 34,079.69
	Work Change Directive (WCD)			
		\$ 6,975,278.43	\$ 326,936.05	\$ 608,357.62

AS OF 05-20-2026			
Contract Balances to Project Close Out			
	Magney	\$	608,357.62
	B&V	\$	113,354.99
	AET	\$	32,461.90
Remaining Contract Balances Total		\$	754,174.51
	Project Starting Balance	\$	8,194,300.00
	Minus total pay apps to date	\$	(7,367,061.54)
Remaining Project Balance		\$	827,238.46
	Remaining Project Balance	\$	827,238.46
	Minus remaining Contract Balances	\$	(754,174.51)
Contingency		\$	73,063.95

WORK CHANGE DIRECTIVE

No. 12

DATE OF ISSUANCE 05/15/2026

EFFECTIVE DATE _____

Owner: City of Dayton, MN

Contractor: Magney Construction, Inc.

Contract: Dayton Filtration Plant Wells 4 & 5

Project: City of Dayton Filtration Plant Wells 4 & 5

Engineer's Project No.: 414098

Contractor is directed to proceed promptly with the following change(s):
Description:

- Revise the discharge of the Sludge Pumps that route to the Building Sanitary Sump Pit to route above the operating floor of the filter room and discharge through an air gap and standpipe into the Building Sump. New piping above the filter room floor shall be 6" Sch. 80 PVC and shall be field routed between 869.50' and 872.00'.
- The discharge of both pumps shall tee together before entering the standpipe.
- Provide a P-trap beneath the standpipe before entering the Building Sump.
- Modify or replace the Building Sanitary hatch as required to connect the standpipe to the hatch. The hatch and all connections shall be gas-tight.
- Plug the existing 4" pipe connecting to the Building Sump at the sump wall. Install a blind flange above the floor on the flange of the abandoned 4" pipe that routes below the floor to the sump.
- Replace the impeller on both Sludge Pumps (2 total) with model CB1.1_PE 20-6, 8-3/8" impeller. Coordinate with Zane Johnson of Midwest Pump for the impeller selection.
- Install a 4" PVC overflow from the Building Sanitary Sump to the drainage ditch in the yard to the east of the building. Core drill a hole for the overflow and provide a link seal for the wall connection. Where the overflow terminates in the yard, provide a duckbill gate or flap gate to prevent drafts and pests from entering the sump pit.
- Refer to WCD 12 Attachments for additional clarification of the revisions.

This work will be performed on a time & materials basis, not to exceed the amount listed in this document. The costs will be adjusted after the work is complete.

Attachments: WCD 12 Attachments (markups of Drawings M-102, M-502, C-203 and a new Standpipe Detail), COR 15.

Purpose for Work Change Directive:

Directive to proceed promptly with the Work described herein, prior to agreeing to changes in Contract Price and Contract Times, is issued due to:

- Non-Agreement on pricing of proposed change.
- Necessity to proceed for schedule or other Project reasons.

Estimated Change in Contract Price and Contract Times (non-binding, preliminary):

Estimated increase (decrease) in Contract Price:

\$ 34,079.69.

Estimated increase (decrease) in Contract Times:

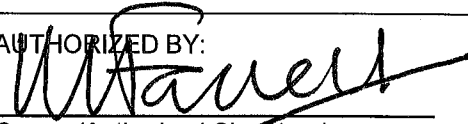
Substantial Completion: 0 days;
Ready for final payment: 0 days.

If the change involves an increase, the estimated amount is not to be exceeded without further authorization.

Basis of estimated change in Contract Price:

- Unit Price
 - Lump Sum
 - Cost of the Work
 - Other
-

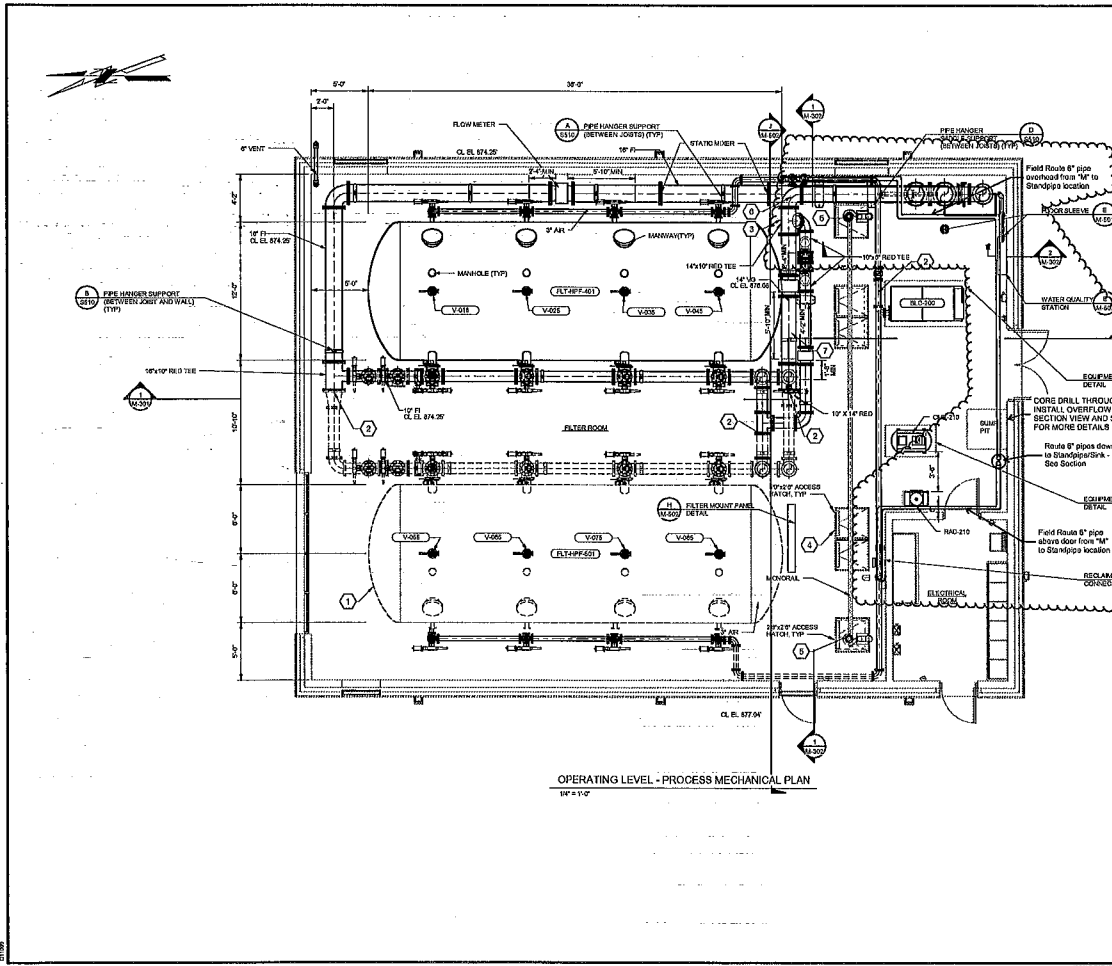
AUTHORIZED BY:



Owner (Authorized Signature)

Title: PW DIRECTOR

Date: 5-15-26



GENERAL NOTES

1. ALL FLOOR PENETRATIONS SHALL BE WATER-TIGHT.
2. CONTRACTOR SHALL BE RESPONSIBLE FOR CONNECTING TO THE FIRM FROM THE FIRM PANEL TO ASSOCIATED VALVES.
3. ALL FLOW METERS SHALL HAVE A GROUND-UP RUN UPSTREAM AT A MINIMUM LENGTH OF 4 TIMES THE DIAMETER, AND DOWNSTREAM AT A MINIMUM LENGTH OF 2 TIMES THE DIAMETER.

SHEET KEYNOTES

1. THE 45° DASHED ON THIS DRAWING ARE PART OF BID ALTERNATE A WHICH INCLUDES FLOWMETERS AND RECLAIM/REUSE SWAMP CONNECTION DETAIL LOCATED BENEATH PIPING. SEE OTHER DRAWINGS FOR LOCATION.
2. PROVIDE 6" X 6" FLANGE IF BID ALTERNATE A IS NOT PROVIDED.
3. THIS SHALL BE ROTATED 45 DEGREES FROM PARALLEL.
4. ALL VALVES ARE TO BE SLUSH WITH FLOOR ELEVATION OF 867.00 AND SHALL HAVE AN INCH DOWN DRAIN CONNECTED TO THE SANITARY LINE AS SHOWN ON P-F-11 (TYP) FOR MORE DETAILS.
5. CONTRACTOR TO PROVIDE STOPS FOR MANHOLES TO ALLOW THE HOIST TO STOP AT THE CENTER POINT OF THE PUMP/LET-FITTING.
6. RECLAIM/REUSE SWAMP CONNECTION DETAIL LOCATED BENEATH PIPING. SEE OTHER DRAWINGS FOR LOCATION.
7. FILTER MOUNTING PANEL DETAIL LOCATED BENEATH PIPING. SHALL MATCH DETAIL AND OTHER FILTER LOCATION.

BLACK & VEATCH
Black & Veatch Corporation
Minneapolis, Minnesota

DESIGNED BY: [REDACTED]
CHECKED BY: [REDACTED]
DATE: 09/23/09

CITY OF DAYTON
City of Dayton
Dayton, Ohio

DAYTON FILTRATION PLANT WELLS 4 & 5
PROCESS MECHANICAL
OPERATING LEVEL PLAN

REVISIONS AND RECORD OF ISSUE

NO.	DATE	DESCRIPTION
1	09/23/09	ISSUED FOR PERMIT

PROJECT NO.: 414005

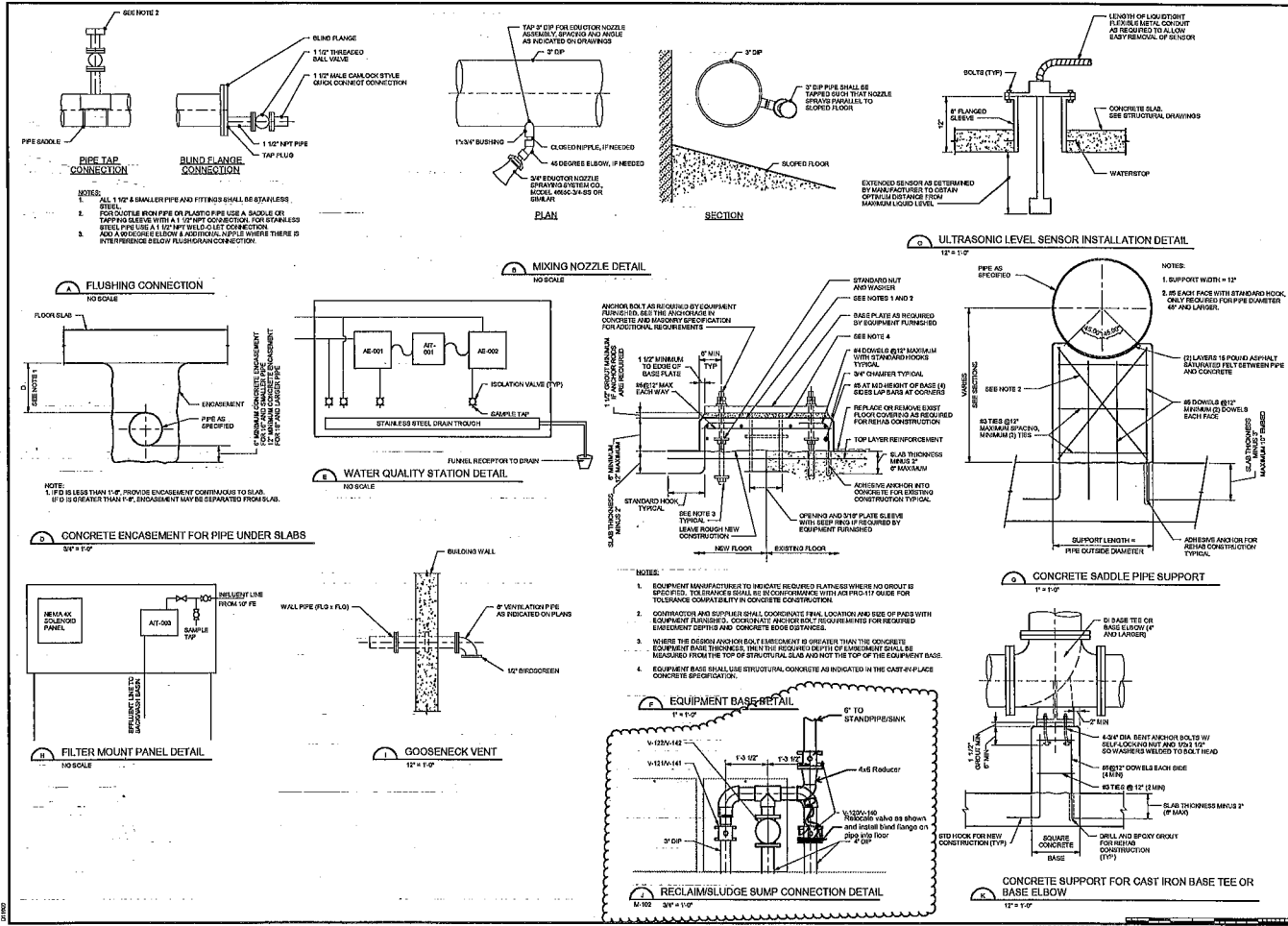
DAYTON FILTRATION PLANT WELLS 4 & 5
PROCESS MECHANICAL
OPERATING LEVEL PLAN

M-102
42 OF 78

OPERATING LEVEL - PROCESS MECHANICAL PLAN
1/4" = 1'-0"



SCALE DRAWN BY: PATTIL (SCALE) 3



BLACK & VEATCH
Black & Veatch Corporation
Minneapolis, Minnesota

1. DESIGN CHECKED BY: [Signature]
2. PROJECT NO.: 2019-003
3. DATE: 07/10/2019
4. SHEET NO.: 48 OF 78

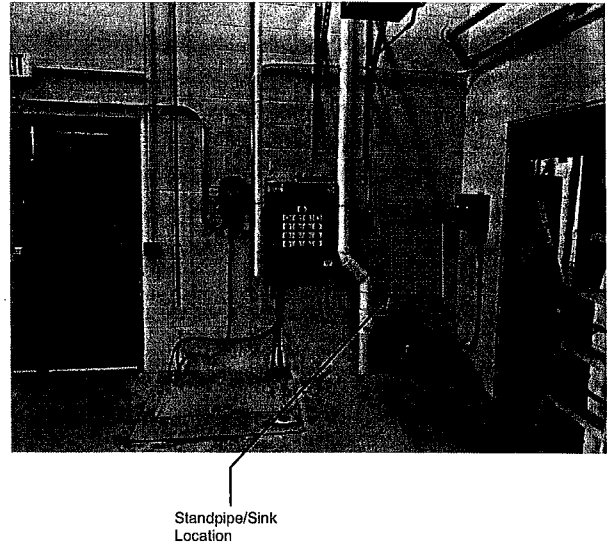
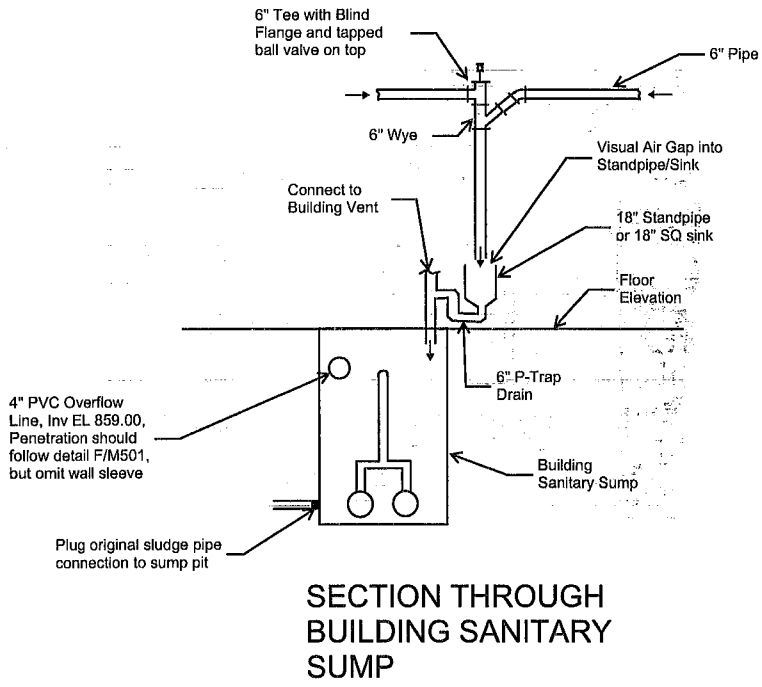
CITY OF DAYTON
Dayton, Ohio

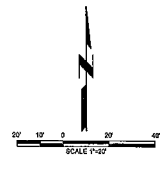
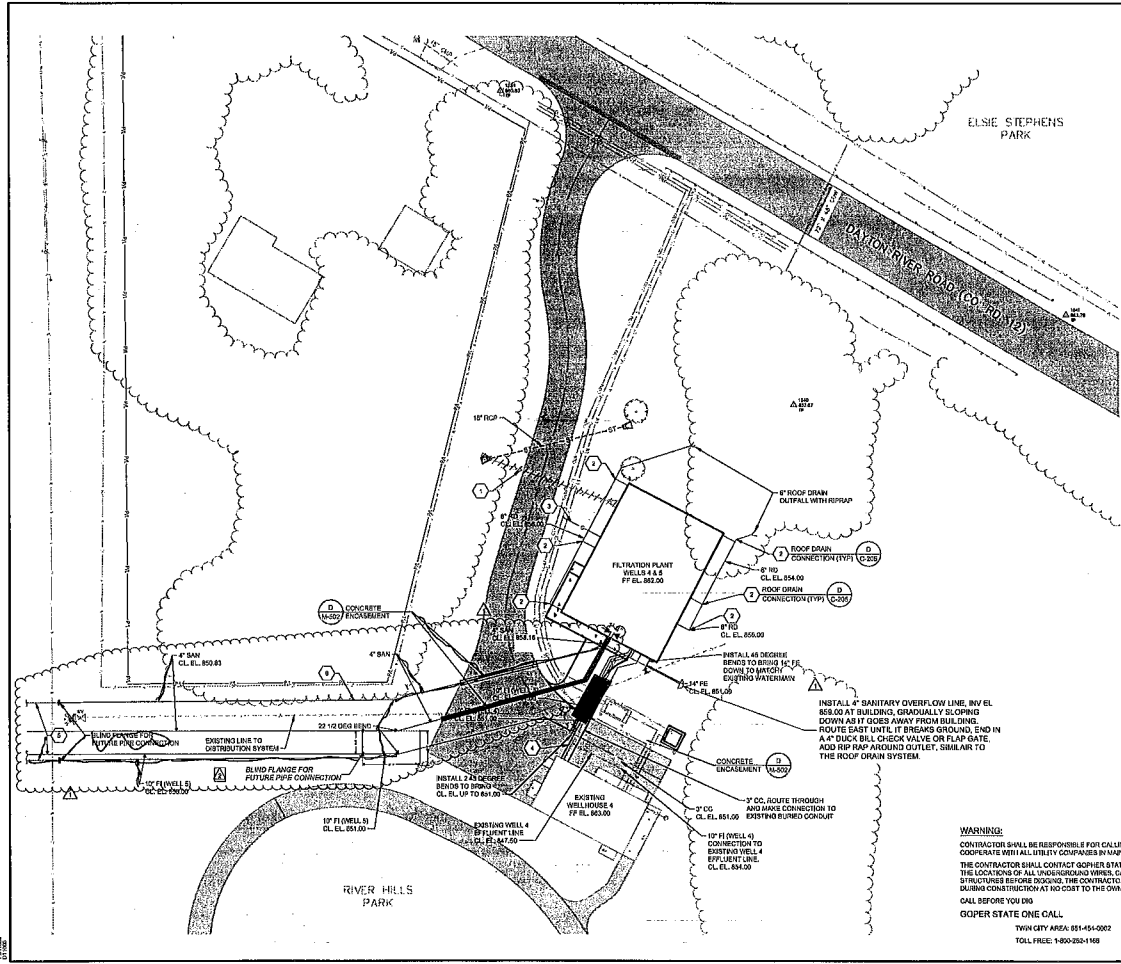
DAYTON FILTRATION PLANT WELLS 4 & 5

PROCESS MECHANICAL

DETAILS 2

M-502





GENERAL NOTES:

1. CONTRACTOR TO FIELD LOCATE PIPE AND REMOVE PAVEMENT AS REQUIRED FOR RELOCATING CONDUITS TO DEEPER PAVEMENT TO MATCH PRE-EXISTING CONDUIT.
2. ALL BURIED CP PIPE SHALL BE FULLY RESTRAINED PERMANENTLY.
3. PROVIDE A BRACKET AND SHROUD SYSTEM FOR PROTECTION OF THE ROAD AND EXISTING PIPES AND EQUIPMENT.

KEY NOTES:

1. EXISTING STORMWATER PIPE MAY BE REUSED AT THE DISCRETION OF THE CONTRACTOR IF IT IS SALVAGEABLE AND IN ACCORDANCE WITH THE SPECIFICATIONS OF THE OWNER.
2. ROOF DRAIN CONNECTION SHALL BE SHIELD AND BE PROTECTED BY METAL DOWNSPOUT LOCATIONS.
3. SEE PLAN FOR NATURAL GAS PIPING ELEVATION.
4. CONTRACTOR TO FIELD VERIFY EXISTING CONDUIT LOCATIONS, ROUTES TO NEW BUILDING AT ELEVATION OF 84.00.
5. COORDINATE CONNECTION OF PIPE WITH PIPELINE FROM DEVELOPMENT FROM THE WEST INSTALLED UNDER A SEPARATE CONTRACT. FIELD VERIFY LOCATION OF PIPELINE FROM DEVELOPMENT.
6. REMOVE AND REINSTALL FENCE AS REQUIRED FOR INSTALLATION OF NEW PIPELINE.

INSTALL 4\"/>

WARNING:

CONTRACTOR SHALL BE RESPONSIBLE FOR CALLING FOR LOCATIONS OF ALL EXISTING UTILITIES. THEY SHALL COOPERATE WITH ALL UTILITY COMPANIES IN MARKING THEIR SERVICE AND/OR LOCATIONS OF LINES. THE CONTRACTOR SHALL CONTACT GOPHER STATE ONE CALL AT 88-1-64-0002 AT LEAST 48 HOURS IN ADVANCE FOR THE LOCATION OF ALL UNDERGROUND UTILITIES. CHECKS, CONDUITS, PIPES, MANHOLES, VALVES OR OTHER BURIED STRUCTURES BEFORE DIGGING. THE CONTRACTOR SHALL REPAIR OR REPLACE THE ABOVE WHEN DAMAGED DURING CONSTRUCTION AT NO COST TO THE OWNER.

CALL BEFORE YOU DIG
 GOPHER STATE ONE CALL
 TWIN CITY AREA: 651-454-0002
 TOLL FREE: 1-800-250-1166

BLACK & VEATCH
 Black & Veatch Corporation
 Minneapolis, Minnesota

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY AN ENGINEER OR ARCHITECT LICENSED IN THE STATE OF MINNESOTA.

BENJAMIN R. CLAPP
 DATE: _____
 LICENSE NO.: 48119

CITY OF DAYTON
 City of Dayton
 DAYTON, OHIO

DAYTON FILTRATION
 PLANT WELLS 4 & 5

PRELIMINARY
 NOT FOR
 CONSTRUCTION

DESIGNED BY	_____
CHECKED BY	_____
DATE	_____
PROJECT NO.	_____

SITE

CIVIL

PIPING PLAN
 C-203 OF



SEND TO	FROM
Company Name Black & Veatch	Magney Representative Dan Fuhrman
Attention Benjamin Clapp, PE	Date 4/29/2026 --- updated 5/14/2026
Fax (952) 896-0702	Proposal # COR No. 15 - Sludge Pump Discharge Revisions

COMMENTS

Dear Mr. Clapp, PE

Magney Construction, Inc. is pleased to present the following cost to include the necessary labor, material and equipment to make revisions to the sludge pump discharge at the Dayton Filtration Plant, as well as add an overflow line as required by the MDH. The estimated cost of this work has been summarized below, with the plan to perform the work on a T&M Basis.

LABOR:	\$	16,606.80
Layout/install new PVC sludge pump discharge headers, drop pipe, P-Trap, excavate outside of bldg. to core drill for new overflow line, excavate and install overflow line from bldg. to the creek, install rip rap and duckbill		
MATERIAL:	\$	12,550.63
Pipe supports/hangers, anchors and hardware, PVC Pipe & Fittings, impellers, equipment fuel, sales tax, freight		
EQUIPMENT:	\$	650.00
Mini-Excavator (2) Days		
SUBCONTRACTOR:	\$	1,500.00
Erickson Electric - Disconnect & Re-connect conduit/wiring on valves for the sludge pump discharge piping changes		
SUBCONTRACTOR:	\$	2,000.00
Champion Coatings - Painting the added PVC piping/fittings		
SUBCONTRACTOR:	\$	1,500.00
Midwest Controls - Relocate control box to avoid interference with new drop pipe		
CREDIT: Copper piping for pneumatic valves	\$	(2,855.00)
CREDIT: Submittal Reviews	\$	(3,000.00)
Subtotal:	\$	28,952.43
Contractor's Overhead:	\$	4,623.62
Bond & Insurance Premium:	\$	503.64
LUMP SUM TOTAL	\$	34,079.69
Sludge Pump Discharge Revisions Cost:	\$	27,836.16
4" Overflow Addition Cost:	\$	12,186.36

Additional Working Days Required for this Change	0
--	---

EXCLUSIONS: No cold weather conditions, testing, painting, electrical, or mechanical not listed (above/attached) are included. If this work can take place concurrently with the other work in the contract documents, and current sequence of work, then additional supervision time will not be needed - if it is required, additional supervision cost will need to be added.

Please review and contact me if you have any questions.

Thank You,

Daniel Fuhrman
Project Manager

Accepted By:

Date:



MCM Manufacturing
 441 W. Dual Blvd.
 P.O. Box 489
 Isanti, MN 55040-0489
 United States of America

Ph: 763-444-7335

Fax: None

Quote

ID: 35252 Date: 16-Apr-26

To

Quote To

Magney Construction
 1401 Park Rd
 Chanhassen, MN 55317
 United States of America

Magney Construction Inc.
 Magney Construction Inc.
 1401 Park Rd
 Chanhassen, MN 55317
 United States of America

Ph: 952-474-1674

Ph: 952-474-1674

Terms		Ship Via		Salesperson
Net 30		Best Way		
Quantity	Description	Unit Price	Amount	
	To submit an order, please send to orders@mcm-mfg.com. DO NOT submit orders to your sales person!			
	Lead times can vary for many reasons, please contact Robyn for current leadtimes of your products. robyn@mcm-mfg.com or 763-452-0462			
	Line: 001 Expiration Date: 30-Apr-26 Part: MWTP-C 18 Rev: A Wall brackets 18" Powder coat zinc rich primer. REV A change holes to .625" in Back Plate MWTP-C 18-04			
10 ea		\$131.68		\$1,316.80
25 ea		\$400.00		\$10,000.00
	Line: 002 Expiration Date: 30-Apr-26 Part: U-CLAMP-375-ROD Rev: - U clamp formed rod Formed from 3/8-16 galvanized rod SAME GEOMETRY AS U-CLAMP SIDE VIEW			
10 ea		\$36.30		\$363.00
25 ea		\$26.00		\$650.00
	Please reference this quote number on all inquiries for lead time and also on your purchase orders.			
	To submit an order, please send to orders@mcm-mfg.com. Any other placement of the order cannot be guaranteed and your order may not be entered in a timely manner.			

B-FIRST INDUSTRIAL

600 INDUSTRIAL DR SW
WILLMAR, MN 56201
PHONE 320-235-0555
WWW.BFIRSTINDUSTRIAL.COM

QUOTE

EXPIRATION DATE	QUOTE DATE	QUOTE #
	04/16/26	80033496-00
OUTSIDE REP	PO #	PAGE #
WADE SEBOLD		1 of 1
TAKEN BY	INSTRUCTIONS	
CHRIS WHARTON		
CONTACT NAME		

BILL TO: 549043	MAGNEY CONSTRUCTION INC. 1401 PARK ROAD CHANHASSEN, MN 55317-9592
--------------------	---

SHIP TO:	MAGNEY CONSTRUCTION INC. 1401 PARK ROAD CHANHASSEN, MN 55317
----------	--

TERMS	SHIP POINT	SHIP VIA
1% 10 NET 30	BFIRST INDUSTRIAL - 0080	ZONE 1

LINE #	PRODUCT AND DESCRIPTION	QUANTITY ORDERED	QTY U/M	UNIT PRICE	PRICE U/M	EXT. PRICE
1	DIT 050 RED 1/2 MULTISETTING TOOL REDHD (RT-112) INTERCHANGE PROD: R-RT12	1	EA	4.06154	EA	4.06
2	DIL 050 RED 1/2 MULTISSET ANCHOR REDH D CUST ITEM: R-RM12	50	EA	0.79185	EA	39.59
3	WJ 050 0550 RED 1/2 X 5-1/2 WEDGE ANCHOR RED HEAD INTERCHANGE PROD: R-WS1254	25	EA	1.26918	EA	31.73
4	TRODZ 100 0600 8 1-8X6' TROD ZINC	3	EA	35.82819	EA	107.48
5	TRODZ 050 0600 13 1/2-13X6' TROD ZINC 12/TUBE	2	EA	6.31661	EA	12.63

5 LINES TOTAL	TOTAL	195.49
	ROAD IMPROVEMENT AND FOOD DELIVERY FEE	0.50
	TAXES	16.38
	QUOTE TOTAL	212.37

UNLESS OTHERWISE STATED ABOVE, ACTUAL QUANTITY SHIPPED MAY VARY BY 10%. CUSTOMER AGREES TO PAY THE INVOICE; REASONABLE ATTORNEYS' FEES AND COSTS MAY BE ACCESSED FOR COLLECTION ON ANY ACCOUNTS THAT EXCEED THEIR SPECIFIED PAYMENT TERMS. CUSTOMER MUST OBTAIN A RETURN AUTHORIZATION NUMBER WITHIN 90 DAYS FROM ORIGINAL INVOICE TO RETURN PRODUCT. RETURNS MAY BE SUBJECT TO A RESTOCKING CHARGE. SPECIAL ORDER ITEMS ARE NOT RETURNABLE. PRICING IS SUBJECT TO TARIFFS. ORDERS MAY BE SUBJECT TO DELIVERY CHARGES. SELLER MAKES NO WARRANTIES, EITHER EXPRESS OR IMPLIED, AS TO THE GOODS SOLD HEREBY, INCLUDING NO WARRANTIES OF MERCHANTABILITY OR FITNESS FOR A PARTICULAR PURPOSE. CUSTOMER SHALL USE THE PRODUCTS ONLY AS SPECIFIED BY THE DESIGN AND ENGINEERING AND AS INTENDED BY THE MANUFACTURER. IN NO EVENT SHALL SELLER BE LIABLE FOR INCIDENTAL, SPECIAL, INDIRECT, CONSEQUENTIAL OR PUNITIVE DAMAGES RESULTING FROM THE SALE OF THE PRODUCTS. CUSTOMER SHALL DEFEND, INDEMNIFY AND HOLD SELLER HARMLESS FROM ANY AND ALL CLAIMS, SUITS, JUDGMENTS AND LIABILITIES OF ANY NATURE WHATSOEVER, ARISING OUT OF OR RESULTING FROM THE SALE OF THE PRODUCTS.



6530 Cambridge Street
 NORTH DOCK #2
 Minneapolis, MN 55426
 Phone: 952/925-5075

SALES ORDER

SO0610139

Sales Order Date: 4/21/2026
Terms: Net 30 Days
Contact: AP-GENERAL
ContactPh: 952/474-1674
Customer ID: MCM002
Shipto Code: MCM001

Sold To:
 MAGNEY CONSTRUCTION, INC.
 AP-GENERAL
 1401 PARK RD
 CHANHASSEN, MN 55317-9592
 United States

Ship To:
 MAGNEY CONSTRUCTION, INC.
 1401 Park Road
 CHANHASSEN, MN 55317

PO #: TRAVIS

ShipVia	FOB	SalesPerson	Entered By
	Origin	Steve Zukowski	Steve Zukowski

Ordered Qty	Shipped Qty	B/O Qty	Item Number Item Description	Cust PN	UoM	Loc	Ship Date	Unit Price	Total Price
2			837-532 6x4 SPxS BUSHING, PVC SCH 80		EACH	00	4/21/2026	28.44	56.88
1			875-060 6" SOC WYE, PVC SCH 80		EACH	00	4/21/2026	175.63	175.63
1			801-060 6" SOC TEE, PVC SCH 80		EACH	00	4/21/2026	71.98	71.98
1			853-060 6" BLIND FLANGE, PVC SCH 80		EACH	00	4/21/2026	71.89	71.89
1			FG-060E 6" FLANGE GASKET, 1/8" THICK, 150# FF, EPDM		EACH	00	4/21/2026	8.14	8.14
6			817-060 6" SOC 45 DEG ELL, PVC SCH 80		EACH	00	4/21/2026	51.43	308.58
12			806-060 6" SOC 90 DEG ELL, PVC SCH 80		EACH	00	4/21/2026	42.80	513.60
1			8172-060 6" TxT PVC-NEOPRENE BULKHEAD FITTING (SPEARS)		EACH	00	4/21/2026	920.43	920.43
1			854-060 6" SOC V.S. FLANGE, PVC SCH 80		EACH	00	4/21/2026	32.65	32.65
120			80-060 6" SCH 80 GRAY PVC PIPE		FOOT	00	4/21/2026	13.19	1,582.80
1			836-060 6" MALE ADAPTER, PVC SCH 80		EACH	00	4/21/2026	51.55	51.55
1			806-040 4" SOC 90 DEG ELL, PVC SCH 80		EACH	00	4/21/2026	15.04	15.04
* 80			80-040 4" SCH 80 GRAY PVC PIPE		FOOT	00	4/21/2026	6.81	544.80
1			S1520-60 6" SOC SWING CHECK VALVE, PVC EPDM - SPEARS		EACH	00	4/21/2026	270.67	270.67

All orders are subject to the terms and conditions available @ shop.indelco.com/terms-service. All pipe and fabricated items are non-cancelable and non-returnable. Returned goods will not be accepted without an authorization # and are subject to a restock charge. claims for shortages must be made within 5 days after receipt of goods.



6530 Cambridge Street
 NORTH DOCK #2
 Minneapolis, MN 55426
 Phone: 952/925-5075

SALES ORDER

SO0610139

Sales Order Date: 4/21/2026
 Terms: Net 30 Days
 Contact: AP-GENERAL
 ContactPh: 952/474-1674
 Customer ID: MCM002
 Shipto Code: MCM001

Sold To:
 MAGNEY CONSTRUCTION, INC.
 AP-GENERAL
 1401 PARK RD
 CHANHASSEN, MN 55317-9592
 United States

Ship To:
 MAGNEY CONSTRUCTION, INC.
 1401 Park Road
 CHANHASSEN, MN 55317

PO #: TRAVIS

ShipVia	FOB	SalesPerson	Entered By
	Origin	Steve Zukowski	Steve Zukowski

Ordered Qty	Shipped Qty	B/O Qty	Item Number Item Description	Cust PN	UoM	Loc	Ship Date	Unit Price	Total Price
1			MISC-2 4" duck bill - low pressure chcek valve- series 730m - FITS SC80 PVC		EACH	00	4/21/2026	139.95	139.95
2			10119 CEMENT, PVC, 711 GRAY #711121, QT (IPS)		EACH	00	4/21/2026	44.37	88.74
2			10222 PRIMER, PVC/CPVC, P-70 CLEAR #770021, QT (IPS)		EACH	00	4/21/2026	39.00	78.00

Non-taxable Subtotal: 0.00
 Taxable Subtotal: 4,660.66 ~~4,393.34~~
 Total Sales Tax: 397.32 ~~389.86~~
Total: 5,057.98 ~~4,783.20~~

All orders are subject to the terms and conditions available @ shop.indelco.com/terms-service. All pipe and fabricated items are non-cancelable and non-returnable. Returned goods will not be accepted without an authorization # and are subject to a restock charge. claims for shortages must be made within 5 days after receipt of goods.

4/21/2026 8:27 AM

Page: 2 of 2



-a UFT Company-

Midwest Pump Works
1 Cannon St W
Dundas, MN 55019

Quote
#QTE011296
04/20/2026

Bill To
Magney Construction
1401 Park Rd
Chanhassen MN 55317
United States
Phone:

Ship To
Magney Construction
1401 Park Rd
Chanhassen MN 55317
United States

Details

Prepared By Zane Johnson **Phone** 877-645-8004 **Email** info@midwestpumpworks.com

Sales Rep House Account **Expires** 04/30/2026 **Terms** Net 30

Item	QTY	Comment	Rate	Amount
10770 ABS HYDRAULIC KIT - XFP080C CB1.1	2		\$1,361.71	\$2,723.42
INSTALL SERVICES	1			\$1,626.10
			Subtotal	\$4,349.52

Total \$4,735.59

Pricing is valid for 10 days and does not include freight charges or applicable taxes.

Items quoted for repair and leftover 30 days, without a decision to repair, will be discarded.

Signature: _____ Date: _____

NOTE: All orders are subject to Tariff Surcharges. Tariff Surcharges are based on Tariff values charged by individual Vendors and are not set amount. These Tariff Surcharges are subject to change at any time.

Online credit card/ECheck payments are handled by a third party processor and 3% fees will apply when making payment.

Thank you for your business.
Toll Free: 877-645-8004 | Email: info@midwestpumpworks.com | Website: https://midwestpumpworks.com



ACCEPTANCE

The following Terms and Conditions are an integral part of the offer to sell the equipment and/or services offered in this proposal. When the BUYER signifies acceptance of this quotation by submission of a Purchase Order or signed SELLER Quotation, it shall become a binding contract when accepted and signed by an authorized signer of the SELLER. Any changes or amendments to this proposal made by the BUYER must have SELLER's approval in writing to become a part of this contract. These Terms and Conditions and the accompanying Purchase Order or signed SELLER Quotation shall comprise the entire agreement between the parties and no course of prior dealings between the parties and no usage of the trade shall be relevant to supplement or explain any terms used in this contract. Unless stated otherwise, the terms and conditions of the manufacturers listed herein will apply to this quotation. Any attachments or listed documents are considered a part of this quotation and are made part of the agreement. **Quote is firm for thirty (30) days unless otherwise stated on the face of the attached quotation.**

APPROVAL DRAWINGS

All items listed are based on SELLER'S interpretation of the requirements in accordance with the plans and specifications. Any preliminary drawings or literature attached to our quotation are for illustration purposes only to show approximate arrangements. Specific drawings and submittal data will be furnished for approval as required after receipt and acceptance of the BUYER'S order. Any submittal or manuals when provided by SELLER will be in the form of a PDF electronic file only. Any form of media beyond the electronic file would be the responsibility of BUYER. Fabrication of products or equipment ordered will not begin until approval and direction to proceed is received in writing. No warranty is made regarding quantities, materials of construction or type of materials quoted. Operation, installation, and maintenance of materials quoted are the responsibility of the OWNER or CONTRACTOR.

DELIVERY

Any shipment or delivery date recited represents our best estimate, but no liability, direct or indirect, is assumed by SELLER for failure to ship or deliver on such dates. Unless otherwise directed, SELLER shall have the right to make early or partial shipments and invoices covering the same to BUYER shall be due and payable in accordance with payment terms hereof. FOB shall be origin unless stated otherwise on the front of these Terms and Conditions. Delivery schedule(s) will be contingent on supply-chain availability and variability for material components, therefore, lead-times are subject to change without notice. Published weights are careful estimates but are not guaranteed. SELLER will endeavor, insofar, as it is possible, to comply with shipping instructions specified by the Purchaser. However, SELLER reserves the right to ship merchandise by such means of transportation as it may select. The manufacturer will ship the equipment via best way. Demurrage shall be billed to the account of the Purchaser. **DAMAGE CLAIMS:** Care is taken in packaging all shipments. After BUYER has been given the receipt by the transportation company, all claims for breakage or shortages, whether concealed or obvious, must be made in writing by the BUYER to the carrier and SELLER within seven (7) days after receipt of shipment. When damage or shortages are obvious, written comments on the bill of lading are required before the driver is released. **RETURNED PRODUCTS:** In no instance is equipment to be returned without first obtaining SELLER'S written approval and returned materials authorization. If shipment is postponed at the request of the purchaser after manufacturing has been commenced, payment will be due on notice from us that the equipment is ready for shipment. Pro rata payments shall be made for partial shipments.

STORAGE

Any item of the product on which shipment is delayed by BUYER may be placed in storage by SELLER at BUYER'S expense and risk. If a delay in shipment is requested by BUYER after an order has been entered and accepted:

- a. No charge will be made if the request for delay is made more than six (6) weeks before acknowledged shipping date and the requested delay is for a period not in excess of thirty (30) days.
- b. A charge will be made if the requested delay exceeds a period of thirty (30) days or if the request is made within six (6) weeks of the acknowledged shipping date. SELLER will advise BUYER of the charge within ten (10) days of receiving BUYER'S request for delay.
- c. If the product is within six (6) weeks of the acknowledged shipping date, then SELLER has the option of completing, invoicing and storing the product and charging one and one-half percent (1.5%) per month, or the maximum percentage permitted by law, whichever is lesser, of the established price for such product, plus storage cost.

PAYMENT

Payment terms, upon credit approval, are of net thirty (30) days from the date of each invoice for material shipped (or when ready for shipment if shipment is deferred by BUYER) **unless stated otherwise on the face of the attached quotation.** Flow down provisions are not accepted and shall not be enforceable against SELLER. Retention is not allowed. In the event any payment becomes past due, a charge of one-half percent (1.5%) will be assessed monthly. These terms are completely independent from, and not contingent upon, when BUYER receives payment from the OWNER. A processing fee of up to four percent (4%) will be added for credit card payments. All merchandise sold is subject to lien laws. Partial or final payment shall constitute acceptance of delivered materials, products, or equipment.

FORCE MAJEURE

Neither Party will be liable for any failure or delay in performing an obligation under these Terms and Conditions that is due to any of the following causes, to the extent beyond its reasonable control: acts of God, accident, riots, war, terrorist act, epidemic, pandemic, quarantine, civil commotion, breakdown of communication facilities, breakdown of web host, breakdown of internet service provider, natural catastrophes, governmental acts or omissions, changes in laws or regulations, national strikes, fire, explosion, generalized lack of availability of raw materials or energy. For the avoidance of doubt, Force Majeure shall not include (a) financial distress nor the inability of either party to make a profit or avoid a financial loss, (b) changes in market prices or conditions, or (c) a party's financial inability to perform its obligations hereunder.

TAXES AND BONDS

Taxes and bonds are **NOT** included in our pricing. Any applicable taxes or bonds will be added to the price and shown separately on each invoice. All prices exclude sales, use, duties, excise, and other taxes in respect to manufacture, sale, or delivery, all of which are to be paid by the buyer unless a proper exemption certificate is furnished. BUYER agrees to reimburse our company for taxes SELLER must pay on BUYER'S behalf.

PRICE ESCALATION and/or MATERIAL DEPOSITS

If between the proposal date and actual procurement and through no fault of the SELLER, the relevant cost of labor, material, freight, brokerage fees, tariffs, and other SELLER costs combined relating to the contract increase, then the contract price shall be subject to escalation and increased accordingly. If required by the BUYER, increase shall be verified by documentation and the amount of contract price escalation shall be calculated as either the actual increased cost to the Seller or, if agreed by the Parties, the equivalent increase of a relevant industry recognized third-party index. SELLER shall undertake good faith efforts to obtain savings in its procurement of materials to avoid escalation costs. BUYER shall cooperate with SELLER in such efforts to obtain such cost savings. SELLER shall contemporaneously track any escalation costs.

**IOWA
MINNESOTA
WISCONSIN**



**PUMP
WORKS**

Phone
515.635.0025

Email
info@lowapumpworks.com

Fax
855.228.6383

Website
www.lowapumpworks.com

CLAIMS AND BACKCHARGES

BUYER agrees to examine all materials immediately upon delivery and report to SELLER in writing any defects or shortages noted no later than ten (10) days following the date of receipt. The parties agree that if no such claim is made within said time, it shall be considered acceptable and in good order with respect to any defect or shortage which would have been revealed by such an inspection. In no event will SELLER be responsible for any charge for modification, servicing, adjustment or for any other expense without written authorization from SELLER prior to the performance of any such work. IN NO EVENT SHALL SELLER BE LIABLE TO BUYER OR ANY THIRD PARTY FOR ANY LOSS OF USE, REVENUE OR PROFIT, OR FOR CONSEQUENTIAL, INDIRECT, INCIDENTAL, SPECIAL, EXEMPLARY, OR PUNITIVE DAMAGES, FOR ANY REASON, INCLUDING WITHOUT LIMITATION, DAMAGES ARISING OUT OF A DELAY IN OR FAILURE OF DELIVERY, DEFECTS IN MATERIAL AND WORKMANSHIP AND/OR FAILURE OF GOODS TO PERFORM TO APPLICABLE SPECIFICATIONS, DRAWINGS, BLUEPRINTS OR SAMPLES AS SET FORTH OR DESCRIBED HEREIN, IF ANY, OF A BREACH BY SELLER OF ANY OTHER TERM OR OBLIGATION OF SELLER UNDER THE CONTRACT. No penalty clauses of any description will be effective unless approved in writing over the signature of a principal of SELLER. Under no circumstances shall SELLER be liable for any consequential, special or incidental damages, including liquidated damages, arising from any breach by it in this transaction, AND ALL SUCH CONSEQUENTIAL, SPECIAL AND INCIDENTAL DAMAGES, INCLUDING LIQUIDATED DAMAGES, ARE EXCLUDED FROM ANY REMEDIES AVAILABLE TO THE BUYER.

SECURITY INTEREST & TITLE

Until all amounts due SELLER have been paid in full, SELLER shall retain a security interest in the product and have all rights of a secured party under the Uniform Commercial Code and applicable law, including the right to repossess the product or equipment without legal process and the right to require the BUYER to assemble the equipment and make it available to SELLER at a place reasonably convenient to both parties.

WARRANTY

Equipment and parts not manufactured by the SELLER carry only the warranty of the manufacturer of said parts. SELLER does not make any express or implied warranty for equipment and/or parts it did not manufacture. Credits for defective material and workmanship in said equipment and/or parts are only in accordance with the underlying company policy of the manufacturer. SELLER makes no warranty whatsoever with respect to any equipment and/or parts as to their merchantability or fitness for a particular purpose. It is further agreed that the SELLER assumes no liability whatsoever for failure of equipment due to normal usage and wear.

INDEMNIFICATION

To the fullest extent permitted by the law in which the project is located, BUYER and SELLER shall indemnify and hold one another and their respective employees and agents harmless from and against all claims, damages, losses, liabilities, actions, causes of action, demands, fines, penalties, judgments, costs, and expenses, including but not limited to attorneys' fees, court costs, expert fees and costs, arising out of or resulting from BUYER's or SELLER's own negligent acts, omissions or misconduct, to the extent such negligence is covered by BUYER's and SELLER's respective insurance policies. In the event any third party asserts against SELLER a claim for patent infringement, royalties or licensing fees with respect to BUYER's use of the products, materials, or equipment provided hereunder, BUYER agrees to indemnify SELLER for all liability damages, costs and expenses in connection therewith.

CANCELLATION

Buyer may cancel this contract only in writing signed by BUYER's duly authorized agent and acknowledged in writing by SELLER's duly authorized agent. Should this order be cancelled, BUYER shall be obligated to pay for the level of work performed and products shipped. Work performed includes any engineering, calculations, preparation of submittals, drawings, and/or travel to job site in relation to this order. In addition to any other remedies provided under these Terms and Conditions, SELLER may terminate this contract with immediate effect by providing signed, written notice to BUYER, if BUYER: (i) fails to pay any amount when due under the contract and such failure continues for 30 days after BUYER's receipt of written notice of nonpayment; (ii) has not otherwise performed or complied with any of these Terms and Conditions; or (iii) becomes insolvent, files a petition for bankruptcy or commences or has commenced against it proceedings in bankruptcy, receivership, reorganization or assignment for the benefit of creditors.

FIELD WORK

Unless specifically stated on our quotation, installation, start-up service, field testing, supervision, operation, and training are not included in our pricing of product. In the event that SELLER or any of its employees or agents do perform work or services on-site at the project's location, BUYER agrees to hold SELLER and its employees or agents harmless for any injuries or damage to property caused by their acts or omission, except to the extent said injuries or property damage arise from gross negligence or intentional misconduct.

MODIFICATIONS

This contract can be modified only in writing which specifically states that it amends these Terms and Conditions and is signed by both parties and their duly authorized agents. It is further agreed that this contract shall not be modified in any respect except in writing signed by the party and their duly authorized agent against whom the modification is sought to be enforced.

AUTHORITY OF SELLER'S AGENTS

No agent, employee or representative of the SELLER has any authority to bind the SELLER to any affirmation, representation or warranty concerning the goods sold under this Contract, and unless an affirmation, representation or warranty made by an agent, employee, or representative is specifically included within this written contract, it shall not be enforceable by the BUYER.

NO THIRD-PARTY BENEFICIARIES

This contract is for the sole benefit of BUYER and SELLER and their respective successors and permitted assigns and nothing herein, express or implied, is intended to or shall confer upon any other person or entity any legal or equitable right, benefit, or remedy of any nature whatsoever under or by reason of these Terms and Conditions.

GOVERNING LAW

All matters arising of or relating to the contract or the Terms and Conditions shall be governed by and construed in accordance with the laws of the state in which the project is located.

Quotation, r4.2025

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DISPUTE RESOLUTION

In the event of any dispute between BUYER and SELLER arising out of the terms of the contract and these Terms and Conditions, such dispute shall be decided by arbitration administered by the American Arbitration Association in accordance with the then-prevailing Commercial Arbitration Rules and Mediation Procedures of the American Arbitration Association. BUYER and SELLER mutually agree that any dispute involving claims valued at or above \$1,000,000.00 shall be heard by a panel of three (3) arbitrators. The venue for all arbitration proceedings shall be the State of California. The foregoing agreement to arbitrate shall be specifically enforceable in any court of competent jurisdiction. The award rendered by the arbitrators shall be final and judgment may be entered upon it in accordance with applicable law in any court of competent jurisdiction.

SEVERABILITY

The partial or complete invalidity of any one or more provisions of these Terms and Conditions shall not affect the validity or continuing force and effect of any other provision. If any provision is invalid, in whole or in part, the provision shall be considered reformed to reflect the intent thereof to the greatest extent possible consistent with applicable law.

ASSIGNMENT – DELEGATION

No right or interest in this Contract shall be assigned by the BUYER without the written permission of the SELLER, and no delegation of any obligation owed, or of the performance of any obligation by the BUYER shall be made without the written permission of the SELLER. Any attempted assignment or delegation shall be wholly void and totally ineffective for all purposes unless made in conformity with this paragraph.

Please Remit to:
PO Box 735936
Chicago IL, 60673-5936
Online payment accepted at <http://www.lowapumpworks.com>
Pay Now 3% charge for credit card and \$2.25 charge for e-check
PAST DUE INVOICES ARE SUBJECT TO 1.5% PER MONTH FINANCE CHARGE

Quotation, r4.2025

Dan Fuhrman

From: Kyle K <KyleK@ericksonelectricmn.com>
Sent: Tuesday, May 5, 2026 7:45 AM
To: Dan Fuhrman
Subject: RE: Dayton

Dan

We can do this for \$1,500.00. Let me know if that works.

Kyle Klemek

Estimator/Project Manager

320-774-5613

kylek@ericksonelectricmn.com

www.ericksonelectricmn.com



ERICKSON ELECTRIC COMPANY, INC.
3308 SOUTHWAY DRIVE • SAINT CLOUD MN 56301-9513
PHONE 320.251.1501 • FAX 320.251.1504

Dan Fuhrman

From: Paul Meuwissen <paulmeuwissen@yahoo.com>
Sent: Wednesday, April 22, 2026 8:06 AM
To: Dan Fuhrman
Subject: Re: Dayton WTP - Control Box Relocate

Dan,

After reviewing the pictures and our phone conversation, the price to move the fan control box in Dayton will be \$1,500. The price will include labor and material to move the control box to the other side of the door.

Feel free to call me with any questions. 612-210-2223.

Thanks Paul
Sent from my iPhone

Councilmember Henderson April 28th Council Meeting Request Memo

Mayor and Council -

During the April 28th council meeting, Councilmember Henderson requested that staff provide a summary of information related to meals, employee appreciation, council meals, department budgets for meals, etc. Administrator Doud requested that an email be sent to ensure staff was providing the information that was requested in full order. Councilmember Henderson followed up with the following request via email:

A breakdown of amounts paid by each department for employee appreciation events, deposits for events, graduations, lunches, dinners, snacks, etc. Including food and supplies. I would like this from 2020 to present. If you have questions, please let me know.

The Results:

City staff dug into the request and worked to derive a solution that would work for the request with the best of our abilities and using the financial software to its greatest extent. The report that we felt was the most effective for this request ended up being an invoice register which is very similar to the one seen by Council each meeting for approval. This was run from January 1, 2020 until May 1, 2026. The following pages have all of that detail laid out for review if desired. A summary by Department by Year is below for consumption:

Department		Year							Total by Dept
		2020	2021	2022	2023	2024	2025	2026	
41110	Council	-	-	-	136.38	146.54	1,674.01	251.00	\$ 2,207.93
41310	Administration	-	-	44.94	326.01	22.40	227.07	10.41	\$ 630.83
41410	Clerk	-	-	150.27	-	-	-	-	\$ 150.27
41650	Recycling	-	-	-	-	-	556.21	-	\$ 556.21
41910	Activity Center	-	952.95	2,633.30	4,008.34	4,670.08	7,896.45	824.03	\$ 20,985.15
41950	Farmers Market	-	-	-	1,750.01	-	-	-	\$ 1,750.01
42120	Police	-	-	-	1,160.28	1,869.87	1,706.12	420.75	\$ 5,157.02
42260	Fire	-	-	-	2,153.12	9,672.04	2,944.15	218.65	\$ 14,987.96
43100	Public Works	-	-	184.12	220.09	138.67	210.77	470.89	\$ 1,224.54
45200	Parks	-	-	-	30.77	-	-	-	\$ 30.77
49999	Contingency	-	-	-	-	-	509.01	-	\$ 509.01
49400	Water/Sewer	-	-	-	-	-	58.46	-	\$ 58.46
Total by Year		\$ -	\$ 952.95	\$ 3,012.63	\$ 9,785.00	\$ 16,519.60	\$ 15,782.25	\$ 2,195.73	

One item to note from the above summary and detail on the subsequent pages is the descriptions were not great for past purchases up until probably 2023 when more specific descriptions were given on payments. This is context that matters as there could have been items of meals or purchases of food in the past for events that staff cannot accurately or effectively add to this list due to the amount of time needed to review each invoice that states “Supplies” as an example if that means actual supplies, program supplies, event supplies, food, or something else. While staff does not have concerns that items listed with generic descriptions

represent improper purchases, the data may appear skewed toward higher spending in recent years. This may in part be due to changes in staffing levels, operations, and Employee Appreciation Policies, the use of generic descriptions may also artificially exaggerate that data.

Another item to note, the items noted above on the summary and subsequent detail only include items through April that were entered into the Financial software. It is a snapshot in time so items entered into the Financial software in May are not included on the summary above which would include any purchases on the credit card in April or any invoices that were received in May from something that may have been received or ordered in April or prior.

Follow-Up Action:

As a result of reviewing these items, Administrator Doud and Assistant Administrator Benting have already begun work on creating a policy for meals, travel, etc. along with amending the employee appreciation program policy already in place. This will help ensure public confidence in the City's expenditures and promote consistency across the organization, reducing the risk of differing interpretations of the current directives. City Council should expect to see these policies in front of them within the next 4 meetings.

Please let me know if you have any questions,

Zach Doud, City Administrator

05/11/2026 INVOICE REGISTER REPORT FOR CITY OF DAYTON MN
 EXP CHECK RUN DATES 01/01/2020 - 05/01/2026
 BOTH JOURNALIZED AND UNJOURNALIZED
 PAID

Vendor	Inv Date	Due Date	Inv Amt	Status	Jrnalized	Year
Description	Entered By				Post Date	
GL Distribution	GL Distribution	Description				
	BRIAN MURPHY			Paid		
	MEAL				Y	
101-42120-50331	101-42120-50331	LODGING/MEALS/MILEAGE	25.75		10/08/2025	2025
	CITY OF ROGERS			Paid		
	FD; 2024 NW SUBURBAN FIRE ACADEMY				Y	
101-42260-50207	101-42260-50207	FD; 2024 NW SUBURBAN FIRE ACADEMY	200.00		07/25/2024	2024
	CITY OF ROGERS			Paid		
	2025 NW SUBURBAN FIRE ACADEMY GRAD.				Y	
101-42260-50208	101-42260-50208	2025 NW SUBURBAN FIRE ACADEMY GRAD.	240.00		06/25/2025	2025
	DAVID FASHANT			Paid		
	LMC CONFERENCE-DULUTH REIMBURSEMENT				Y	
101-41110-50208	101-41110-50208	PROFESSIONAL DEVELOPMENT;MEAL	15.70		06/25/2025	2025
	DAYTONA COUNTRY CLUB INC			Paid		
	FD; RECRUITMENT AND RETENTION				Y	
101-42260-50207	101-42260-50207	FD; RECRUITMENT AND RETENTION	2,025.00		10/24/2023	2023
	DEHNS COUNTRY MANOR			Paid		
	FD; RECRUITMENT AND RETENTION				Y	
101-42260-50207	101-42260-50207	FD; RECRUITMENT AND RETENTION	128.12		10/16/2023	2023
	J.P. MORGAN CHASE BANK NA			Paid		
	VISA ACCOUNT #2745 DEC 2024 STATEMENT				Y	
101-41910-50213	101-41910-50213	EMPLOYEE APPRECIATION PROGRAM	993.70		12/31/2024	2024
101-41910-50213	101-41910-50213	EMPLOYEE APPRECIATION PROGRAM	261.25		12/31/2024	2024
	J.P. MORGAN CHASE BANK NA			Paid		
	CC J.P MORGAN CHASE- JANUARY 2025				Y	
101-41310-50208	101-41310-50208	FOOD FOR PEER GROUP-JIMMY JOHN'S	14.50		01/31/2025	2025
101-43100-50210	101-43100-50210	MEAL AT CONFERENCE-MALONES	76.31		01/31/2025	2025
101-42120-50331	101-42120-50331	LODGING/MEALS/MILEAGE-CUB	49.87		01/31/2025	2025
101-42120-50331	101-42120-50331	LODGING/MEALS/MILEAGE-DAVANNI'S	225.70		01/31/2025	2025
	J.P. MORGAN CHASE BANK NA			Paid		
	food Council				Y	
101-41110-50210	101-41110-50210	food Council	38.36		02/11/2025	2025
	J.P. MORGAN CHASE BANK NA			Paid		
	Council food				Y	
101-41110-50210	101-41110-50210	Council food	207.47		02/11/2025	2025
	J.P. MORGAN CHASE BANK NA			Paid		
	food				Y	
101-41110-50208	101-41110-50208	food	33.98		02/25/2025	2025
	J.P. MORGAN CHASE BANK NA			Paid		
	EMPLOYEE BREAKFAST				Y	
101-41910-50213	101-41910-50213	EMPLOYEE BREAKFAST	642.63		02/07/2025	2025
	J.P. MORGAN CHASE BANK NA			Paid		
	CREDIT REFUND				Y	
101-41910-50213	101-41910-50213	CREDIT REFUND	(50.48)		02/11/2025	2025
	J.P. MORGAN CHASE BANK NA			Paid		
	Cookies for Department Meeting				Y	
101-42260-50207	101-42260-50207	Cookies for Department Meeting	25.96		02/20/2025	2025
	J.P. MORGAN CHASE BANK NA			Paid		
	PD-MEETING SNACKS				Y	
101-42120-50331	101-42120-50331	PD-MEETING SNACKS	11.34		02/12/2025	2025
	J.P. MORGAN CHASE BANK NA			Paid		
	Council food				Y	
101-41110-50210	101-41110-50210	Council food	216.06		03/11/2025	2025
	J.P. MORGAN CHASE BANK NA			Paid		
	Refreshments				Y	
101-42260-50207	101-42260-50207	Refreshments	27.95		03/12/2025	2025
	J.P. MORGAN CHASE BANK NA			Paid		
	Training Meal				Y	
101-42120-50331	101-42120-50331	Training Meal	47.00		03/17/2025	2025

	J.P. MORGAN CHASE BANK NA		04/19/2025	05/27/2025		Paid		
	Egg Hunt Volunteers Food		CHOYT				Y	
101-41910-50211	101-41910-50211	Egg Hunt Volunteers Food			27.60		04/19/2025	2025
	J.P. MORGAN CHASE BANK NA		04/19/2025	05/27/2025		Paid		
	Egg Hunt Volunteers Food		CHOYT				Y	
101-41910-50211	101-41910-50211	Egg Hunt Volunteers Food			16.52		04/19/2025	2025
	J.P. MORGAN CHASE BANK NA		04/23/2025	05/27/2025		Paid		
	Administrative Appreciation		CHOYT				Y	
101-41910-50213	101-41910-50213	Administrative Appreciation			29.15		04/23/2025	2025
	J.P. MORGAN CHASE BANK NA		04/08/2025	05/27/2025		Paid		
	Training Meal		CHOYT				Y	
101-42120-50208	101-42120-50208	Training Meal			29.43		04/08/2025	2025
	J.P. MORGAN CHASE BANK NA		04/09/2025	05/27/2025		Paid		
	Training Meal		CHOYT				Y	
101-42120-50208	101-42120-50208	Training Meal			60.66		04/09/2025	2025
	J.P. MORGAN CHASE BANK NA		04/09/2025	05/27/2025		Paid		
	Training Meal		CHOYT				Y	
101-42120-50208	101-42120-50208	Training Meal			41.00		04/09/2025	2025
	J.P. MORGAN CHASE BANK NA		04/09/2025	05/27/2025		Paid		
	Training Meal		CHOYT				Y	
101-42120-50208	101-42120-50208	Training Meal			35.14		04/09/2025	2025
	J.P. MORGAN CHASE BANK NA		04/10/2025	05/27/2025		Paid		
	Training Meal		CHOYT				Y	
101-42120-50208	101-42120-50208	Training Meal			47.41		04/10/2025	2025
	J.P. MORGAN CHASE BANK NA		04/08/2025	05/27/2025		Paid		
	Event Supplies		CHOYT				Y	
101-41910-50213	101-41910-50213	Event Supplies			103.12		04/08/2025	2025
	J.P. MORGAN CHASE BANK NA		04/16/2025	05/27/2025		Paid		
	Employee breakfast supplies		CHOYT				Y	
101-41910-50213	101-41910-50213	Employee breakfast supplies			168.23		04/16/2025	2025
	J.P. MORGAN CHASE BANK NA		04/30/2025	06/24/2025		Paid		
	Conference Lunch - Travel		CHOYT				Y	
101-41310-50208	101-41310-50208	Conference Lunch - Travel			13.43		04/30/2025	2025
	J.P. MORGAN CHASE BANK NA		05/02/2025	06/24/2025		Paid		
	Clean Up Day Food		CHOYT				Y	
101-41650-50388	101-41650-50388	Clean Up Day Food			525.24		05/02/2025	2025
	J.P. MORGAN CHASE BANK NA		05/03/2025	06/24/2025		Paid		
	Cleanup Day Drinks		CHOYT				Y	
101-41650-50388	101-41650-50388	Cleanup Day Drinks			30.97		05/03/2025	2025
	J.P. MORGAN CHASE BANK NA		05/09/2025	06/24/2025		Paid		
	Compass Group Lunch		CHOYT				Y	
101-41310-50200	101-41310-50200	Compass Group Lunch			9.61		05/09/2025	2025
	J.P. MORGAN CHASE BANK NA		05/16/2025	06/24/2025		Paid		
	Staff Appreciation		CHOYT				Y	
101-41910-50213	101-41910-50213	Staff Appreciation			52.05		05/16/2025	2025
	J.P. MORGAN CHASE BANK NA		05/19/2025	06/24/2025		Paid		
	Lunch Meeting		CHOYT				Y	
101-41310-50200	101-41310-50200	Lunch Meeting			25.71		05/19/2025	2025
	J.P. MORGAN CHASE BANK NA		05/21/2025	06/24/2025		Paid		
	Dinner Meeting		CHOYT				Y	
101-41310-50200	101-41310-50200	Dinner Meeting			15.56		05/21/2025	2025
	J.P. MORGAN CHASE BANK NA		05/13/2025	06/24/2025		Paid		
	Council food		CHOYT				Y	
101-41110-50210	101-41110-50210	Council food			76.61		05/13/2025	2025
	J.P. MORGAN CHASE BANK NA		05/03/2025	06/24/2025		Paid		
	STAFF BREAKFAST		CHOYT				Y	
101-43100-50210	101-43100-50210	STAFF BREAKFAST			134.46		05/03/2025	2025
	J.P. MORGAN CHASE BANK NA		05/16/2025	06/24/2025		Paid		
	Extrication Training Breakfast		CHOYT				Y	
101-49999-50429	101-49999-50429	Extrication Training Breakfast			172.39		05/16/2025	2025
	J.P. MORGAN CHASE BANK NA		05/16/2025	06/24/2025		Paid		
	Refreshments Extrication Training		CHOYT				Y	
101-49999-50429	101-49999-50429	Refreshments Extrication Training			35.41		05/16/2025	2025
	J.P. MORGAN CHASE BANK NA		05/16/2025	06/24/2025		Paid		
	Refreshments Extrication Training		CHOYT				Y	

101-49999-50429	101-49999-50429	Refreshments Extrication Training			6.27	05/16/2025	2025
	J.P. MORGAN CHASE BANK NA		05/18/2025	06/24/2025		Paid	
	Pizza Extrication Training		CHOYT				Y
101-49999-50429	101-49999-50429	Pizza Extrication Training			294.94	05/18/2025	2025
	J.P. MORGAN CHASE BANK NA		05/13/2025	06/24/2025		Paid	
	FOOD FOR OPEN HOUSE		CHOYT				Y
101-41910-50211	101-41910-50211	FOOD FOR OPEN HOUSE			840.36	05/13/2025	2025
	J.P. MORGAN CHASE BANK NA		05/13/2025	06/24/2025		Paid	
	FOOD FOR OPEN HOUSE		CHOYT				Y
101-41910-50211	101-41910-50211	FOOD FOR OPEN HOUSE			275.64	05/13/2025	2025
	J.P. MORGAN CHASE BANK NA		05/22/2025	06/24/2025		Paid	
	EVENT FOOD REFUND		CHOYT				Y
101-41910-50210	101-41910-50210	EVENT FOOD REFUND			(36.96)	05/22/2025	2025
	J.P. MORGAN CHASE BANK NA		06/10/2025	07/22/2025		Paid	
	Worksession Food		CHOYT				Y
101-41110-50210	101-41110-50210	Worksession Food			33.43	06/30/2025	2025
	J.P. MORGAN CHASE BANK NA		06/10/2025	07/22/2025		Paid	
	Worksession Food		CHOYT				Y
101-41110-50210	101-41110-50210	Worksession Food			226.55	06/30/2025	2025
	J.P. MORGAN CHASE BANK NA		06/10/2025	07/22/2025		Paid	
	Council Meal		CHOYT				Y
101-41110-50210	101-41110-50210	Council Meal			12.10	06/30/2025	2025
	J.P. MORGAN CHASE BANK NA		06/24/2025	07/22/2025		Paid	
	Council food		CHOYT				Y
101-41110-50210	101-41110-50210	Council food			120.53	06/30/2025	2025
	J.P. MORGAN CHASE BANK NA		06/05/2025	07/22/2025		Paid	
	AC; Get Moving May-Water Bottles		CHOYT				Y
101-41910-50213	101-41910-50213	AC; Get Moving May-Water Bottles			163.21	06/30/2025	2025
	J.P. MORGAN CHASE BANK NA		07/17/2025	08/26/2025		Paid	
	Officer Lunch Meeting		CHOYT				Y
101-42260-50207	101-42260-50207	Officer Lunch Meeting			60.00	07/17/2025	2025
	J.P. MORGAN CHASE BANK NA		07/03/2025	08/26/2025		Paid	
	New Officer Training Meal		CHOYT				Y
101-42120-50331	101-42120-50331	New Officer Training Meal			89.32	07/03/2025	2025
	J.P. MORGAN CHASE BANK NA		07/14/2025	08/26/2025		Paid	
	Training Meal		CHOYT				Y
101-42120-50331	101-42120-50331	Training Meal			75.85	07/14/2025	2025
	J.P. MORGAN CHASE BANK NA		07/17/2025	08/26/2025		Paid	
	PD BBQ Supplies		CHOYT				Y
101-42120-50331	101-42120-50331	PD BBQ Supplies			29.71	07/17/2025	2025
	J.P. MORGAN CHASE BANK NA		07/14/2025	08/26/2025		Paid	
	Refreshments		CHOYT				Y
101-42260-50207	101-42260-50207	Refreshments			8.57	07/14/2025	2025
	J.P. MORGAN CHASE BANK NA		08/26/2025	09/23/2025		Paid	
	Food for Worksession		KTHELEN				Y
101-41110-50210	101-41110-50210	Food for Worksession			329.92	08/26/2025	2025
	J.P. MORGAN CHASE BANK NA		08/26/2025	09/23/2025		Paid	
	Food for Worksession		KTHELEN				Y
101-41110-50210	101-41110-50210	Food for Worksession			39.64	08/26/2025	2025
	J.P. MORGAN CHASE BANK NA		08/05/2025	09/23/2025		Paid	
	Refreshments		KTHELEN				Y
101-42260-50207	101-42260-50207	Refreshments			205.43	08/05/2025	2025
	J.P. MORGAN CHASE BANK NA		08/12/2025	09/23/2025		Paid	
	Refreshments		KTHELEN				Y
101-42260-50207	101-42260-50207	Refreshments			54.82	08/12/2025	2025
	J.P. MORGAN CHASE BANK NA		08/20/2025	09/23/2025		Paid	
	Treats		KTHELEN				Y
101-42260-50200	101-42260-50200	Treats			68.64	08/20/2025	2025
	J.P. MORGAN CHASE BANK NA		08/25/2025	09/23/2025		Paid	
	Recognition Event		KTHELEN				Y
101-42260-50207	101-42260-50207	Recognition Event			1,500.00	08/25/2025	2025
	J.P. MORGAN CHASE BANK NA		08/13/2025	09/23/2025		Paid	
	Meals for Programs		KTHELEN				Y
101-41910-50211	101-41910-50211	Meals for Programs			52.02	08/13/2025	2025
	J.P. MORGAN CHASE BANK NA		09/11/2025	10/14/2025		Paid	

101-42120-50331	Department Meeting Food	DBRUNETTE				Y	
101-42120-50331	Department Meeting Food		407.59		09/11/2025		2025
	J.P. MORGAN CHASE BANK NA	09/11/2025		10/14/2025		Paid	
101-41910-50213	Employee Appreciation	DBRUNETTE				Y	
101-41910-50213	Employee Appreciation		83.15		09/11/2025		2025
	J.P. MORGAN CHASE BANK NA	09/18/2025		10/14/2025		Paid	
601-49400-50200	Water Long Term Plan Lunch	DBRUNETTE				Y	
601-49400-50200	Water Long Term Plan Lunch		29.23		09/18/2025		2025
	J.P. MORGAN CHASE BANK NA	09/18/2025		10/14/2025		Paid	
602-49400-50200	Sewer Long Term Plan Lunch	DBRUNETTE				Y	
602-49400-50200	Sewer Long Term Plan Lunch		29.23		09/18/2025		2025
	J.P. MORGAN CHASE BANK NA	09/18/2025		10/14/2025		Paid	
101-42120-50331	Heritage Days Food for Officers	DBRUNETTE				Y	
101-42120-50331	Heritage Days Food for Officers		156.69		09/18/2025		2025
	J.P. MORGAN CHASE BANK NA	09/18/2025		10/14/2025		Paid	
101-42120-50331	Meal Critical Incident Training	DBRUNETTE				Y	
101-42120-50331	Meal Critical Incident Training		53.20		09/18/2025		2025
	J.P. MORGAN CHASE BANK NA	09/04/2025		10/14/2025		Paid	
101-41910-50213	Top Golf, Employee Appreciation	DBRUNETTE				Y	
101-41910-50213	Top Golf		582.00		09/04/2025		2025
	J.P. MORGAN CHASE BANK NA	09/12/2025		10/14/2025		Paid	
101-41910-50213	Event Supplies	DBRUNETTE				Y	
101-41910-50213	Event Supplies		9.54		09/12/2025		2025
	J.P. MORGAN CHASE BANK NA	10/01/2025		11/25/2025		Paid	
101-42120-50208	Training Meal	CHOYT				Y	
101-42120-50208	Training Meal		41.81		10/01/2025		2025
	J.P. MORGAN CHASE BANK NA	10/01/2025		11/25/2025		Paid	
101-42120-50208	Training Meal	CHOYT				Y	
101-42120-50208	Training Meal		30.00		10/01/2025		2025
	J.P. MORGAN CHASE BANK NA	10/02/2025		11/25/2025		Paid	
101-42120-50208	Training Meal	CHOYT				Y	
101-42120-50208	Training Meal		30.00		10/02/2025		2025
	J.P. MORGAN CHASE BANK NA	10/22/2025		11/25/2025		Paid	
101-42260-50207	Lunch while out of town training	CHOYT				Y	
101-42260-50207	Lunch while out of town training		27.02		10/22/2025		2025
	J.P. MORGAN CHASE BANK NA	09/24/2025		11/25/2025		Paid	
101-42120-50331	EMR Training Meal	CHOYT				Y	
101-42120-50331	EMR Training Meal		218.65		09/24/2025		2025
	J.P. MORGAN CHASE BANK NA	11/13/2025		12/23/2025		Paid	
101-41310-50200	Meeting Meal	CHOYT				Y	
101-41310-50200	Meeting Meal		10.84		11/13/2025		2025
	J.P. MORGAN CHASE BANK NA	11/24/2025		12/23/2025		Paid	
101-41310-50200	Champlin Admin Meeting Meal	CHOYT				Y	
101-41310-50200	Champlin Admin Meeting Meal		19.79		11/24/2025		2025
	J.P. MORGAN CHASE BANK NA	11/25/2025		12/23/2025		Paid	
101-41310-50200	Lunch Meeting	CHOYT				Y	
101-41310-50200	Lunch Meeting		19.64		11/25/2025		2025
	J.P. MORGAN CHASE BANK NA	11/25/2025		12/23/2025		Paid	
101-41310-50200	Meeting Meal with Landform	CHOYT				Y	
101-41310-50200	Meeting Meal with Landform		48.10		11/25/2025		2025
	J.P. MORGAN CHASE BANK NA	11/26/2025		12/23/2025		Paid	
101-41910-50213	Employee Appreciation Donuts	CHOYT				Y	
101-41910-50213	Employee Appreciation Donuts		13.98		11/26/2025		2025
	J.P. MORGAN CHASE BANK NA	11/13/2025		12/23/2025		Paid	
101-41110-50210	Food for Legislature visit	CHOYT				Y	
101-41110-50210	Food for Legislature visit		55.31		11/13/2025		2025
	J.P. MORGAN CHASE BANK NA	11/17/2025		12/23/2025		Paid	
101-41110-50210	food for dayton visit	CHOYT				Y	
101-41110-50210	food for dayton visit		152.47		11/17/2025		2025
	J.P. MORGAN CHASE BANK NA	11/18/2025		12/23/2025		Paid	
101-41910-50213	Event Supplies	CHOYT				Y	
101-41910-50213	Event Supplies		68.22		11/18/2025		2025
	J.P. MORGAN CHASE BANK NA	11/18/2025		12/23/2025		Paid	
101-41910-50213	Event Supplies	CHOYT				Y	
101-41910-50213	Event Supplies		89.96		11/18/2025		2025

	J.P. MORGAN CHASE BANK NA		11/19/2025	12/23/2025		Paid		
	Program Supplies		CHOYT				Y	
101-41910-50213	101-41910-50213	Program Supplies			615.12		11/19/2025	2025
	J.P. MORGAN CHASE BANK NA		12/04/2025	01/27/2026		Paid		
	Meal for Employees - HoliDayton		CHOYT				Y	
101-41910-50218	101-41910-50218	Meal for Employees - HoliDayton			897.15		12/04/2025	2025
	J.P. MORGAN CHASE BANK NA		12/08/2025	01/27/2026		Paid		
	Meeting Meal (2)		CHOYT				Y	
101-41310-50200	101-41310-50200	Meeting Meal (2)			49.89		12/08/2025	2025
	J.P. MORGAN CHASE BANK NA		12/11/2025	01/27/2026		Paid		
	2025 Year End Recap Meeting		CHOYT				Y	
101-41910-50213	101-41910-50213	2025 Year End Recap Meeting			2,705.42		12/11/2025	2025
	J.P. MORGAN CHASE BANK NA		12/02/2025	01/27/2026		Paid		
	Deposit for End of Year Training Meal		CHOYT				Y	
101-42260-50207	101-42260-50207	Deposit for End of Year Training Meal			169.95		12/02/2025	2025
	J.P. MORGAN CHASE BANK NA		12/18/2025	01/27/2026		Paid		
	Drinks Plastic Forks Cookies		CHOYT				Y	
101-42260-50207	101-42260-50207	Drinks Plastic Forks Cookies			45.96		12/18/2025	2025
	J.P. MORGAN CHASE BANK NA		12/19/2025	01/27/2026		Paid		
	Remaining Balance for Dinner		CHOYT				Y	
101-42260-50207	101-42260-50207	Remaining Balance for Dinner			509.85		12/19/2025	2025
	J.P. MORGAN CHASE BANK NA		12/01/2025	01/27/2026		Paid		
	Event Supplies		CHOYT				Y	
101-41910-50213	101-41910-50213	Event Supplies			164.83		12/01/2025	2025
	J.P. MORGAN CHASE BANK NA		12/10/2025	01/27/2026		Paid		
	EMPLOYEE APPRECIATION		CHOYT				Y	
101-41910-50213	101-41910-50213	EMPLOYEE APPRECIATION			383.99		12/10/2025	2025
	J.P. MORGAN CHASE BANK NA		02/10/2026	04/07/2026		Paid		
	Training Meal		CHOYT				Y	
101-42120-50331	101-42120-50331	Training Meal			124.07		02/10/2026	2026
	J.P. MORGAN CHASE BANK NA		02/11/2026	04/07/2026		Paid		
	Training Meal		CHOYT				Y	
101-42120-50331	101-42120-50331	Training Meal			124.07		02/11/2026	2026
	J.P. MORGAN CHASE BANK NA		02/05/2026	04/07/2026		Paid		
	PW Q1 Meeting Food		CHOYT				Y	
101-43100-50210	101-43100-50210	PW Q1 Meeting Food			402.73		02/05/2026	2026
	J.P. MORGAN CHASE BANK NA		02/09/2026	04/07/2026		Paid		
	Q1 Staff Meeting Lunch		CHOYT				Y	
101-43100-50210	101-43100-50210	Q1 Staff Meeting Lunch			68.16		02/09/2026	2026
	J.P. MORGAN CHASE BANK NA		02/11/2026	04/07/2026		Paid		
	Council food		CHOYT				Y	
101-41110-50210	101-41110-50210	Council food			45.52		02/11/2026	2026
	J.P. MORGAN CHASE BANK NA		02/04/2026	04/07/2026		Paid		
	Jersey Mikes Interview Lunch		CHOYT				Y	
101-42260-50207	101-42260-50207	Jersey Mikes Interview Lunch			86.65		02/04/2026	2026
	J.P. MORGAN CHASE BANK NA		02/09/2026	04/07/2026		Paid		
	Training Meal		CHOYT				Y	
101-42120-50331	101-42120-50331	Training Meal			65.26		02/09/2026	2026
	J.P. MORGAN CHASE BANK NA		03/03/2026	04/28/2026		Paid		
	Dinner Meeting		CHOYT				Y	
101-41310-50200	101-41310-50200	Dinner Meeting			10.41		03/03/2026	2026
	J.P. MORGAN CHASE BANK NA		03/13/2026	04/28/2026		Paid		
	Donuts - Haydens Last Day		CHOYT				Y	
101-41910-50213	101-41910-50213	Donuts - Haydens Last Day			15.00		03/13/2026	2026
	J.P. MORGAN CHASE BANK NA		03/10/2026	04/28/2026		Paid		
	Council food		CHOYT				Y	
101-41110-50210	101-41110-50210	Council food			174.49		03/10/2026	2026
	J.P. MORGAN CHASE BANK NA		03/10/2026	04/28/2026		Paid		
	Council food		CHOYT				Y	
101-41110-50210	101-41110-50210	Council food			30.99		03/10/2026	2026
	J.P. MORGAN CHASE BANK NA		03/19/2026	04/28/2026		Paid		
	Officer Meeting Lunch		CHOYT				Y	
101-42260-50207	101-42260-50207	Officer Meeting Lunch			132.00		03/19/2026	2026
	J.P. MORGAN CHASE BANK NA		03/12/2026	04/28/2026		Paid		
	Food for Going Away for Hayden		CHOYT				Y	

101-42120-50331	101-42120-50331	Food for Going Away for Hayden			107.35	03/12/2026	2026
	J.P. MORGAN CHASE BANK NA		03/16/2026	04/28/2026		Paid	
	Employee breakfast supplies		CHOYT				Y
101-41910-50210	101-41910-50210	Employee breakfast supplies			24.53	03/16/2026	2026
	LYNDE'S RESTAURANT & CATERING		03/11/2026	03/24/2026		Paid	
	EMPLOYEE APPRECIATION PROGRAM APRIL 2021		CHOYT				Y
101-41910-50213	101-41910-50213	EMPLOYEE APPRECIATION PROGRAM			634.60	03/09/2026	2026
	LYNDE'S RESTAURANT & CATERING		04/15/2026	04/28/2026		Paid	
	EMPLOYEE APPRECIATION PROGRAM APRIL		CHOYT				Y
101-41910-50213	101-41910-50213	EMPLOYEE APPRECIATION PROGRAM			149.90	04/09/2026	2026
	SAMS CLUB INC		12/02/2024	12/10/2024		Paid	
	CH; OPERATING SUPPLIES		CHOYT				Y
101-41110-50210	101-41110-50210	COUNCIL MEETING DINNER			146.54	11/25/2024	2024
	SCOTT SALONEK		03/04/2025	03/11/2025		Paid	
	REIMBURSEMENT FOR COUNCIL MEAL 2/25/25		CHOYT				Y
101-41110-50210	101-41110-50210	REIMBURSEMENT FOR COUNCIL MEAL 2/25/25			115.88	02/25/2025	2025
	STERLING TROPHY INC		11/26/2024	12/10/2024		Paid	
	EMPLOYEE APPRECIATION PROGRAM-CHILE COOK		CHOYT				Y
101-41910-50213	101-41910-50213	EMPLOYEE APPRECIATION PROGRAM-CHILE COOK			40.00	11/26/2024	2024
	VISA-AC		10/01/2021	10/26/2021		Paid	
	ACTIVITY CTR VISA; SEPT 2021		sbrown				Y
101-41910-50213	101-41910-50213	EMPLOYEE APPRECIATION LUNCH			40.98	10/13/2021	2021
101-41910-50213	101-41910-50213	EMPLOYEE APPRECIATION LUNCH			29.99	10/13/2021	2021
101-41910-50213	101-41910-50213	EMPLOYEE APPRECIATION LUNCH			37.47	10/13/2021	2021
101-41910-50213	101-41910-50213	EMPLOYEE APPRECIATION LUNCH			52.18	10/13/2021	2021
101-41910-50213	101-41910-50213	EMPLOYEE APPRECIATION LUNCH			129.01	10/13/2021	2021
101-41910-50213	101-41910-50213	EMPLOYEE APPRECIATION LUNCH			110.59	10/13/2021	2021
101-41910-50213	101-41910-50213	EMPLOYEE APPRECIATION LUNCH			4.28	10/13/2021	2021
	VISA-AC		05/01/2022	05/24/2022		Paid	
	AC VISA; APRIL 2022		sbrown				Y
101-41910-50211	101-41910-50211	MEALS & FOOD FOR PROGRAMS			260.76	05/18/2022	2022
	VISA-AC		09/19/2022	09/27/2022		Paid	
	AC; VISA AUGUST 2022		DBRUNETTE				Y
101-41910-50213	101-41910-50213	EMPLOYEE EVENT			642.57	09/19/2022	2022
	VISA-AC		12/20/2022	12/27/2022		Paid	
	ACTIVITY CENTER VISA; NOV 2022		DBRUNETTE				Y
101-41910-50213	101-41910-50213	AC; UGLY SWEATER LUNCH			19.58	12/20/2022	2022
	VISA-AC		01/18/2023	01/24/2023		Paid	
	7144 ACTIVITY CTR VISA; DEC 2022		DBRUNETTE				Y
101-41910-50213	101-41910-50213	AC; UGLY SWEATER LUNCH			439.54	12/31/2022	2022
	VISA-AC		03/21/2023	03/28/2023		Paid	
	7144 VISA AC; FEB 2023		DBRUNETTE				Y
101-41910-50213	101-41910-50213	AC; DAVANNIS			155.38	03/21/2023	2023
	VISA-AC		05/25/2023	06/13/2023		Paid	
	7144 VISA AC; APRIL 2023		CHOYT				Y
101-41910-50211	101-41910-50211	AC; FOOD FOR PROGRAMS			56.22	05/25/2023	2023
101-41910-50213	101-41910-50213	AC; WAFFLE BREAKFAST			10.24	05/25/2023	2023
101-41910-50213	101-41910-50213	AC; WAFFLE BREAKFAST			218.17	05/25/2023	2023
101-41910-50211	101-41910-50211	AC; FOOD FOR PROGRAMS			107.77	05/25/2023	2023
101-41910-50213	101-41910-50213	AC; SNOW PLOW APPRECIATION			96.47	05/25/2023	2023
	VISA-AC		06/08/2023	06/27/2023		Paid	
	7144 VISA AC; MAY 2023		CHOYT				Y
101-41910-50213	101-41910-50213	AC; PD APPRECIATION WEEK			5.40	06/08/2023	2023
101-41910-50213	101-41910-50213	AC; PD APPRECIATION WEEK			250.00	06/08/2023	2023
101-41910-50211	101-41910-50211	AC; MEALS & FOOD FOR OPEN HOUSE			477.61	06/08/2023	2023
	VISA-CH		09/01/2021	09/28/2021		Paid	
	CH VISA; AUGUST 2021		sbrown				Y
101-41910-50213	101-41910-50213	EMPLOYEE RECOGNITION EVENT			60.00	09/13/2021	2021
	VISA-CH		10/01/2021	10/26/2021		Paid	
	CITY HALL VISA; SEPT 2021		sbrown				Y
101-41910-50213	101-41910-50213	EMPLOYEE APPRECIATION LUNCH			488.45	10/13/2021	2021
	VISA-CH		06/01/2022	06/28/2022		Paid	
	CH VISA; MAY 2022		sbrown				Y
101-41910-50213	101-41910-50213	PW/PD EMP APPR DAY			83.07	06/09/2022	2022
	VISA-CH		09/19/2022	09/27/2022		Paid	

	CH VISA; AUGUST 2022								Y	
101-41310-50200	101-41310-50200	INTERVIEW; DINNER				44.94			09/19/2022	2022
101-41410-50210	101-41410-50210	ELECTIONS; FOOD				32.71			09/19/2022	2022
101-41410-50210	101-41410-50210	ELECTIONS; FOOD				117.56			09/19/2022	2022
	VISA-CH		10/18/2022	10/25/2022				Paid		
	CH VISA; SEPT 2022								Y	
101-41910-50213	101-41910-50213	WHIRLYBALL; APPRECIATION EVENT				1,048.58			10/18/2022	2022
	VISA-CH		02/22/2023	02/28/2023				Paid		
	6773; CH VISA; JAN 2023								Y	
101-41110-50210	101-41110-50210	COUNCIL DINNER; JAN 2023				136.38			02/22/2023	2023
101-43100-50210	101-43100-50210	APPRECIATION PW; JAN 2023				182.39			02/22/2023	2023
	VISA-CH		08/08/2023	08/22/2023				Paid		
	6773; VISA CH-JULY 2023								Y	
101-41310-50200	101-41310-50200	CH; EMPLOYEE APPRECIATION				88.57			08/08/2023	2023
101-41950-50200	101-41950-50200	CH; MARKET BBQ FOOD TRUCK				1,750.01			08/08/2023	2023
	VISA-CH		05/14/2024	05/28/2024				Paid		
	5321 VISA CH; APRIL 2024								Y	
101-41910-50213	101-41910-50213	COOKIES FOR PD-SAM'S				19.98			04/30/2024	2024
	VISA-FD2		09/12/2023	09/26/2023				Paid		
	7284; VISA FD-AUGUST 2023								Y	
101-41910-50213	101-41910-50213	EMPLOYEE APPRECIATION PROGRAM				200.00			09/12/2023	2023
	VISA-FD2		10/11/2023	10/24/2023				Paid		
	7284 VISA FD; SEPTEMBER 2023								Y	
101-41910-50213	101-41910-50213	EMPLOYEE APPRECIATION PROGRAM				727.30			10/11/2023	2023
101-41910-50213	101-41910-50213	EMPLOYEE APPRECIATION PROGRAM				328.00			10/11/2023	2023
	VISA-FD2		04/12/2024	04/23/2024				Paid		
	5396 VISA FD; MARCH 2024								Y	
101-42260-50207	101-42260-50207	FD; RECRUITMENT AND RETENTION-GRADUATION				45.53			03/31/2024	2024
	VISA-FD2		06/11/2024	06/25/2024				Paid		
	5396 VISA FD; MAY 2024								Y	
101-42260-50207	101-42260-50207	FD; RECRUITMENT AND RETENTION-FIREHOUSE				174.42			05/31/2024	2024
101-42260-50208	101-42260-50208	FD; PROFESSIONAL DEVELOPMENT-DUNKIN				128.50			05/31/2024	2024
	VISA-FD2		08/13/2024	08/27/2024				Paid		
	5396 VISA FD; JUL 2024								Y	
101-42260-50207	101-42260-50207	FD; RECRUITMENT AND RETENTION-DEPOSIT				1,500.00			07/31/2024	2024
	VISA-FD2		09/11/2024	09/24/2024				Paid		
	5396 VISA FD; AUG 2024								Y	
101-42260-50207	101-42260-50207	FD; RECRUITMENT AND RETENTION-JERSEY MIK				178.69			08/31/2024	2024
101-42260-50207	101-42260-50207	FD; RECRUITMENT AND RETENTION-BIGBOREBBQ				165.00			08/31/2024	2024
101-42260-50207	101-42260-50207	FD; RECRUITMENT AND RETENTION-ETSY AWARD				227.74			08/31/2024	2024
101-42260-50207	101-42260-50207	FD; RECRUITMENT AND RETENTION-ETSY FFOTY				130.23			08/31/2024	2024
	VISA-FD2		09/30/2024	10/22/2024				Paid		
	5396 VISA FD; SEPT 2024								Y	
101-42260-50207	101-42260-50207	FD; RECRUITMENT AND RETENTION-AL ALMAS				5,706.27			09/30/2024	2024
101-42260-50207	101-42260-50207	FD; RECRUITMENT AND RETENTION-AL & ALMAS				528.89			09/30/2024	2024
101-42260-50207	101-42260-50207	FD; RECRUITMENT AND RETENTION-SUNDANCE				143.50			09/30/2024	2024
101-42260-50208	101-42260-50208	FD; PROFESSIONAL DEVELOPMENT-JERSEY MIKE				43.95			09/30/2024	2024
101-42260-50207	101-42260-50207	FD; RECRUITMENT AND RETENTION-BIG BORE				499.32			09/30/2024	2024
	VISA-PD		10/18/2022	10/25/2022				Paid		
	PD; VISA SEPT 2022								Y	
101-41910-50213	101-41910-50213	FLAMING BOWL; PD				91.20			10/18/2022	2022
101-41910-50213	101-41910-50213	FLAMING BLOW; PD				48.00			10/18/2022	2022
	VISA-PD		05/17/2023	05/23/2023				Paid		
	6005 VISA PD; APR 2023								Y	
101-42120-50331	101-42120-50331	LODGING/MEALS/MILEAGE				66.11			05/17/2023	2023
101-42120-50331	101-42120-50331	LODGING/MEALS/MILEAGE				31.86			05/17/2023	2023
101-42120-50331	101-42120-50331	LODGING/MEALS/MILEAGE				28.35			05/17/2023	2023
101-42120-50331	101-42120-50331	LODGING/MEALS/MILEAGE				31.34			05/17/2023	2023
101-42120-50331	101-42120-50331	LODGING/MEALS/MILEAGE				59.32			05/17/2023	2023
101-42120-50331	101-42120-50331	LODGING/MEALS/MILEAGE				44.67			05/17/2023	2023
101-42120-50331	101-42120-50331	LODGING/MEALS/MILEAGE				84.99			05/17/2023	2023
	VISA-PD		06/12/2023	06/27/2023				Paid		
	6005 VISA PD; MAY 2023								Y	
101-42120-50331	101-42120-50331	PD; LODGING/MEALS/MILEAGE				88.55			06/12/2023	2023
101-42120-50331	101-42120-50331	PD; LODGING/MEALS/MILEAGE				36.83			06/12/2023	2023

	VISA-PD		07/11/2023	07/25/2023		Paid		
	6005; VISA PD/JUNE 2023		CHOYT				Y	
101-42120-50331	101-42120-50331	PD; MEALS			90.64		07/11/2023	2023
101-42120-50331	101-42120-50331	PD; MEALS/TRAINING PUPS & DONUTS			63.47		07/11/2023	2023
101-42120-50331	101-42120-50331	PD; MEALS/PIZZA WITH A COP			43.19		07/11/2023	2023
101-42120-50331	101-42120-50331	PD; MEALS/PIZZA WITH A COP			42.96		07/11/2023	2023
	VISA-PD		08/08/2023	08/22/2023		Paid		
	6005; VISA PD- JULY 2023		CHOYT				Y	
101-42120-50331	101-42120-50331	PD; LODGING/MEALS/MILEAGE			52.40		08/08/2023	2023
101-42120-50331	101-42120-50331	LODGING/MEALS/MILEAGE			60.88		08/08/2023	2023
	VISA-PD		11/09/2023	11/28/2023		Paid		
	6005 VISA PD; OCTOBER 2023		CHOYT				Y	
101-42120-50331	101-42120-50331	PD; MEALS/BIG BORE			230.87		11/09/2023	2023
101-42120-50331	101-42120-50331	PD; MEALS/RJ RICHES			39.00		11/09/2023	2023
	VISA-PD		12/13/2023	12/22/2023		Paid		
	5081 VISA PD; NOVEMBER 2023		CHOYT				Y	
101-42120-50331	101-42120-50331	PD; LODGING/MEALS/MILEAGE			12.37		12/13/2023	2023
101-42120-50331	101-42120-50331	PD; LODGING/MEALS/MILEAGE			52.48		12/13/2023	2023
	VISA-PD		02/20/2024	02/27/2024		Paid		
	5081 VISA PD; JANUARY 2024		CHOYT				Y	
101-42120-50331	101-42120-50331	PD; LODGING/MEALS/MILEAGE			124.84		02/20/2024	2024
	VISA-PD		03/18/2024	03/26/2024		Paid		
	5081 VISA PD; FEBRUARY 2024		CHOYT				Y	
101-42120-50331	101-42120-50331	PD; LODGING/MEALS/MILEAGE			57.55		03/18/2024	2024
101-42120-50331	101-42120-50331	PD; LODGING/MEALS/MILEAGE			64.18		03/18/2024	2024
101-42120-50331	101-42120-50331	PD; LODGING/MEALS/MILEAGE-FAT NAT'S			67.94		03/18/2024	2024
101-42120-50331	101-42120-50331	PD; LODGING/MEALS/MILEAGE-JERSEY MIKES			194.77		03/18/2024	2024
	VISA-PD		04/11/2024	04/23/2024		Paid		
	5081 VISA PD; MARCH 2024		CHOYT				Y	
101-42120-50331	101-42120-50331	PD; LODGING/MEALS/MILEAGE-RECRUITMENT			58.27		03/31/2024	2024
101-42120-50331	101-42120-50331	PD; LODGING/MEALS/MILEAGE-RECRUITMENT			16.89		03/31/2024	2024
	VISA-PD		05/14/2024	05/28/2024		Paid		
	5081 VISA PD; APRIL 2024		CHOYT				Y	
101-42120-50208	101-42120-50208	PD; PROFESSIONAL DEVELOPMENT-FOOD			46.96		04/30/2024	2024
101-42120-50331	101-42120-50331	PD; LODGING/MEALS/MILEAGE-HYVVEE			140.80		04/30/2024	2024
101-42120-50331	101-42120-50331	PD; LODGING/MEALS/MILEAGE-HYVVEE			8.85		04/30/2024	2024
101-42120-50331	101-42120-50331	PD; LODGING/MEALS			27.27		04/30/2024	2024
101-42120-50331	101-42120-50331	PD; LODGING/MEALS			81.66		04/30/2024	2024
101-42120-50331	101-42120-50331	PD; LODGING/MEALS			45.09		04/30/2024	2024
	VISA-PD		07/11/2024	07/23/2024		Paid		
	5081 VISA PD; JUNE 2024		CHOYT				Y	
101-42120-50331	101-42120-50331	PD; LODGING/MEALS/MILEAGE-CULVERS			54.18		06/30/2024	2024
	VISA-PD		08/13/2024	08/27/2024		Paid		
	5081 VISA PD; JULY 2024		CHOYT				Y	
101-42120-50331	101-42120-50331	PD; MEALS-TRAINING /CHIPOLTE			26.86		07/31/2024	2024
	VISA-PD		10/01/2024	10/22/2024		Paid		
	5081 VISA PD; SEPTEMBER 2024		CHOYT				Y	
101-42120-50331	101-42120-50331	PD; LODGING/MEALS/MILEAGE			12.00		10/01/2024	2024
101-42120-50331	101-42120-50331	PD; LODGING/MEALS/MILEAGE			55.04		10/01/2024	2024
101-41910-50211	101-41910-50211	PD; MEALS & FOOD FOR PROGRAMS-FIREPOT			34.20		10/01/2024	2024
	VISA-PD		11/13/2024	11/26/2024		Paid		
	5081 VISA PD; OCTOBER 2024		CHOYT				Y	
101-42120-50331	101-42120-50331	PD; DAVANNI'S			210.02		10/31/2024	2024
101-42120-50331	101-42120-50331	PD; MEALS-BIG BORE			358.65		10/31/2024	2024
101-42120-50331	101-42120-50331	PD; LODGING/MEALS/MILEAGE-KWIK TRIP			6.49		10/31/2024	2024
101-42120-50331	101-42120-50331	PD; MEALS-RJ RICHES			32.05		10/31/2024	2024
101-41910-50213	101-41910-50213	PD; EMPLOYEE APPRECIATION PROGRAM-DAVANN			360.84		10/31/2024	2024
101-42120-50331	101-42120-50331	PD; MEALS- SNUFFY'S			43.38		10/31/2024	2024
101-42120-50331	101-42120-50331	PD; MEALS-DUKE'S			54.58		10/31/2024	2024
101-42120-50331	101-42120-50331	PD; MEALS-SNUFFY'S			49.02		10/31/2024	2024
101-42120-50331	101-42120-50331	PD; MEALS-HAM'S			32.53		10/31/2024	2024
	VISA-PD		12/16/2024	12/23/2024		Paid		
	5081 VISA PD; NOVEMBER 2024		CHOYT				Y	
101-41910-50213	101-41910-50213	EMPLOYEE APPRECIATION PROGRAM-STERLING			40.00		11/30/2024	2024
	VISA-PW		10/18/2022	10/25/2022		Paid		

	PW VISA; SEPT 2022		DBRUNETTE			Y	
101-43100-50210	101-43100-50210	PW; SEASONAL APPRECIATION			184.12	10/18/2022	2022
	VISA-PW		01/11/2024	01/23/2024		Paid	
	5016 VISA PW-DECEMBER 2023		CHOYT			Y	
101-41910-50213	101-41910-50213	AC; EMPLOYEE APPRECIATION PROGRAM			126.64	12/31/2023	2023
101-45200-50210	101-45200-50210	PW; OPERATING SUPPLIES-JIMMY JOHNS			30.77	12/31/2023	2023
101-41910-50213	101-41910-50213	AC; EMPLOYEE APPRECIATION PROGRAM			244.14	12/31/2023	2023
101-41910-50213	101-41910-50213	AC; EMPLOYEE APPRECIATION PROGRAM			112.00	12/31/2023	2023
101-41910-50213	101-41910-50213	AC; EMPLOYEE APPRECIATION PROGRAM			17.94	12/31/2023	2023
101-41910-50213	101-41910-50213	AC; EMPLOYEE APPRECIATION PROGRAM-FOOD			875.06	12/31/2023	2023
101-43100-50210	101-43100-50210	PW; OPERATING SUPPLIES ARBY'S			37.70	12/31/2023	2023
	VISA-PW		05/14/2024	05/28/2024		Paid	
	PW; PROFESSIONAL DEVELOPMENT-ARBORIST		CHOYT			Y	
101-41910-50213	101-41910-50213	EMPLOYEE APPRECIATION PROGRAM-BREAKFAST			19.42	04/30/2024	2024
101-41910-50213	101-41910-50213	EMPLOYEE APPRECIATION PROGRAM-BREAKFAST			185.48	04/30/2024	2024
101-41910-50213	101-41910-50213	EMPLOYEE APPRECIATION PROGRAM-BREAKFAST			559.65	04/30/2024	2024
	VISA-PW		06/10/2024	06/25/2024		Paid	
	7665 VISA PW; MAY 2024		CHOYT			Y	
101-41910-50210	101-41910-50210	PW; SUPPLIES-OPEN HOUSE MINI DONUTS			402.80	05/31/2024	2024
101-43100-50210	101-43100-50210	PW; SUPPLIES-DENNY'S			50.39	05/31/2024	2024
	VISA-PW		08/13/2024	08/27/2024		Paid	
	7665 VISA PW; JULY 2024		CHOYT			Y	
101-43100-50210	101-43100-50210	PW; CLIVE'S			88.28	07/31/2024	2024
	VISA-PW		09/18/2024	09/24/2024		Paid	
	7665 VISA PW; AUG 2024		KTHELEN			Y	
101-41910-50213	101-41910-50213	PW; EMPLOYEE APPRECIATION PRGM PINTS&PAD			956.52	09/02/2024	2024
	VISA-PW		11/13/2024	11/26/2024		Paid	
	7665 VISA PW; OCT 2024		CHOYT			Y	
101-41910-50213	101-41910-50213	EMPLOYEE APPRECIATION PROGRAM			796.24	10/31/2024	2024
	ZACH DOUD		03/21/2023	03/28/2023		Paid	
	REIMBURSEMENT; MILEAGE/RECEIPTS DOUD		D Brunette			Y	
101-41310-50320	101-41310-50320	LUNCH; ED, AMY, ZACH			75.71	03/21/2023	2023
	ZACH DOUD		10/02/2023	10/10/2023		Paid	
	MILEAGE/LUNCH REIMBURSEMENT; DOUD		CHOYT			Y	
101-41310-50200	101-41310-50200	FOOD REIMBURSEMENT; 06/29-09/29/2023			57.21	10/02/2023	2023
	ZACH DOUD		12/19/2023	12/22/2023		Paid	
	MILEAGE/LUNCH REIMBURSEMENT; DOUD		CHOYT			Y	
101-41310-50208	101-41310-50208	PROFESSIONAL DEVELOPMENT LUNCH			20.34	12/19/2023	2023
101-41310-50200	101-41310-50200	SUPPLIES; LUNCH MEETING			84.18	12/19/2023	2023
	ZACH DOUD		07/02/2024	07/09/2024		Paid	
	MILEAGE/LUNCH REIMBURSEMENT; DOUD APR-. CHOYT					Y	
101-41310-50200	101-41310-50200	FOOD REIMBURSEMENT; 4/24			22.40	06/30/2024	2024

48,248.16

ITEM:

Approval of Memorandum of Understanding

PREPARED BY:

Zach Doud, City Administrator

POLICY DECISION / ACTION TO BE CONSIDERED:

Approval of Memorandum of understanding

BACKGROUND:

While reviewing the AFSCME contract staff noticed a section on comp time that was agreed to be changed and a sentence was missed this is more of a grammatical error but wanted to make sure that the union contract is correct and employees are aware that comp time is not allowed to be carried over from year to year.

CRITICAL ISSUES:

RECOMMENDATION:

Approve the MOU

ATTACHMENT(S):

MOU for AFSCME

**MEMORANDUM OF UNDERSTANDING
BETWEEN THE CITY OF DAYTON AND
AMERICAN FEDERATION OF STATE,
COUNTY AND MUNICIPAL EMPLOYEES,
MINNESOTA COUNCIL 5, LOCAL 2454**

This Memorandum of Understanding ("MOU") is entered into between the City of Dayton, a Minnesota municipal corporation (hereinafter the "Employer" or "City") and American Federation of State, County, and Municipal Employees, Minnesota Council 5, Local 2454 (hereinafter the "Union" or "Employees"), who are part of a Labor Agreement and mutually desire to enter this Memorandum of Understanding.

WHEREAS, this Memorandum of understanding to update the contract language for comp time. ; and

WHEREAS, the Dayton City Council accepted the ASCME contract on 10-14-25

WHEREAS, the contract had stated that 40 hours of comp could remain in the banked hours; and

NOW, THEREFORE, the City ("Employer") and Union ("Employees") Agree that the Labor Agreement between the parties will be amended as follows: All overtime shall be taken at the choice of the employee as compensatory time off or in pay. An employee may accumulate eighty (80) hours of compensatory time in a bank. Employees have the option to cash out of compensatory time hours on a quarterly basis. Any remaining comp will be paid by the Employer in December of each calendar year.

Approved this _____ day of _____, 2025 by the City and the Union.

FOR THE UNION,

FOR THE CITY,

Paul Gammel Field Representative AFSCME Council 5

Dennis Fisher, Mayor

Gary Vendela, Field Director AFSCME Council 5

Zach Doud, City Administrator

Lynn Reichstadt, Local 2454

Ryan Eggink, Local 2454

PRESENTER: Amy Benting

ITEM: Approval of Election Timeframe Change for Early Voting

PREPARED BY: Amy Benting

POLICY DECISION / ACTION TO BE CONSIDERED: Approval of State Legislature Change for Early Voting for 18 days

BACKGROUND: Absentee Voting is 46 days prior to each election. Voters could fill out their ballot and put it into a series of envelopes to be fed through the ballot tabulator later. Direct Balloting (now Early Voting) is the 18 days prior to each election. Voters could fill out their ballot and feed it through the ballot tabulator similar to Election Day.

State Legislature changed rules and the governor signed HF4239 into law on May 18, 2026. Part of the law approves cities in Minnesota to decide whether to be open 46 days or the final 18 for early voting. This change is not for city, town, and school district elections not held on the same day as a statewide election.

The City of Dayton voter turnout for 2024 elections:

	Absentee	Direct
Presidential Nomination Party:	0	29
Primary:	15	48
General:	309	2094

Due to low turnout of voters during the Absentee Voting period, City staff would prefer to operate the 18-day Early Voting period moving forward. The timeframe change encourages work-life balance, less burnout, increase efficiency, reduce the potential for errors, and improve voter experience. Most voters prefer to put their ballot in the ballot tabulator themselves. This change would go into effect for the August 2026 Primary Election.

Voters have the options to vote by mail, in-person absentee voting at Hennepin County Government Center, Early Voting at City Hall during normal business hours (and state required dates), and on election day at their polling location. Absentee voters would be able to return their ballot to City Hall during the entire 46-day Absentee Voting period; the City would not be able to hand out a ballot during that time.

BUDGET IMPACT: Election judges will no longer be needed the full 46 days of Absentee Voting and only the final 18 days of Early Voting. Budget impacts for those 28 days are around \$8,000.

Supplies needed for Absentee Voting (ex. envelopes, labels, machines, computer usage, I Voted stickers, pens, paper, etc.) would decrease expenditure.

RECOMMENDATION: Approval of Election Timeframe Change for Early Voting

ATTACHMENT(S): None.

ITEM:

NW Water Tower

PREPARED BY:

Jason Quisberg, Engineering

POLICY DECISION / ACTION TO BE CONSIDERED:

Accept (Draft) NW Water Tower Preliminary Design Report

BACKGROUND:

It is known that NW Dayton, often referred to as the “Historic Village Area”, lacks water storage. With only a 2,000 gallon tank serving as storage for the system, flows to support fire suppression needs are unavailable. Additionally, the number of users connected is at the maximum recommended for a system with this limited volume of storage available.

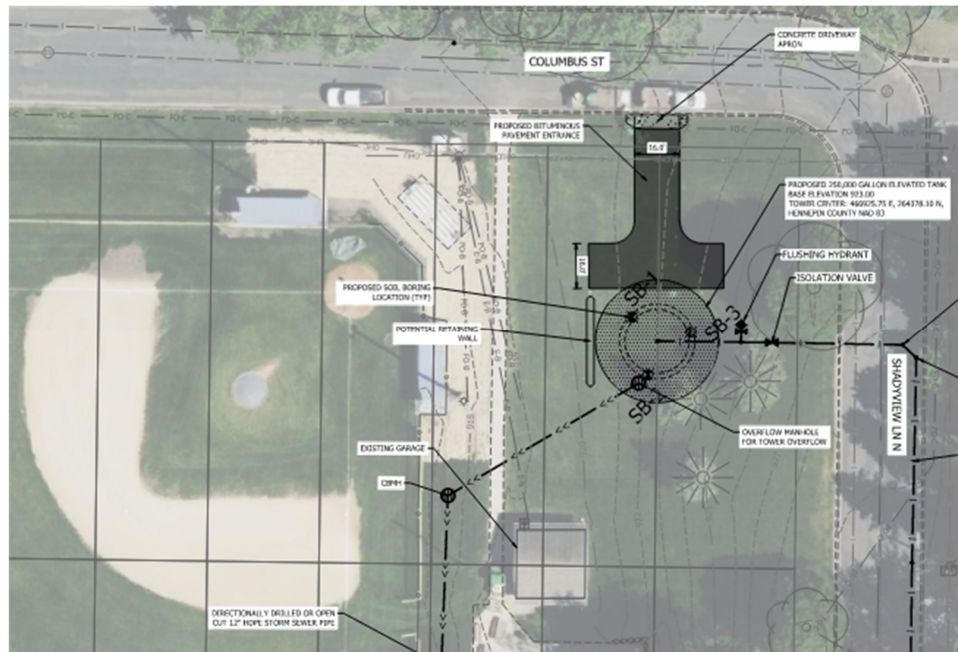
In September 2025, Council authorized preliminary engineering for a project involving the construction of elevated storage (water tower) to address the lack of storage concern. This work has been completed and a report of the findings is ready for review.

Summary of findings:

- Design tank size: 250,000 gal
- Tower type: Spheroid



- Location: McNeil Park (confirmed viable)



- Other improvements: Construct new/additional 12” main, between tower and source
 - Planned route: Shadyview Lane (directionally drilled)
- Anticipated project costs:

System Component	Total Length/ Height	Cost
12-inch Trunk Watermain	1,500 feet	\$830,000
0.25 MG Elevated Water Storage Tank	139 feet to HWL	\$2,835,000
Total		\$3,665,000

- Schedule:
 - Summer 2026 – Final Design
 - Fall 2026 – Bid/Start Construction
 - Summer/Fall 2028 – Complete construction (tower on-line)

The complete report is attached.

Other information/considerations:

- State bonding funds have been awarded for this project. It should be noted that any specific project contracting or construction requirements or conditions associated with that funding source may change the scope and/or cost of the project.
- The paint scheme, including any desired lettering or logo, for the tower needs to be determined. Note: Depending on the complexity of the scheme, the painting could add significant cost to the project. It is recommended this be figured out sooner rather than later.

Requested actions:

- Review the Preliminary Design Report
- Provide feedback/input regarding the project, including interest in paint scheme and logo
 - This can be done prior to (preferred), at, or after, the meeting
- Accept the report, with or without requested modifications

Next step: Final design. An engineering proposal for final design is planned for consideration at the June 9th (or June 23rd) Council meeting.

RECOMMENDATION:

Accept NW Dayton Water Tower Preliminary Design Report (with or without requested modifications)

.

ATTACHMENT(S):

NW Dayton Water Tower Preliminary Design Report

NW Dayton Water Tower

Preliminary Design Report

Date:

May 19, 2026

Prepared for:

City of Dayton, MN

Prepared by:

Stantec

Project/File:

193807550

DRAFT



Revision Record

Revision	Description	Author	Date	Quality Check	Date	Independent Review	Date

Disclaimer

The conclusions in the Report titled NW Dayton Water Tower are Stantec’s professional opinion, as of the time of the Report, and concerning the scope described in the Report. The opinions in the document are based on conditions and information existing at the time the scope of work was conducted and do not take into account any subsequent changes. The Report relates solely to the specific project for which Stantec was retained and the stated purpose for which the Report was prepared. The Report is not to be used or relied on for any variation or extension of the project, or for any other project or purpose, and any unauthorized use or reliance is at the recipient’s own risk.

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Prepared by:

Signature

Printed Name

Reviewed by:

Signature

Printed Name

Approved by:

Signature

Printed Name

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Appendix B Opinion of Probable Cost

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- B.2 Water Storage Tank



Executive Summary

This Preliminary Design Report evaluates the need, sizing, siting, and preliminary configuration for a new elevated water storage facility to serve Dayton's Historic Village service area (northwest Dayton). The existing system relies on two municipal wells, one treatment wellhouse, and a 3,750-gallon pressure tank that helps stabilize pressures but does not provide meaningful fire protection or emergency storage. As development continues in northwest Dayton, additional storage and associated transmission improvements are needed to improve system reliability, maintain operating pressures, and support future fire flow demands.

Based on historic demand data, projected ultimate build-out demands, and planning-level fire flow criteria, the report recommends construction of a 0.25 MG elevated water storage tank with a high-water level of 1,060 feet MSL. This size balances long-term storage needs with practical interim operations, avoiding the water quality and freezing concerns that can accompany oversized facilities under current low-demand conditions. Among the tank types reviewed, a spheroid tower is recommended as the most economical and maintainable option for the required storage volume.

Four location alternatives were considered, and McNeil Park is recommended as the preferred site because it offers suitable elevation, adequate space, good access, and is already owned by the City, eliminating the need for land acquisition. Supporting evaluations indicate the site is feasible from survey, geotechnical, and permitting perspectives. To connect the proposed tower to the existing system, a new 12-inch trunk watermain along Shadyview Lane is recommended as the preferred route because it is shorter, has fewer conflicts, and is expected to be more cost-effective than the Brockton Lane alternative.

At the preliminary design level, the estimated project cost is approximately \$3.665 million in 2026 dollars, including about \$830,000 for the trunk watermain and \$2.835 million for the elevated storage tank. The anticipated next phases include final design in summer 2026, bidding in fall 2026, and construction beginning in fall 2026, with completion projected in late 2027 or 2028 depending on procurement and construction sequencing.



1 Background

The Northwest area of Dayton, also known as the Historic Village, has operated a municipal water system since 2000. The current system includes two municipal supply wells, one wellhouse for chemical treatment including chlorine disinfection, fluoride, and sequestering agent addition, and one 3,750-gallon pressurized tank.

The existing pressure tank is not intended to provide fire flow storage and is too small to offer meaningful capacity for firefighting purposes. Instead, the tank serves primarily to:

- Maintain system pressures under normal operating conditions
- Reduce short-cycling of well pumps
- Provide a small buffer for managing pump start/stop sequences
- Assist in stabilizing system pressures during low-demand periods

The tank does not provide effective emergency or fire storage, and available fire flow capacity in the Historic Village is currently dependent on well pumping capacity.

As the area continues to grow and develop, additional water storage (water tower) is needed. In addition to a water tower, a new trunk watermain may be required to properly convey water from the supply wells to the proposed water storage location.



2 Water Storage Sizing Evaluation

Water storage sizing evaluation was previously conducted with the Historic Village Utility Water Plan (draft dated January 27, 2026). The following information is from that plan.

2.1 Historic and Projected Domestic Water Demands

2.1.1 Existing Water Demands

Historical well pumping records and residential and non-residential billing data from 2017 through 2024 were evaluated to establish a baseline understanding of typical water demands.

Water use in the Historic Village service area has remained relatively stable over the past eight years. This trend is consistent with the limited growth and development that has occurred in this portion of the City during the period of record.

Table summarizes historical residential use, irrigation use, total water delivered, and total water pumped for the Historic Village service area, along with calculated average day and maximum day demands.

Table 1. Historical Water Demands for Historic Village Service Area

Year	Residential Water Use (MG)	Irrigation Water Use ¹ (MG)	Total Water Delivered (MG)	Total Water Pumped (MG)	UFW (%)	Avg Day Demand (MGD)	Max Day Demand (MGD)	Max/Avg Ratio
2017	6.62	0.0	6.62	7.73	14.4%	0.021	0.046	2.17
2018	7.16	0.0	7.16	8.04	10.9%	0.022	0.045	2.04
2019	6.67	0.59	7.27	8.22	11.6%	0.023	0.042	1.87
2020	7.23	1.22	8.45	9.42	10.2%	0.026	0.056	2.17
2021	6.50	1.01	7.51	8.97	16.3%	0.025	0.055	2.24
2022	6.56	1.15	7.72	9.58	19.4%	0.026	0.069	2.63
2023 ²	6.56	1.34	7.90	9.47	16.6%	0.026	0.067	2.58
2024	7.08	0.53	7.61	7.43	-2.5%	0.020	0.061	3.00
Avg	6.80	0.97	7.53	8.61	12%	0.024	0.055	2.34
Max	7.23	1.34	8.45	9.58	19%	0.026	0.069	3.00

1. Irrigation data for McNeil Ballfield.

2. Well No. 1 pumpage for 2023 was corrected to exclude volumes bled to sanitary sewer during pressure tank repairs. Estimated bled volume was 4,400,000 gallons (Oct 28 – Nov 17).

For establishing design criteria for projected demands, a maximum day to average day ratio of 3.0 was selected based on the maximum observed peaking factor from the record period of 2017-2024. Although the odds of repeating the worst-case peaking factor in any given year are low, it is reasonable and conservative planning to expect that the highest peaking factor will be repeated in a future year.

The peak hour demand is the maximum hour of water demand which occurs on the maximum day. Peak hour demands typically occur in the morning or evening when residential and irrigation water use increases.



Peak hour ratios typically range from 1.4 to 2.0 times the maximum day demand. A higher percentage of industrial or commercial water use decreases the peak, since industrial usage does not fluctuate significantly from hour to hour. An industry standard time of day demand curve from the American Water Works Association suggests a peak hour ratio of 1.6. In lieu of basing the peak hour ratio on hourly pumping and water storage data recorded during a maximum day, it is recommended that a design peak hour ratio of 1.6 be used for projected demands.

2.1.2 Future Water Demands

Future water demands were projected using the land use designations identified in the Historic Village Framework Plan (August 2023) and the latest Water Comprehensive Plan (December 2025). These projections represent ultimate build-out conditions, which provide a long-range basis for sizing trunk water mains, evaluating available fire flow capacity, and identifying future supply and storage needs.

Water demands were estimated by applying per-acre demand factors to each land use category. These factors reflect typical average day demand (ADD), maximum day demand (MDD), and peak hour demand (PHD) water use patterns. Developable acreage was calculated by removing areas that are not suitable for development, including wetlands, steep slopes, right-of-way (ROW), and open water.

Assumptions Used:

- Land use categories and developable acres are based on the Historic Village Framework Plan, dated August 2023.
 - Note: the 2040 Future Land Use Map assumes the Historic Village area is mostly mixed use, which is categorized as a high density residential use in the plan. The Framework plan assumes a mix of low density, medium density, commercial, and recreational/public uses.
- Average day water demand factors are applied on a per-acre basis according to land use category. The following maximum peaking factors are then used:
 - A Maximum Day to Average Day peaking factor of 3.0 is used, based on the highest observed peaking factor from 2017 through 2024.
 - A design Peak Hour ratio of 1.6 is applied to the MDD, based on the AWWA standard time-of-day demand curve. This is also consistent with the City's 2025 Comprehensive Water Plan.

Table shows the developable acres for the ultimate Historic Village service area. Developable acreage excludes ROW, wetlands, steep slopes, and open water. This table also summarizes the average day demand rates from the Water Comprehensive Plan for each applicable future land use type.

Table 2. Developable Acres by Future Land Use and Average Day Demand Factors

Future Land Use	Acres	Avg Day Demands (gal/acre/day)
Low Density Residential (LDR)	84.5	750



Medium Density Residential (MDR)	29.1	1,350
Commercial	6.7	1,500
Industrial	0.5	1,500
Public/Institutional	4.2	1,000
Park/Open Space	14.4	1,000
Total	139.4	

Using the land use assumptions, developable acreages, and demand factors described above, total projected water demands were calculated for the Historic Village at full redevelopment based on the Framework Plan. These projections represent long-term planning values and support evaluation of trunk main capacity, fire flow capability, and future supply and storage requirements. Table represents the projected ultimate water demands for the Historic Village.

Table 3. Projected Ultimate Water Demands – Historic Village Framework Plan

Average Daily Demand	Maximum Daily Demand ¹	Peak Hour Demand ²
0.13 MGD (92 gpm)	0.40 MGD (275 gpm)	0.63 MGD (440 gpm)

¹ Maximum day demand design factor equal to 3.0.

² Peak hour demand design factor equal to 1.6.

2.2 Water Storage Sizing

The City’s 2025 Comprehensive Water Plan evaluated storage needs for each pressure zone using several industry-accepted methods, including fire/emergency storage, operational storage, and equalizing (peak hour) storage. For the Historic Village service area, the Comprehensive Plan determined that storage requirements are driven primarily by fire flow and operational needs, due to the small size of the service area and the absence of existing elevated storage.

The Comprehensive Water Plan recommended a total storage of 0.25 MG for the Historic Village service area. The existing effective storage capacity in this area is 0 MG, as the system currently relies solely on a 3,750-gallon pressure tank, which does not provide effective storage for fire protection.

Fire storage requirements were evaluated assuming a required fire flow of 1,500 gallons per minute (gpm) for a duration of 3 hours, consistent with planning-level fire protection criteria for residential development. Storage requirements were evaluated under firm capacity conditions, accounting for the ability of available wells to contribute to fire flow during an event.

Two firm supply scenarios were considered:

- 300 gpm firm supply, assuming Well No. 1 is available to provide up to 300 gpm during a fire event.
- 600 gpm firm supply, assuming Well No. 3 is available to provide up to 600 gpm during a fire event.

Under each scenario, the remaining fire flow demand must be supplied from storage. In addition to fire storage, equalizing storage must be provided. Equalizing storage was evaluated using two methodologies:



1. 20 percent of Maximum Day Demand (MDD), an industry standard rule-of-thumb.
2. Average Day Demand (ADD), consistent with Recommended Standards for Water Works.

The projected ultimate demands used for this evaluation include an MDD of 0.40 MGD and an ADD of 0.13 MGD. The resulting storage recommendations are summarized in Table .

Table 4. Water Storage Recommendations

Methodology	Recommended Storage Capacity with 300 gpm Firm Supply	Recommended Storage Capacity with 600 gpm Firm Supply
20% of MDD plus fire storage	0.30 MG	0.24 MG
ADD plus fire storage	0.35 MG	0.29 MG

The storage volumes presented above represent planning-level requirements at ultimate build-out of the Historic Village service area. However, consideration must also be given to operational concerns associated with oversizing storage facilities during current and interim conditions. Oversized storage tanks can experience challenges during winter operation, including increased risk of freezing, water quality degradation due to excessive detention times, and inefficient system operation under low-demand conditions. These concerns are particularly relevant for the Historic Village service area, where demands are expected to remain relatively low until full build-out of the Future Framework Plan is achieved.

Considering projected demands, fire protection requirements, interim operating conditions, and long-term system flexibility, a 0.25 MG storage tank is recommended for the Historic Village service area. The recommended tank should be constructed with a High Water Level (HWL) of 1,060 feet MSL, consistent with the HWL of the existing Northeast Water Tower, to maintain compatible system pressures and operational consistency once the two systems are merged, as described in the 2025 Comprehensive Water Plan.

2.3 Evaluation of Water Storage Types

2.3.1 Spheroid

A spheroid water tower (also known as a watersheroid or pedestal spheroid) consists of a steel spheroidal (flattened sphere) tank supported on a single steel pedestal shaft. The spheroidal geometry allows the tank wall thickness to vary with hydrostatic pressure, improving structural efficiency. Access piping, overflow, and interior ladders are typically routed through the pedestal. This configuration is one of the most common elevated tank styles. Spheroids are generally used for water towers between 150,000 and 500,000 gallons in size.

Spheroid tanks perform well structurally and economically for medium to large municipal systems. The efficient shell geometry minimizes steel tonnage and long-term stress concentrations. Maintenance is relatively straightforward since the structure is entirely steel and coatings are uniform across the tank and



pedestal. Spheroids also offer a smaller footprint than multi-leg or composite tanks, making them suitable for constrained sites.

From an operational standpoint, spheroids provide good turnover and mixing when properly designed, reducing water quality issues.

2.3.2 Sphere

A spherical water tower consists of a near-perfect steel sphere mounted on a single column or short pedestal. The true spherical shape distributes internal water pressure uniformly in all directions, making it theoretically the most efficient pressure vessel geometry. Spherical tanks are less common today due to fabrication complexity. Spheres are generally used for water towers between 25,000 to 150,000 gallons in size.

From a structural mechanics standpoint, the spherical tank is highly efficient; however, the **constructability and cost** typically outweigh these benefits for municipal water storage. Fabrication requires complex plate forming and welding, increasing capital cost relative to spheroidal alternatives. Operationally, spheres function similarly to spheroids, with good pressure performance and relatively uniform stress distribution that can translate into long service life if well maintained.

2.3.3 Hydro-pillar

The hydro-pillar is a large-diameter tank with a steel fluted column that is the support structure of the water storage volume circular tank on top. Internally, the column may include multiple floors or compartments for operations, storage, or utilities. Hydro-pillars are not commonly used due to high construction/materials costs and overall repainting costs due to the large surface area of steel needed. Hydro-pillars are generally used for water towers between 500,000 and 2,000,000 gallons in size.

The bowl configuration results in less efficient water turnover and can lead to water quality challenges. Structurally, these tanks are heavy and impose significant foundation loads. Maintenance is also more demanding due to interior compartments and complex structural details.

2.3.4 Composite

A composite elevated tank consists of a steel water tank supported on a reinforced concrete pedestal. The concrete shaft houses access stairs, piping, and in some cases auxiliary space, while the steel tank sits atop the concrete structure. Composite tanks are commonly used where tall storage heights or architectural treatments are desired. Composite tanks are typically used for water towers between 500,000 and 3,000,000 gallons in size.

Composite tanks excel in applications requiring greater heights, enhanced stability, or architectural integration. The concrete pedestal provides excellent stiffness against wind and seismic loads and allows for architectural finishes that blend with surrounding development. The Composite tanks also have less steel construction and require less maintenance regarding rehabilitations of the coatings of the steel



surfaces. Like the Hydro-pillar, these tanks are very heavy structurally and impose significant foundation loads.

2.3.5 Recommendation

The spheroid water tank is recommended for this site as it is most economical for construction and maintenance for the tank size needed.



3 Location Alternatives Analysis

Four sites were identified as a potential location for the water tower: 1) McNeil Park; 2) St. John the Baptist Church Cemetery; 3) the South Parking lot of St. John the Baptist Church; and 4) Well No. 1 Site.

3.1 McNeil Park

PID: 3112122310041, 31121223100040 and 3112122310039

Elevation: 923±

Ownership: City of Dayton

Wetlands: Not present

Tower would be located east of the existing baseball field. The tower needs approximately 100 feet by 100 feet which this site provides. There are trees in the immediate vicinity of the proposed location that may need to be removed or trimmed. This site is recommended for the proposed water tower due to location, elevation, accessibility and that the City currently owns the property and no land acquisition is required.

3.2 St. John the Baptist Church Cemetery

PID: 3112122310058

Elevation: 930±

Ownership: St. John the Baptist Church

Wetlands: Not evaluated

Tower would be located in the north side of the parcel, approximately 400 feet east of Division St. The tower needs approximately 100 feet by 100 feet which this site provides. There are trees in the vicinity of the proposed location that may need to be removed. The Church was contacted to verify their potential interest in providing an easement for a water tower, however, they indicated they were not interested in providing an easement. This site is not recommended for the water tower.

3.3 South Parking Lot for St. John the Baptist Church

PID: 3112122310050

Elevation: 908±

Ownership: St. John the Baptist Church

Wetlands: Not evaluated.



Tower would be located near the center of the parcel, approximately 80 feet west of the baseball field fence line. The tower needs approximately 100 feet by 100 feet which this site provides (with overall into McNeil Park). The tower would displace existing vehicle parking, so new parking would need to be constructed in the adjacent west parcel. The Church was contacted to verify their potential interest in providing an easement for a water tower, however, they indicated they were not interested in providing an easement. This site is not recommended for the water tower.

3.4 Well No. 1 Site

PID: 0612022210002

Elevation: 896±

Ownership: John Bernens Rev Trust Et Al (0612022210002)

Wetlands: Not present within the tower location, however, there is a delineated wetland approximately 100 feet west of the well building.

Tower would be located east of the well building on the north side of the parcel. This location would require an easement. The tower needs approximately 100 feet by 100 feet which this site provides. A water tower close to a supply well is not recommended because it is not centrally located to the service area. This site is also the lowest elevation of the four sites which would require the tallest water tower pedestal which increases cost exponentially at this height. This site is not recommended for the water tower.



4 Evaluation of Selected Site

4.1 Preliminary Site Survey

A site survey was performed at the McNeil Park site (PIDs 3112122310041, 31121223100040 and 3112122310039) in November 2025. Slopes in the tower area generally range from 5 percent to 12 percent, with drainage generally flowing from east to west. There are four trees in the immediate area that may require trimming and/or removal. A concrete sidewalk runs north/south to an existing garage located east of the baseball field and south of the proposed water tower location. An irrigation system is located in the general area of the water tower, however, exact location of the irrigation heads and water lines is unknown at this time. There is a buried electrical line running from a power pole near the backstop to the existing garage. A playground is located south of the garage and water tower location.

4.2 Geotechnical Evaluation

American Engineering Testing (AET) prepared a Report of Geotechnical Exploration, dated April 10, 2026. AET collected two standard penetration test borings to depths of 30 feet and one standard penetration test boring to a depth of 50 feet. The report indicates the water tower can be supported upon a ring foundation as planned.

4.3 FAA Permitting Requirements

The water tower location was reviewed with the FAA's Obstruction Evaluation/Airport Airspace Analysis (OE/AAA) pre-screening tool. The nearest airports are the Crystal Airport (MIC) and the Anoka County-Blaine Airport (ANE). Based on this location, the pre-screening tool indicates the project is not required to file notice with the FAA, however, basic lighting at the top of the tower is recommended (required for structures 200 feet above ground level). As a precaution and engineering due diligence, we have submitted the FAA notification of the tower for their information and review. A response is pending but all indications are that the McNeil Park site location will not have an impact on airspace travel.

4.4 Site Easement and Acquisition Needs

The recommended site is within City property, no easement acquisition is required.

4.5 Recommended Watermain Improvements

A 12-inch watermain is recommended along Shadyview Lane (north) to connect the well supply to the proposed storage tower. This transmission main will provide a reliable, high-capacity connection between supply and storage facilities, improve operational flexibility, and support future increases in pumping rates under firm capacity conditions.



5 Preliminary Design

5.1 Water Tower

The recommended water tower preliminary design includes the construction of a spheroid tank. The finish ground elevation would be around 923.00 for the 32-foot diameter ring-wall that would sit on a spread footing that would spread the load on the existing soil base for combating settlement and support the steel tank. The spread footing diameter will be determined during the final design phase.

The 43-foot to 44-foot diameter spheroid tank would be supported by a 10-foot diameter column with a 31-foot operating range. The high-water level of the tank would be set at 1060 ft MSL to match the maximum zone water level. This will create a tower height to the high-water level of 137 feet with a total overall height of the tower of approximately 144 feet to its highest point of the dual LED obstruction lights. An 8-inch to 10-inch overflow pipe would send tank overflow to an overflow manhole and 12-inch storm sewer system to the southwest to an existing storm sewer draining to the west on the south side of the ball field. A watermain connection from the east with an isolation hydrant for tank flushing and water testing will connect the new tower to the existing water system. A mixer would also be included in the tower to reduce tank freezing during winter months and to improve water quality in the summer months.

Refer to drawings in the appendix for preliminary design of the proposed site plan and the tower elevation. These drawings will be the basis for the final design of the water tower configuration and layout and sizing. A final City Logo/Name sign will need to be determined soon to maintain the project schedule. Tank coloring and logo will be determined during the final design phase also.

5.2 Trunk Watermain

Two watermain routes were evaluated between Well No. 1 and the proposed water tower location.

5.2.1 Shadyview Lane Route

This route would connect to the existing 12-inch watermain east of Well No. 1 south of Shadyview Lane and run north along Shadyview Lane to the water tower site at McNeil Park. This route includes potential conflicts with existing sanitary sewer and watermain along Shadyview. To avoid those conflicts it is proposed that the watermain generally run deep. This route is recommended as it is a shorter route with less crossings/conflicts and lower construction costs.

5.2.2 Brockton Lane Route

This route would connect to the existing 12-inch watermain near Well No. 1, run west to Brockton Lane, then north along the east side of Brockton Lane to the intersection with Dayton River Road. The route would continue east across Dayton River Road, then north along the east side of Dayton River Road to the southwest corner of McNeil Park, then east through the park to the water tower site. This route includes several potential conflicts and crossings with high-pressure gas main as well as several other private



utilities. This route is not recommended due to its crossings with high-pressure gas and higher construction cost.

5.3 Modifications to Existing Well Pumps

The Historic Village service area is supplied by two municipal groundwater wells, Well No. 1 and Well No. 3, both completed in the Tunnel City–Wonewoc aquifer. Both wells are equipped with submersible pumps and variable frequency drives (VFDs). It is understood that currently, each pump is programmed to pump to the pressure tank at a flow setpoint of 150 gpm. This is intended to attenuate pressure fluctuations, reduce pump start/stop cycles, and lengthen pump run times. However, the installed pump ratings are higher, at approximately 300 gpm for Well No. 1 and 600 gpm for Well No. 3. With the construction of a larger storage tank, the pumps can be reprogrammed to run at their design flow ratings.

The preliminary design was reviewed from a hydraulic standpoint to determine whether any modifications would be needed to the existing well pumps. Based on documented pumping water levels, a tower high water level of 1060 ft MSL, and estimated friction and minor headlosses in piping between the well pumps and the tower, no modifications (e.g. de-staging) or replacement of the pumps are expected. As the design progresses, it is recommended that the speed curves for each pump be reviewed against the proposed future conditions.

6 Opinion of Probable Costs

The estimated costs for the proposed water storage improvements are detailed in Table . Costs are shown in 2026 dollars. Contingency, administration, legal, and engineering costs are included; however, land acquisition costs are not included. Costs for painting beyond a basic name sign are not included. See Appendix B for more information.

Table 5. Opinion of Probable Construction Cost

System Component	Total Length/ Height	Cost
12-inch Trunk Watermain	1,500 feet	\$830,000
0.25 MG Elevated Water Storage Tank	139 feet to HWL	\$2,835,000
Total		\$3,665,000

7 Tank Painting Options

There are several painting schemes done on water towers. Many go with an off-white color or light blue or even tan for the entire tank. However, many cities have gone with darker colors of the tank to hide mildew



NW Dayton Water Tower 7 Tank Painting Options

buildup that happens over time and requires tank cleaning to be performed to keep that new tank appearance. In the past, cities steered away from the darker colors because they tended to fade quicker than the lighter colors. However, with the new paint products, fading does not happen at the rate they did in the past and they also retain their shine and new appearance better than the paints used a decade or two ago. The new paint systems also help shed the condensation build up that can develop around the wet areas of the tank that then produce mildew.

The City will want to begin the process of deciding on the tank painting scheme soon. Potential options include 1) matching the name sign painting scheme on the 500,000-gallon composite tank from 2006 (see Figure 1); 2) include the current city logo (see Figures 2 and 3); 3) a more elaborate painting scheme/logo.



Figure 1. Name sign on existing 500,000-gallon composite tank



Figure 2. City Logo option



Figure 3. City Logo option

If the City chooses to go with a logo similar to the above figures, size will need to be determined. To see at a distance, it is recommended that the lettering be at a minimum 1 foot to 1 ½ feet in width. Smaller lettering widths are hard to see at a distance and being able to see the “CITY OF” or the “LIVE THE DIFFERENCE” may be hard to see at any distance except very near to the tank. A painting scheme option



from Figure 3 could include the “D” and the blue and green waves at one location and the “Dayton” name sign at another. If the City wishes to do something more elaborate, we recommend reviewing the Tnemec Tank of the Year Winners for potential options ([Tnemec - Commercial and Industrial Paint Coatings - Tnemec Company, Inc.](#)). Please keep in mind, as colors are added and as the logo/paint scheme gets more complex, the costs will increase.

If the City does want to copy the color used on the 500,000 gallon tower, we would also recommend that the lower part of the pedestal spheroid be painted to match the darker blue color of the Dayton logo. Many tanks painting schemes are now doing this to match the logo color so it ties in nicely and the dark color on the bottom of the bowl will help hide that eventual mildew build up on the bottom of the tower where condensation builds up. This will help keep the tank looking like new and reduce the amount of exterior cleaning the City may want to do.

8 Schedule

The schedule for design and construction of the water tower is anticipated to be as follows:

- Preliminary Design: Fall 2025 to Spring 2026
- Final Design: Summer 2026
- Bidding: Fall 2026
- Start Construction: Fall 2026
- Finish Construction: Fall of 2027 or the Summer/Fall of 2028
 - Finish Construction in the Fall of 2027 can add to the construction cost of the tank by trying to compact all the work into a tight schedule. This will require the tower foundation work to be done over the winter of 2026/2027 and the tower erection immediately after so that there will be time to do the tank painting in the fall. Any issues to the schedule like weather can also delay a project beyond this schedule and thus, tower erectors will add cost for this added risk. If the City were to allow the tank to be completed Summer or Fall of 2028, the bids for the tank may be more favorable because the Contractor will not have the risk of the tight schedule and can fit the work better into their most likely busy schedule.

*Note: Trunk watermain final design and bidding may occur at a later date with the construction to be scheduled for completion around the same time as the water tower.

If getting the system online in 2027 versus 2028 is of high importance that is how the contract should be prepared. However, if there is flexibility in the date the tower is put online, a 2028 completion date is recommended with an alternate for an accelerated schedule for completion in 2027.

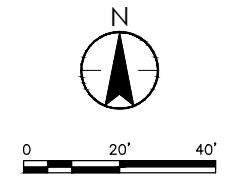
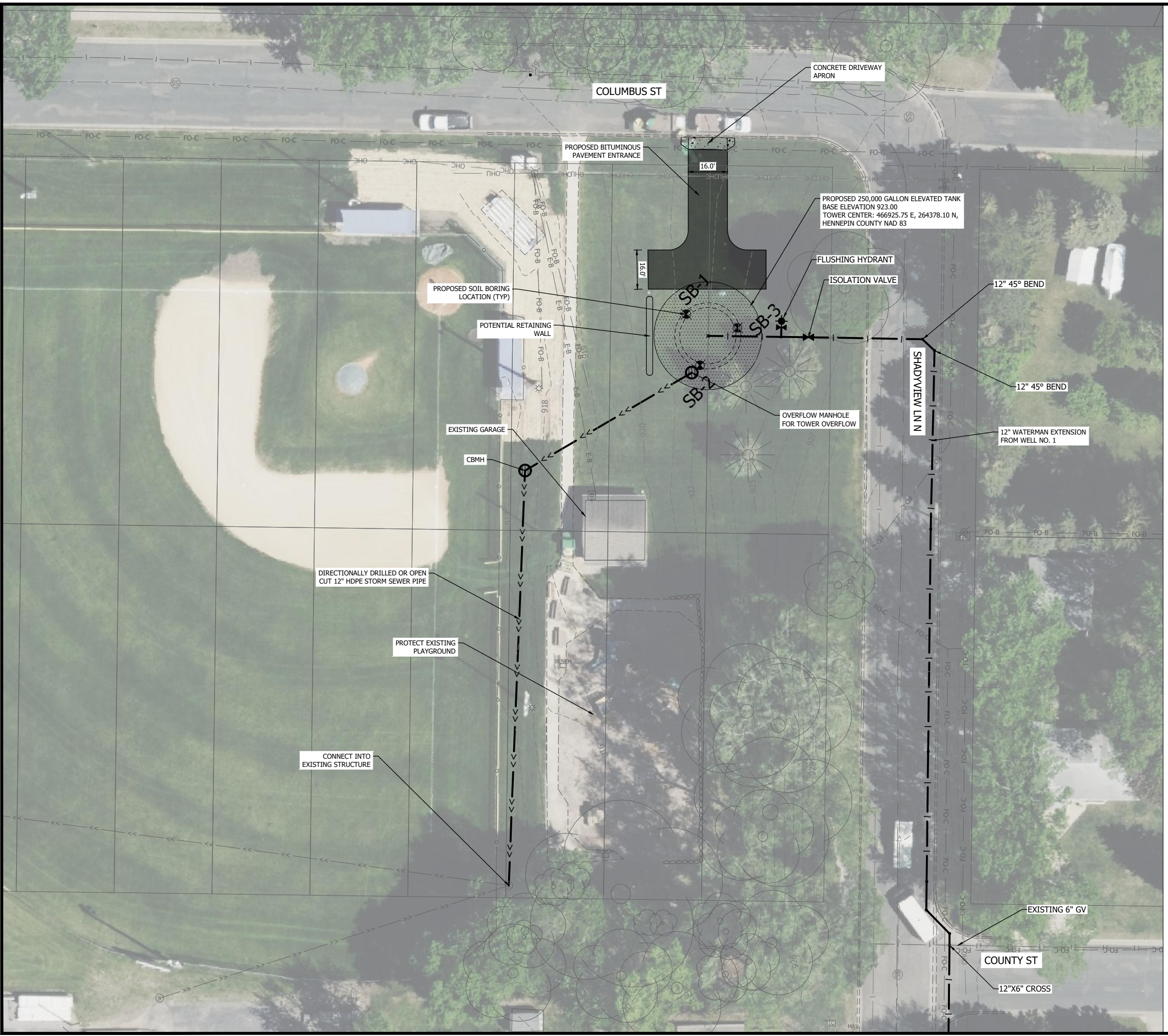
State bonding funds have been acquired for the project. If any specific conditions apply due to the funding source, the schedule and/or scope may need to be altered.



Appendix A Preliminary Design Drawings



Plot Date: 05/08/2026 - 9:24am
 Drawing Name: \\s0242-pdfs01\stated_projects\193807550\CAD\DWG\193807550-C101.dwg
 Xrefs: 193807550-XSHP, 193807550-XSXT, 193807550-BORDER, 193807550-XSS, 193807550-IMAGE



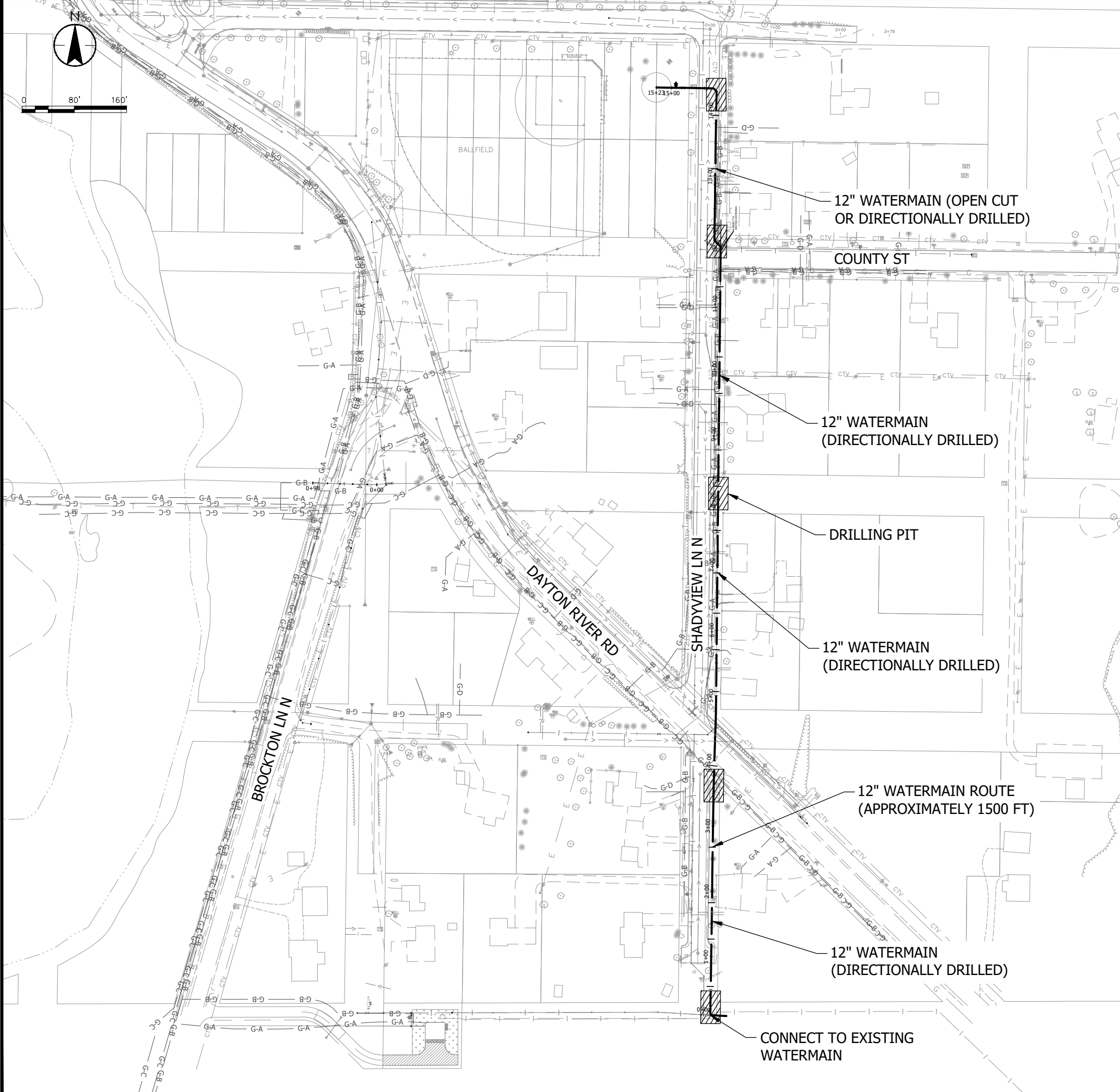
I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY CLOSE PERSONAL SUPERVISION AND THAT I AM A LICENSED PROFESSIONAL ENGINEER UNDER THE ENGINEERING ACT OF MINNESOTA.
 PRINT NAME: _____ LIC. NO.: _____
 SIGNATURE: _____ DATE: _____

CITY OF DAYTON, MINNESOTA
NW WATER STORAGE
SITE PLAN

NO.	REVISION	DATE
1	30% DESIGN	05/19/2026

SURVEY	XXX
DRAWN	JJE
DESIGNED	JJE
CHECKED	NMF
APPROVED	MJS
PROJ. NO.	193807550
SHEET NUMBER	C-101

THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS. DO NOT SCALE THE DRAWING. ANY ERRORS OR OMISSIONS SHALL BE REPORTED TO STANTEC WITHOUT DELAY. STANTEC SHALL NOT BE RESPONSIBLE FOR ANY DAMAGE TO PROPERTY OR PERSONS OR FOR USE FOR ANY PURPOSE OTHER THAN THAT AUTHORIZED BY STANTEC. IS FORBIDDEN.



CENTERPOINT GAS ABANDONED	— G-A —	— G-A —	— G-A —
CENTERPOINT GAS STEEL/PLASTIC (BLUE)	— G-B —	— G-B —	— G-B —
CENTERPOINT GAS STEEL (GREEN)	— G-C —	— G-C —	— G-C —
CENTERPOINT SERVICES	— G-D —	— G-D —	— G-D —
ELK RIVER SCHOOLS FIBER OPTIC	— FO-A —	— FO-A —	— FO-A —
ELK RIVER MUNICIPAL FIBER OPTIC OVERHEAD	— OHC —	— OHC —	— OHC —
ELK RIVER MUNICIPAL FIBER OPTIC UNDERGROUND	— FO-B —	— FO-B —	— FO-B —
SPECTRUM FIBER OPTIC (SOLID)	— FO-C —	— FO-C —	— FO-C —
VERIZON	— C —	— C —	— C —

12" WATERMAIN (OPEN CUT OR DIRECTIONALLY DRILLED)

12" WATERMAIN (DIRECTIONALLY DRILLED)

DRILLING PIT

12" WATERMAIN (DIRECTIONALLY DRILLED)

12" WATERMAIN ROUTE (APPROXIMATELY 1500 FT)

12" WATERMAIN (DIRECTIONALLY DRILLED)

CONNECT TO EXISTING WATERMAIN

Plot Date: 05/08/2026 - 9:24am
 Drawing name: \\s0242-pdfs01\statered_projects\193807550\CAD\DWG\193807550-C401 Layout Figure.dwg
 Xref: 193807550-BORDER; 193807550-SPL; NW Tower Water Main Alignment; 193807550-XSS; 193807550-Gas Mains; 193807550-XST



One Carlson Parkway N, Suite 100
 Plymouth, MN 55447
 www.stantec.com

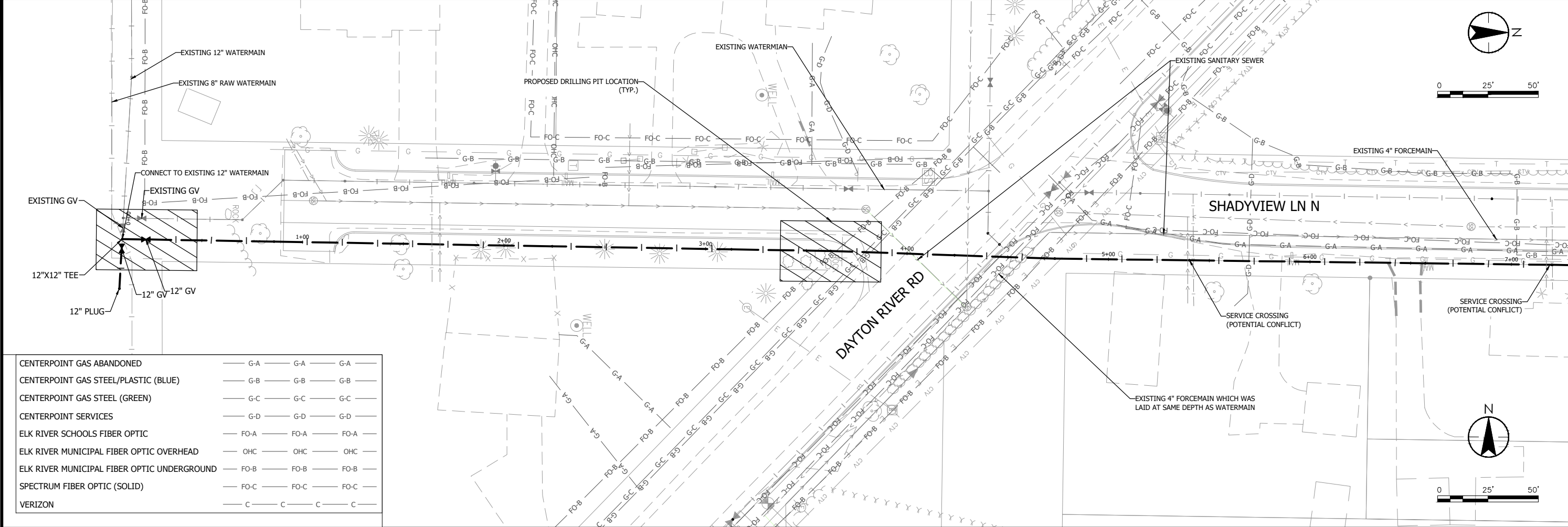
I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY SUPERVISION AND THAT I AM A LICENSED PROFESSIONAL ENGINEER UNDER THE ENGINEERING ACT OF MINNESOTA.
 PRINT NAME: _____
 SIGNATURE: _____
 DATE: _____
 LIC. NO.: _____

CITY OF DAYTON, MINNESOTA
 NW WATER STORAGE
 OVERALL FIGURE

NO.	REVISION	DATE
1	30% DESIGN	05/19/2026
	SURVEY	XXX
	DRAWN	JJE
	DESIGNED	JJE
	CHECKED	NMF
	APPROVED	MJS
	PROJ. NO.	193807550

SHEET NUMBER
C-401

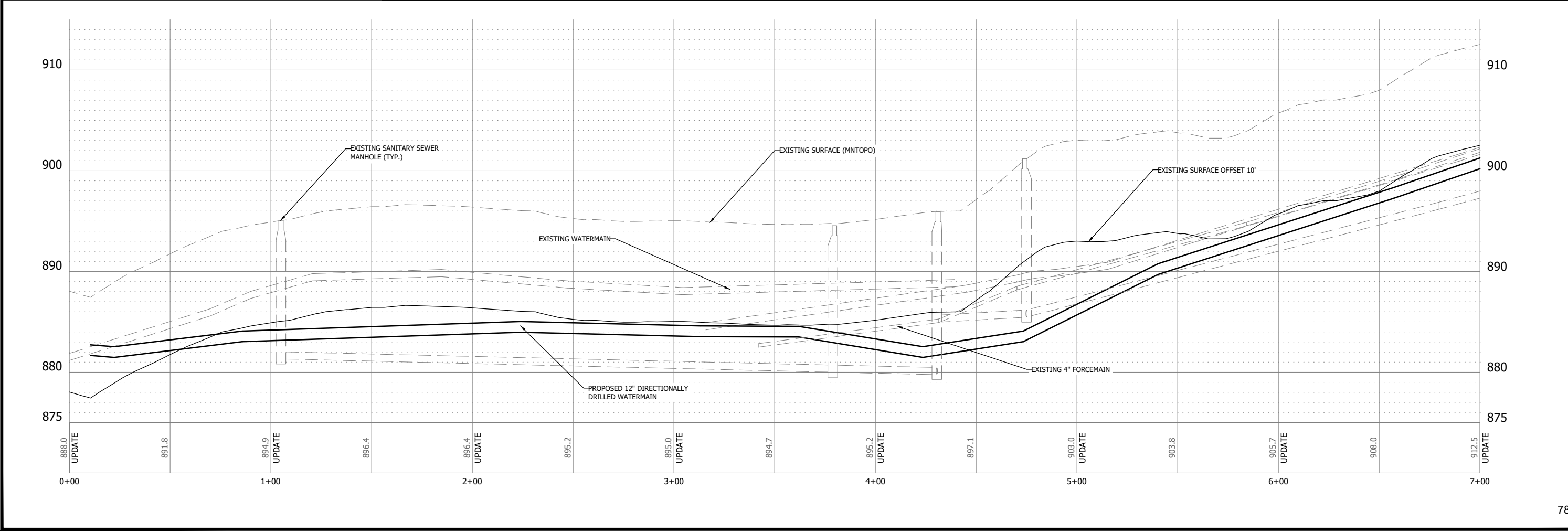
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PRINT NAME: _____
 SIGNATURE: _____
 DATE: _____



CITY OF DAYTON, MINNESOTA
 NW WATER STORAGE
 PROPOSED WATERMAIN ROUTE

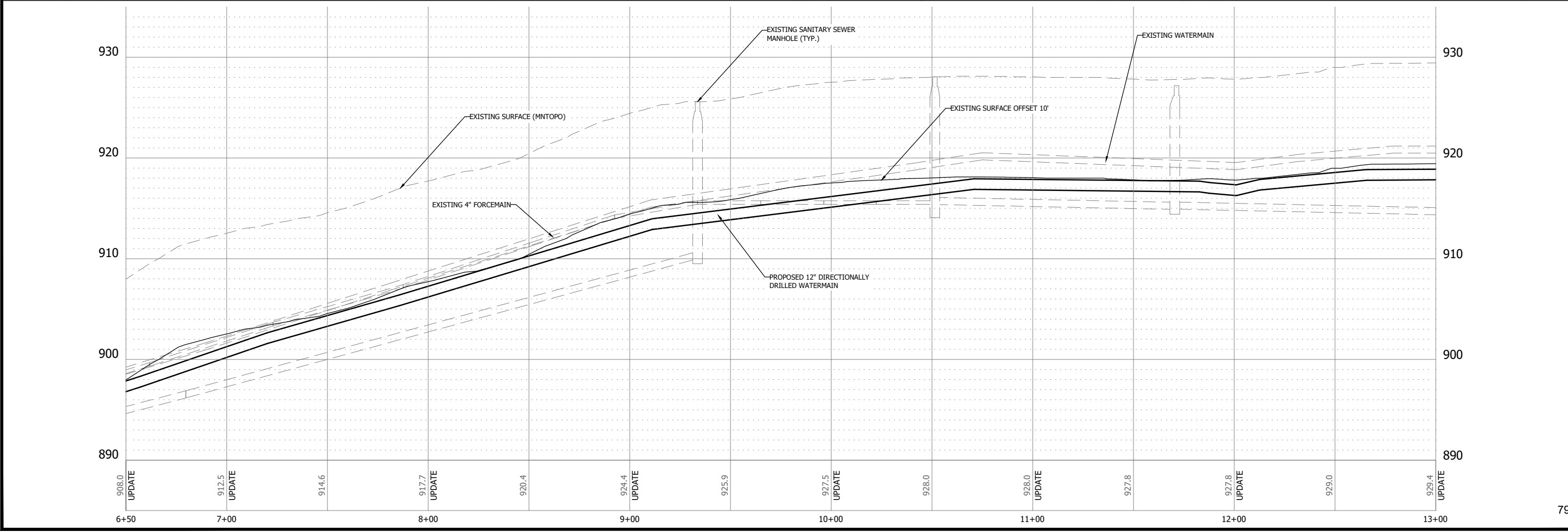
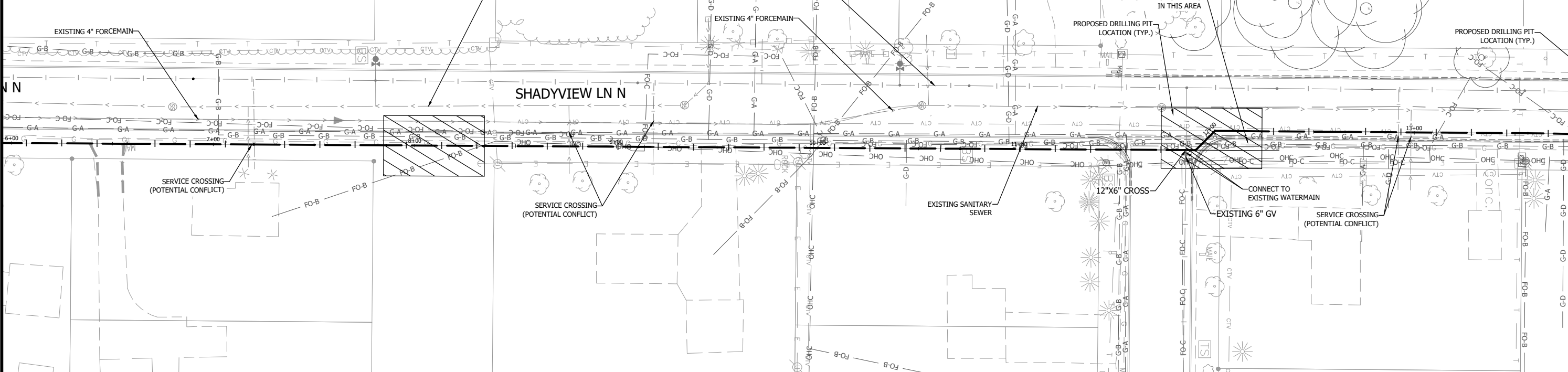
NO.	REVISION	DATE
1	30% DESIGN	05/19/2026

SURVEY: XXXX
 DRAWN: JJE
 DESIGNED: JJE
 CHECKED: NMF
 APPROVED: MJS
 PROJ. NO.: 193807550

SHEET NUMBER
C-402

THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS. DO NOT SCALE THE DRAWING. ANY ERRORS OR OMISSIONS SHALL BE REPORTED TO STANTEC WITHOUT DELAY. STANTEC SHALL NOT BE RESPONSIBLE FOR ANY ERRORS OR OMISSIONS OTHER THAN THOSE AUTHORIZED BY STANTEC. STANTEC IS NOT PROVIDING ANY DESIGN OR CONSTRUCTION SERVICES FOR ANY PURPOSES OTHER THAN THOSE AUTHORIZED BY STANTEC. STANTEC IS NOT PROVIDING ANY DESIGN OR CONSTRUCTION SERVICES FOR ANY PURPOSES OTHER THAN THOSE AUTHORIZED BY STANTEC.

CENTERPOINT GAS ABANDONED	G-A	G-A	G-A
CENTERPOINT GAS STEEL/PLASTIC (BLUE)	G-B	G-B	G-B
CENTERPOINT GAS STEEL (GREEN)	G-C	G-C	G-C
CENTERPOINT SERVICES	G-D	G-D	G-D
ELK RIVER SCHOOLS FIBER OPTIC	FO-A	FO-A	FO-A
ELK RIVER MUNICIPAL FIBER OPTIC OVERHEAD	OHC	OHC	OHC
ELK RIVER MUNICIPAL FIBER OPTIC UNDERGROUND	FO-B	FO-B	FO-B
SPECTRUM FIBER OPTIC (SOLID)	FO-C	FO-C	FO-C
VERIZON	C	C	C



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 I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY SUPERVISION AND THAT I AM A LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
 PRINT NAME: _____ LIC. NO.: _____
 SIGNATURE: _____
 DATE: _____

CITY OF DAYTON, MINNESOTA
 NW WATER STORAGE
 PROPOSED WATERMAIN ROUTE

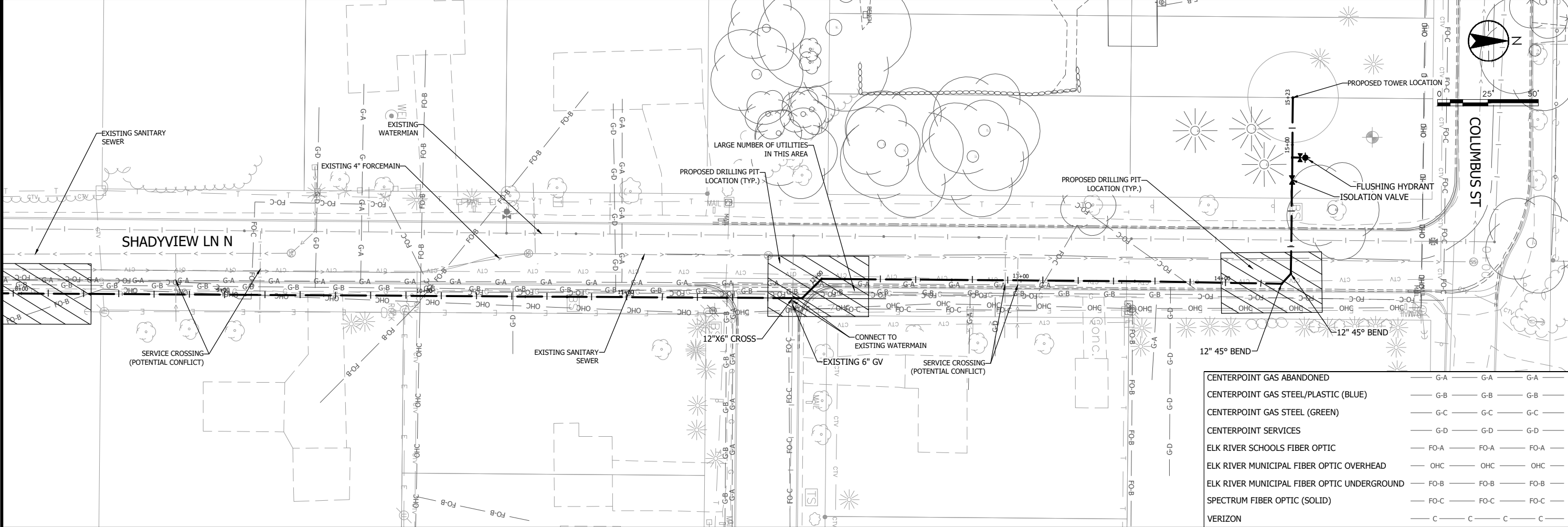
NO.	REVISION	DATE
1	30% DESIGN	05/19/2026

SURVEY: XXXX
 DRAWN: JJE
 DESIGNED: JJE
 CHECKED: NMF
 APPROVED: MJS
 PROJ. NO.: 193807550

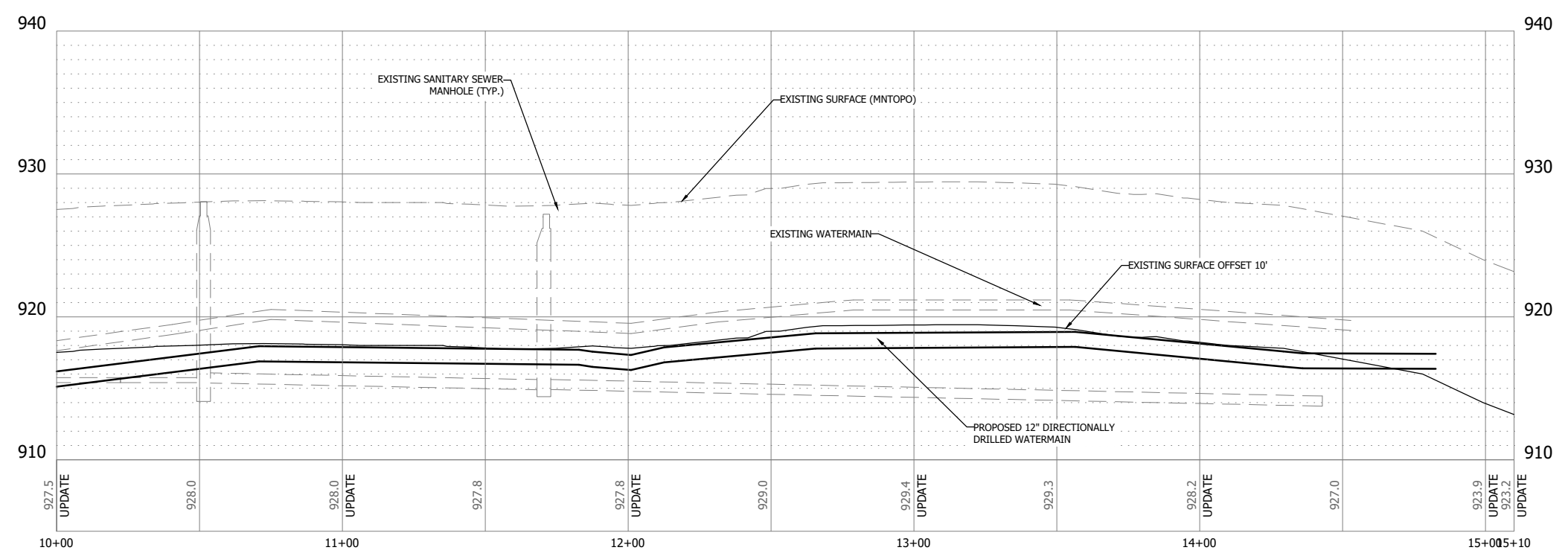
SHEET NUMBER
C-403

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Plot Date: 05/08/2026 - 9:26am
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 Xref: 193807550-BORDER, 193807550-SPL, NW Tower Water Main Alignment, 193807550-XSS, 193807550-XST



CENTERPOINT GAS ABANDONED	G-A	G-A	G-A
CENTERPOINT GAS STEEL/PLASTIC (BLUE)	G-B	G-B	G-B
CENTERPOINT GAS STEEL (GREEN)	G-C	G-C	G-C
CENTERPOINT SERVICES	G-D	G-D	G-D
ELK RIVER SCHOOLS FIBER OPTIC	FO-A	FO-A	FO-A
ELK RIVER MUNICIPAL FIBER OPTIC OVERHEAD	OHC	OHC	OHC
ELK RIVER MUNICIPAL FIBER OPTIC UNDERGROUND	FO-B	FO-B	FO-B
SPECTRUM FIBER OPTIC (SOLID)	FO-C	FO-C	FO-C
VERIZON	C	C	C



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NOT FOR CONSTRUCTION

CITY OF DAYTON, MINNESOTA
 NW WATER STORAGE
 PROPOSED WATERMAIN ROUTE

NO.	REVISION	DATE
1	30% DESIGN	05/19/2026

NO. SURVEY DRAWN DESIGNED CHECKED APPROVED PROJ. NO. SHEET NUMBER

XXX JJE JJE NMF MJS 193807550 C-404

Appendix B Opinion of Probable Cost

B.1 Trunk Watermain

Item Description	Unit	Quantity	Unit Price	Total Price
General	LS	1	\$40,000	\$40,000
Removals	LS	1	\$15,000	\$15,000
Watermain Improvements	LS	1	\$400,000	\$400,000
Street Restoration	LS	1	\$40,000	\$40,000
Erosion Control/Site Restoration	LS	1	\$15,000	\$15,000
Construction Total				\$510,000
Contingency (30%)				\$155,000
Subtotal				\$665,000
Legal, Engineering, Administration, Finance (25%)				\$165,000
Total Project Costs				\$830,000

B.2 Water Storage Tank

Item Description	Unit	Quantity	Unit Price	Total Price
Water Storage Tank	LS	1	\$1,900,000	\$1,900,000
Site Work	LS	1	\$150,000	\$150,000
Construction Total				\$2,050,000
Contingency (15%)				\$310,000
Subtotal				\$2,360,000
Legal, Engineering, Administration, Finance (20%)				\$475,000
Total Project Costs				\$2,835,000





Stantec is a global leader in sustainable engineering, architecture, and environmental consulting. The diverse perspectives of our partners and interested parties drive us to think beyond what's previously been done on critical issues like climate change, digital transformation, and future-proofing our cities and infrastructure. We innovate at the intersection of community, creativity, and client relationships to advance communities everywhere, so that together we can redefine what's possible.

Stantec Consulting Services Inc.
733 Marquette Avenue, Suite 1000
Minneapolis MN 55402-2314
stantec.com



Payments to be approved at City Council Meeting May 26, 2026

	<u>Totals</u>
Claims Roster 05-26-2026	\$ 291,594.74
Prepaid 05-07-2026 EB	\$ 116,744.22
Prepaid 05-15-2026 FB	\$ 10,485.64
Total Payments:	\$ 418,824.60
Payroll 05-07-2026 Bi-Weekly 10	\$ 168,622.36
Payroll 05-15-2026 FD 04.2026	\$ 4,295.34

Check # sequence to be approved by City Council from meeting date of 5/26/2026:

Checks # 080816-080891

05/20/2026

INVOICE REGISTER REPORT FOR CITY OF DAYTON MN
 EXP CHECK RUN DATES 05/26/2026 - 05/26/2026
 BOTH JOURNALIZED AND UNJOURNALIZED
 BOTH OPEN AND PAID

Inv Num Inv Ref#	Vendor Description GL Distribution	Inv Date Entered By	Due Date	Inv Amt	Amt Due	Status
	ASPEN MILLS FD; UNIFORM/ KNIGHT 101-42260-50217 FD; UNIFORM	05/11/2026 CHOYT	05/26/2026	440.62 440.62	440.62	Open
	ASPEN MILLS FD; UNIFORM/ FLERMOEN 101-42260-50217 FD; UNIFORM	05/11/2026 CHOYT	05/26/2026	479.58 479.58	479.58	Open
	ASPEN MILLS FD; UNIFORM/ WENDLANDT 101-42260-50217 FD; UNIFORM	05/20/2026 CHOYT	05/26/2026	391.73 391.73	391.73	Open
	BEAUDRY PW; ULS #2 DYED DIESEL -535.50 101-43100-50212 PW; ULS #2 DYED DIESEL -535.50	05/13/2026 CHOYT	05/26/2026	2,573.61 2,573.61	2,573.61	Open
	BEAUDRY PW; UNLEADED 87 -487.00 101-43100-50212 PW; UNLEADED 87 -487.00	05/13/2026 CHOYT	05/26/2026	1,840.37 1,840.37	1,840.37	Open
	BEAUDRY PW; OPERATING SUPPLIES 101-43100-50210 PW; OPERATING SUPPLIES	05/18/2026 CHOYT	05/26/2026	2,100.00 2,100.00	2,100.00	Open
	BEAUDRY	05/20/2026	05/26/2026	3,120.99	3,120.99	Open

PW; UNLEADED 87 -807.50	CHOYT				
101-43100-50212	PW; UNLEADED 87 -807.50			3,120.99	
<hr/>					
C. VISION PRODUCTION		05/18/2026	05/26/2026	1,000.00	1,000.00 Open
VIDEO TECH; EGG HUNT/OPEN HOUSE	CHOYT				
226-41900-50430	Miscellaneous			1,000.00	
<hr/>					
CENTERPOINT ENERGY		05/11/2026	05/26/2026	30.46	0.00 Paid
RH WELLHOUSE; 11429952-2	CHOYT				
601-49400-50383	RH WELLHOUSE; 11429952-2			30.46	
<hr/>					
CENTERPOINT ENERGY		05/11/2026	05/26/2026	2,412.25	0.00 Paid
PW/PD GAS UTILITIES; 10662228-5 APR 2026	CHOYT				
101-41810-50383	PW/PD GAS UTILITIES; 10662228-5			2,412.25	
<hr/>					
CHARTER COMMUNICATIONS		05/18/2026	05/26/2026	1,285.79	0.00 Paid
ACCOUNT #175337501050726; MAY-JUN 2026	CHOYT				
101-42120-50320	LOCATION #243204401- PD; INTERNET			202.09	
101-43100-50321	LOCATION #243204401-PW; INTERNET			202.09	
101-42260-50320	LOCATION #175337701-FD2; INTERNET			45.15	
101-41820-50308	LOCATION #175337801; CH/INTERNET;			199.98	
601-49400-50321	LOCATION #175337201; WELLHOUSE/INTERNET;			99.98	
101-42260-50320	LOCATION #175337601; FD 1/INTERNET;			119.99	
101-42120-50320	ACCOUNT# 175351601- PD;NUMBER FORWARDING			16.51	
101-41820-50308	LOCATION #175337901- CH/FIBER INTERNET			400.00	
<hr/>					
CINTAS		05/11/2026	05/26/2026	154.10	154.10 Open
PW; UNIFORMS	CHOYT				
101-43100-50217	PW; UNIFORMS			154.10	
<hr/>					
CINTAS		05/18/2026	05/26/2026	153.10	153.10 Open
PW; UNIFORMS	CHOYT				
101-43100-50217	PW; UNIFORMS			153.10	
<hr/>					

CITY OF ANOKA	05/11/2026	05/26/2026	277.40	0.00	Paid
22-393200-01 CENTRAL PARK; APR 2026	CHOYT				
101-41810-50381	22-393200-01 CENTRAL PARK; APR 2026		277.40		

CITY OF ANOKA	05/11/2026	05/26/2026	23.50	0.00	Paid
22-393400-00 SDLR SIREN; APR 2026	CHOYT				
101-41810-50381	22-393400-00 SDLR SIREN; APR 2026		23.50		

CITY OF ANOKA	05/11/2026	05/26/2026	913.88	0.00	Paid
22-396000-01 CH; APR 2026	CHOYT				
101-41810-50381	22-396000-01 CH; APR 2026		913.88		

CITY OF ANOKA	05/11/2026	05/26/2026	514.10	0.00	Paid
22-990002-01 STREET LIGHTS; APR 2026	CHOYT				
101-43100-50230	22-990002-01 STREET LIGHTS; APR 2026		514.10		

CITY OF ANOKA	05/11/2026	05/26/2026	81.19	0.00	Paid
22-396030-00 BALSAM LANE PED;APR 2026	CHOYT				
101-43100-50230	22-396030-00 BALSAM LANE PED;APR 2026		81.19		

CITY OF DAYTON	05/13/2026	05/26/2026	200.00	200.00	Open
15060 DIAMOND LK RD-REINSPECTION X2	CHOYT				
601-49400-50300	15060 DIAMOND LK RD-REINSPECTION		200.00		

CITY OF MAPLE GROVE	05/19/2026	05/26/2026	41,404.35	41,404.35	Open
16,237,000 GALLONS WATER USED MAR/APR 202	CHOYT				
601-49400-50389	16,237,000 GALLONS WATER USAGE MAR/APR		41,404.35		

CMT JANITORIAL SERVICES	05/20/2026	05/26/2026	1,602.00	1,602.00	Open
CONTRACT SERVICES-OFC CLEANING JUN 2026	CHOYT				
101-41910-50308	CONTRACT SERVICES-OFC CLEANING		602.00		
101-41810-50308	CONTRACT SERVICES-OFC CLEANING		1,000.00		

DATAWORKS PLUS LLC	05/13/2026	05/26/2026	1,122.16	1,122.16	Open

MAINTENANCE FEE SW&HW 5/1/2026-4/30/2027 CHOYT					
101-42120-50308	CONTRACT SERVICES 5/1/2026-4/30/2027			1,122.16	
ECKBERG LAMMERS PC		05/11/2026	05/26/2026	789.50	789.50 Open
PROFESSIONAL SRVS; APR 2026	CHOYT				
225-41710-50300	PROFESSIONAL SRVS; APR 2026			470.00	
101-41640-50312	LEGAL OTHER-APR 2026			319.50	
ECM PUBLISHERS, INC		05/11/2026	05/26/2026	105.00	105.00 Open
GENERAL NOTICES AND PUB INFO-FILING NOTIC	CHOYT				
101-41410-50210	GENERAL NOTICES AND PUB INFO-FILING NOTI			105.00	
GUIDANCEPOINT TECHNOLOGIES		05/11/2026	05/26/2026	700.35	700.35 Open
OFFICE 365 -1 YR BILLED MONTHLY	CHOYT				
101-41820-50205	OFFICE 365 -1 YR BILLED MONTHLY			700.35	
GUIDANCEPOINT TECHNOLOGIES		05/13/2026	05/26/2026	185.00	185.00 Open
IT; PHONE CHANGE/UPDATE	CHOYT				
101-41820-50300	IT; PHONE CHANGE/UPDATE			185.00	
GUIDANCEPOINT TECHNOLOGIES		05/13/2026	05/26/2026	185.00	185.00 Open
IT; WATCHGUARD INSTALL	CHOYT				
101-41820-50300	IT; WATCHGUARD INSTALL			185.00	
GUIDANCEPOINT TECHNOLOGIES		05/13/2026	05/26/2026	555.00	555.00 Open
IT; PROFESSIONAL SRVS DOOR LOCK SYSTEM	CHOYT				
101-41820-50300	IT; PROFESSIONAL SRVS DOOR LOCK SYSTEM			555.00	
GUIDANCEPOINT TECHNOLOGIES		05/18/2026	05/26/2026	175.00	175.00 Open
IT; PROFESSIONAL SRVS	CHOYT				
101-41820-50300	IT; PROFESSIONAL SRVS			175.00	
GUIDANCEPOINT TECHNOLOGIES		05/18/2026	05/26/2026	305.00	305.00 Open
IT; OFFSITE BACKUP	CHOYT				

101-41820-50300	IT; OFFSITE BACKUP			305.00		
HANSON BUILDERS		05/11/2026	05/26/2026	3,000.00	3,000.00	Open
11188 KINGSVIEW LN N LANDSCAPE ESCROW RE CHOYT						
420-00000-22100	11188 KINGSVIEW LN N LANDSCAPE ESCROW RE			3,000.00		
HANSON BUILDERS		05/11/2026	05/26/2026	3,000.00	3,000.00	Open
11178 KINGSVIEW LN N LANDSCAPE ESCROW RE CHOYT						
420-00000-22100	11178 KINGSVIEW LN N LANDSCAPE ESCROW RE			3,000.00		
HANSON BUILDERS		05/11/2026	05/26/2026	3,000.00	3,000.00	Open
11087 KINGSVIEW LN N LANDSCAPE ESCROW RE CHOYT						
420-00000-22100	11087 KINGSVIEW LN N LANDSCAPE ESCROW RE			3,000.00		
HANSON BUILDERS		05/11/2026	05/26/2026	3,000.00	3,000.00	Open
11088 KINGSVIEW LN N LANDSCAPE ESCROW RE CHOYT						
420-00000-22100	11088 KINGSVIEW LN N LANDSCAPE ESCROW RE			3,000.00		
HENNEPIN COUNTY		05/11/2026	05/26/2026	2,254.82	2,254.82	Open
FD; RADIO LEASE-APR 2026	CHOYT					
101-42260-50320	FD; RADIO LEASE-APR 2026			2,254.82		
HENNEPIN COUNTY		05/13/2026	05/26/2026	2,926.18	0.00	Paid
PD; RADIO LEASE- APR 2026	CHOYT					
101-42120-50320	PD; RADIO LEASE- APR 2026			2,926.18		
HENNEPIN COUNTY FIRE CHIEFS ASSOC.		05/18/2026	05/26/2026	900.00	0.00	Paid
BLUE CARD CE TRAINING	CHOYT					
101-42260-50208	BLUE CARD CE			900.00		
HENNEPIN COUNTY SHERIFFS OFFIC		05/19/2026	05/26/2026	533.68	533.68	Open
PD; PER DIEM AND PROCESSING APR 2026	CHOYT					
101-42120-50306	PD; PER DIEM AND PROCESSING APR			533.68		

HP GROUP HEALTH NON-PATIENT A/R	05/13/2026	05/26/2026	105.25	0.00	Paid
EAP (CUST#12750101) APR 2026	CHOYT				
101-41810-50205	EAP NON MEMBER(CUST# 12750101);APR 2026		66.30		
101-41810-50205	EAP MEMBER (CUST# 12750101); APR 2026		38.95		

INNOVATIVE OFFICE SOLUTIONS	05/13/2026	05/26/2026	220.90	0.00	Paid
CH; SUPPLIES	CHOYT				
101-41810-50200	CH; SUPPLIES		220.90		

IYANU DARAMOLA	05/18/2026	05/26/2026	825.00	825.00	Open
DAC RENTAL DEPOSIT REFUND;EVENT CANCELLE	CHOYT				
101-00000-21716	DAC RENTAL DEPOSIT REFUND;EVENT CANCEL		825.00		

J R ADVANCED RECYCLERS	05/19/2026	05/26/2026	1,375.25	1,375.25	Open
CITY CLEAN-UP DAY 2026	CHOYT				
101-41650-50388	CITY CLEAN-UP DAY 2026		1,375.25		

J.P. MORGAN CHASE BANK NA	04/13/2026	05/26/2026	30.00	30.00	Open
ID Card Subscription	CHOYT				
101-41810-50205	ID Card Subscription		30.00		

J.P. MORGAN CHASE BANK NA	04/13/2026	05/26/2026	35.00	35.00	Open
NTOA 2026 Membership	CHOYT				
101-42120-50205	NTOA 2026 Membership		35.00		

J.P. MORGAN CHASE BANK NA	04/27/2026	05/26/2026	171.95	171.95	Open
UNIFORM ALLOWANCE-MURPHY	CHOYT				
101-42120-50217	Uniform Allowance		171.95		

J.P. MORGAN CHASE BANK NA	04/27/2026	05/26/2026	120.00	120.00	Open
UNIFORM ALLOWANCE-MURPHY	CHOYT				
101-42120-50217	Uniform Allowance		120.00		

J.P. MORGAN CHASE BANK NA	04/28/2026	05/26/2026	225.00	225.00	Open

Firearms Safety Equip	CHOYT				
101-42120-50580	Firearms Safety Equip			225.00	
J.P. MORGAN CHASE BANK NA	03/31/2026	05/26/2026		38.99	38.99 Open
Safety Vests for Easter Egg volunteers	CHOYT				
101-41910-50210	Safety Vests for Easter Egg volunteers			38.99	
J.P. MORGAN CHASE BANK NA	04/01/2026	05/26/2026		49.81	49.81 Open
STAFF LUNCH MTG	CHOYT				
101-43100-50210	STAFF LUNCH MTG			49.81	
J.P. MORGAN CHASE BANK NA	04/01/2026	05/26/2026		169.37	169.37 Open
STAFF LUNCH MTG	CHOYT				
101-43100-50210	STAFF LUNCH MTG			169.37	
J.P. MORGAN CHASE BANK NA	04/02/2026	05/26/2026		39.00	39.00 Open
Vest\s for volunteers East4er Egg Hunt	CHOYT				
101-41910-50210	Vest\s for volunteers East4er Egg Hunt			39.00	
J.P. MORGAN CHASE BANK NA	04/08/2026	05/26/2026		35.99	35.99 Open
Replacement Power strip	CHOYT				
101-43100-50210	Replacement Power strip			35.99	
J.P. MORGAN CHASE BANK NA	04/09/2026	05/26/2026		334.35	334.35 Open
Replace Vacuum cleaner	CHOYT				
101-41910-50220	Replace Vacuum cleaner			334.35	
J.P. MORGAN CHASE BANK NA	04/15/2026	05/26/2026		2,649.95	2,649.95 Open
Field Equipment Storage boxed	CHOYT				
101-41910-50210	Field Equipment Storage boxed			2,649.95	
J.P. MORGAN CHASE BANK NA	04/28/2026	05/26/2026		315.00	315.00 Open
APWA Annual Membership	CHOYT				
101-43100-50205	APWA Annual Membership			315.00	

J.P. MORGAN CHASE BANK NA	04/10/2026	05/26/2026	15.78	15.78	Open
Peer Compass Group Lunch	CHOYT				
101-41310-50200 Peer Compass Group Lunch			15.78		
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J.P. MORGAN CHASE BANK NA	04/22/2026	05/26/2026	2.50	2.50	Open
Senate Meeting on Tier 1 EIS	CHOYT				
101-41310-50331 Senate Meeting on Tier 1 EIS			2.50		
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J.P. MORGAN CHASE BANK NA	04/24/2026	05/26/2026	25.00	25.00	Open
Lunch Meeting	CHOYT				
101-41310-50200 Lunch Meeting			25.00		
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J.P. MORGAN CHASE BANK NA	04/27/2026	05/26/2026	70.00	70.00	Open
First Responders Event	CHOYT				
101-41110-50208 First Responders Event			70.00		
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J.P. MORGAN CHASE BANK NA	04/27/2026	05/26/2026	70.00	70.00	Open
First Responders Event	CHOYT				
101-41310-50208 First Responders Event			70.00		
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J.P. MORGAN CHASE BANK NA	04/14/2026	05/26/2026	64.54	64.54	Open
Council food	CHOYT				
101-41110-50210 Council food			64.54		
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J.P. MORGAN CHASE BANK NA	04/20/2026	05/26/2026	698.43	698.43	Open
Digium April	CHOYT				
101-41810-50308 Digium April			698.43		
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J.P. MORGAN CHASE BANK NA	04/21/2026	05/26/2026	39.99	39.99	Open
phone screen new employee	CHOYT				
101-43100-50321 phone screen new employee			39.99		
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J.P. MORGAN CHASE BANK NA	04/23/2026	05/26/2026	11.00	11.00	Open

BI pro		CHOYT				
101-41820-50308	BI pro				11.00	
J.P. MORGAN CHASE BANK NA		04/23/2026	05/26/2026		201.25	201.25 Open
tabs for trailer		CHOYT				
101-43100-50205	tabs for trailer				201.25	
J.P. MORGAN CHASE BANK NA		04/23/2026	05/26/2026		4.33	4.33 Open
tabs for trailer		CHOYT				
101-43100-50205	tabs for trailer				4.33	
J.P. MORGAN CHASE BANK NA		04/26/2026	05/26/2026		52.09	52.09 Open
zoom		CHOYT				
101-41500-50205	zoom				52.09	
J.P. MORGAN CHASE BANK NA		03/31/2026	05/26/2026		1,188.00	1,188.00 Open
ClerkMinutes subscription		CHOYT				
101-41810-50205	ClerkMinutes subscription				1,188.00	
J.P. MORGAN CHASE BANK NA		04/01/2026	05/26/2026		479.87	479.87 Open
envelopes		CHOYT				
101-41810-50200	envelopes				479.87	
J.P. MORGAN CHASE BANK NA		04/03/2026	05/26/2026		152.28	152.28 Open
title dais nameplates		CHOYT				
101-41810-50200	title dais nameplates				152.28	
J.P. MORGAN CHASE BANK NA		04/06/2026	05/26/2026		540.00	540.00 Open
clerk institute-year two		CHOYT				
101-41420-50208	clerk institute-year two				540.00	
J.P. MORGAN CHASE BANK NA		04/08/2026	05/26/2026		34.16	34.16 Open
title dais nameplates		CHOYT				
101-41810-50200	title dais nameplates				34.16	

J.P. MORGAN CHASE BANK NA	04/08/2026	05/26/2026	37.99	37.99	Open
office supplies- tab folders	CHOYT				
101-41810-50200	office supplies- tab folders		37.99		
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J.P. MORGAN CHASE BANK NA	04/11/2026	05/26/2026	60.78	60.78	Open
office supplies- USB memory sticks	CHOYT				
101-41810-50200	office supplies- USB memory sticks		60.78		
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J.P. MORGAN CHASE BANK NA	04/18/2026	05/26/2026	16.54	16.54	Open
Office Supplies	CHOYT				
101-41810-50200	Office Supplies		16.54		
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J.P. MORGAN CHASE BANK NA	04/23/2026	05/26/2026	26.99	26.99	Open
nameplate holders	CHOYT				
101-41810-50200	nameplate holders		26.99		
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J.P. MORGAN CHASE BANK NA	04/02/2026	05/26/2026	65.10	65.10	Open
UNIFORM ALLOWANCE-JUNTUNEN	CHOYT				
101-42120-50217	Uniform Allowance		65.10		
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J.P. MORGAN CHASE BANK NA	04/10/2026	05/26/2026	375.00	375.00	Open
Needham DMT Training	CHOYT				
101-42120-50208	Needham DMT Training		375.00		
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J.P. MORGAN CHASE BANK NA	04/02/2026	05/26/2026	59.25	59.25	Open
Black Dirt	CHOYT				
101-43100-50224	Black Dirt		59.25		
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J.P. MORGAN CHASE BANK NA	04/02/2026	05/26/2026	1,447.70	1,447.70	Open
Sand	CHOYT				
101-43100-50224	Sand		1,447.70		
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J.P. MORGAN CHASE BANK NA	04/02/2026	05/26/2026	59.25	59.25	Open

Sand		CHOYT				
101-43100-50220	Sand				59.25	
J.P. MORGAN CHASE BANK NA		04/03/2026	05/26/2026		315.00	315.00 Open
APWA Annual Membership		CHOYT				
101-43100-50205	APWA Annual Membership				315.00	
J.P. MORGAN CHASE BANK NA		04/09/2026	05/26/2026		3.25	3.25 Open
SUBSCRIPTION		CHOYT				
101-43100-50210	SUBSCRIPTION				3.25	
J.P. MORGAN CHASE BANK NA		04/21/2026	05/26/2026		120.00	120.00 Open
Brush Disposal		CHOYT				
101-43100-50210	Brush Disposal				120.00	
J.P. MORGAN CHASE BANK NA		04/29/2026	05/26/2026		125.00	125.00 Open
FUEL SUPPLY SUBSCRIPTION		CHOYT				
101-43100-50212	FUEL SUPPLY SUBSCRIPTION				125.00	
J.P. MORGAN CHASE BANK NA		04/03/2026	05/26/2026		72.29	72.29 Open
Phoenix Dog Food		CHOYT				
101-42260-50200	Phoenix Dog Food				72.29	
J.P. MORGAN CHASE BANK NA		04/06/2026	05/26/2026		4,090.00	4,090.00 Open
FFI FFII Haz-Mat		CHOYT				
101-42260-50208	FFI FFII Haz-Mat				4,090.00	
J.P. MORGAN CHASE BANK NA		04/08/2026	05/26/2026		75.00	75.00 Open
Chief One Firefighting License		CHOYT				
101-42260-50208	Chief One Firefighting License				75.00	
J.P. MORGAN CHASE BANK NA		04/08/2026	05/26/2026		92.50	92.50 Open
Airport Parking		CHOYT				
101-42260-50208	Airport Parking				92.50	

J.P. MORGAN CHASE BANK NA	04/10/2026	05/26/2026	20.97	20.97	Open
Live Burn Breakfast	CHOYT				
101-42260-50208	Live Burn Breakfast		20.97		
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J.P. MORGAN CHASE BANK NA	04/16/2026	05/26/2026	39.06	39.06	Open
Chief One Car Wash	CHOYT				
101-42260-50220	Chief One Car Wash		39.06		
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J.P. MORGAN CHASE BANK NA	04/18/2026	05/26/2026	318.60	318.60	Open
Cell Phone Bill	CHOYT				
101-42260-50320	Cell Phone Bill		318.60		
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J.P. MORGAN CHASE BANK NA	04/19/2026	05/26/2026	3.79	3.79	Open
Water	CHOYT				
101-42260-50208	Water		3.79		
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J.P. MORGAN CHASE BANK NA	04/20/2026	05/26/2026	52.05	52.05	Open
FDIC Dinner	CHOYT				
101-42260-50208	FDIC Dinner		52.05		
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J.P. MORGAN CHASE BANK NA	04/20/2026	05/26/2026	36.88	36.88	Open
FDIC Uber	CHOYT				
101-42260-50208	FDIC Uber		36.88		
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J.P. MORGAN CHASE BANK NA	04/20/2026	05/26/2026	80.50	80.50	Open
FDIC Dinner	CHOYT				
101-42260-50208	FDIC Dinner		80.50		
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J.P. MORGAN CHASE BANK NA	04/20/2026	05/26/2026	7.03	7.03	Open
Coffee	CHOYT				
101-42260-50208	Coffee		7.03		
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J.P. MORGAN CHASE BANK NA	04/20/2026	05/26/2026	15.18	15.18	Open

FDIC Lunch		CHOYT				
101-42260-50208	FDIC Lunch				15.18	
J.P. MORGAN CHASE BANK NA		04/21/2026	05/26/2026		82.00	82.00 Open
FDIC Dinner		CHOYT				
101-42260-50208	FDIC Dinner				82.00	
J.P. MORGAN CHASE BANK NA		04/21/2026	05/26/2026		8.51	8.51 Open
FDIC Lunch		CHOYT				
101-42260-50208	FDIC Lunch				8.51	
J.P. MORGAN CHASE BANK NA		04/21/2026	05/26/2026		7.03	7.03 Open
Coffee		CHOYT				
101-42260-50208	Coffee				7.03	
J.P. MORGAN CHASE BANK NA		04/22/2026	05/26/2026		17.42	17.42 Open
FDIC Lunch		CHOYT				
101-42260-50208	FDIC Lunch				17.42	
J.P. MORGAN CHASE BANK NA		04/22/2026	05/26/2026		16.02	16.02 Open
Coffee		CHOYT				
101-42260-50208	Coffee				16.02	
J.P. MORGAN CHASE BANK NA		04/23/2026	05/26/2026		16.46	16.46 Open
Coffee		CHOYT				
101-42260-50103	Coffee				16.46	
J.P. MORGAN CHASE BANK NA		04/25/2026	05/26/2026		31.99	31.99 Open
FDIC Uber		CHOYT				
101-42260-50208	FDIC Uber				31.99	
J.P. MORGAN CHASE BANK NA		04/24/2026	05/26/2026		1,480.05	1,480.05 Open
FDIC Hotel		CHOYT				
101-42260-50208	FDIC Hotel				1,480.05	

J.P. MORGAN CHASE BANK NA Coffee 101-42260-50208	Coffee	04/24/2026 CHOYT	05/26/2026	15.70 15.70	15.70	Open
J.P. MORGAN CHASE BANK NA UNIFORM ALLOWANCE-BURSTAD 101-42120-50217	Uniform Allowance	04/06/2026 CHOYT	05/26/2026	149.99 149.99	149.99	Open
J.P. MORGAN CHASE BANK NA MCPA Conference Lodging 101-42120-50331	MCPA Conference Lodging	04/15/2026 CHOYT	05/26/2026	621.03 621.03	621.03	Open
J.P. MORGAN CHASE BANK NA MCPA Conference Lodging 101-42120-50331	MCPA Conference Lodging	04/15/2026 CHOYT	05/26/2026	621.03 621.03	621.03	Open
J.P. MORGAN CHASE BANK NA Cebula DMT Training 101-42120-50208	Cebula DMT Training	04/06/2026 CHOYT	05/26/2026	375.00 375.00	375.00	Open
J.P. MORGAN CHASE BANK NA UNIFORM ALLOWANCE-GRIMSBY 101-42120-50217	Uniform Allowance	04/28/2026 CHOYT	05/26/2026	95.92 95.92	95.92	Open
J.P. MORGAN CHASE BANK NA Bluebeam Software subscription 101-41710-50205	Bluebeam Software subscription	04/01/2026 CHOYT	05/26/2026	302.50 302.50	302.50	Open
J.P. MORGAN CHASE BANK NA Postage for water samples 601-49400-50322	Postage for water samples	04/20/2026 CHOYT	05/26/2026	15.17 15.17	15.17	Open
J.P. MORGAN CHASE BANK NA		04/04/2026	05/26/2026	12.66	12.66	Open

UNIFORM ALLOWANCE-NEEDHAM	CHOYT				
101-42120-50217	Uniform Allowance			12.66	
J.P. MORGAN CHASE BANK NA	04/05/2026	05/26/2026		117.98	117.98 Open
UNIFORM ALLOWANCE-DICKMAN	CHOYT				
101-42120-50217	Uniform Allowance			117.98	
J.P. MORGAN CHASE BANK NA	04/06/2026	05/26/2026		211.31	211.31 Open
UNIFORM ALLOWANCE-NEEDHAM	CHOYT				
101-42120-50217	Uniform Allowance			211.31	
J.P. MORGAN CHASE BANK NA	04/06/2026	05/26/2026		329.94	329.94 Open
Open House Supplies	CHOYT				
101-42120-50395	Open House Supplies			329.94	
J.P. MORGAN CHASE BANK NA	04/08/2026	05/26/2026		42.99	42.99 Open
Office Supplies	CHOYT				
101-42120-50200	Office Supplies			42.99	
J.P. MORGAN CHASE BANK NA	04/08/2026	05/26/2026		49.95	49.95 Open
Office Supplies	CHOYT				
101-42120-50200	Office Supplies			49.95	
J.P. MORGAN CHASE BANK NA	04/08/2026	05/26/2026		28.99	28.99 Open
Open House Supplies	CHOYT				
101-42120-50395	Open House Supplies			28.99	
J.P. MORGAN CHASE BANK NA	04/08/2026	05/26/2026		79.31	79.31 Open
Permit to Purchase Cards	CHOYT				
101-42120-50200	Permit to Purchase Cards			79.31	
J.P. MORGAN CHASE BANK NA	04/13/2026	05/26/2026		37.77	37.77 Open
MCPA Conference Meal *2	CHOYT				
101-42120-50331	MCPA Conference Meal *2			37.77	

J.P. MORGAN CHASE BANK NA	04/14/2026	05/26/2026	40.30	40.30	Open
MCPA Conference Meal *2	CHOYT				
101-42120-50331	MCPA Conference Meal *2		40.30		
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J.P. MORGAN CHASE BANK NA	04/14/2026	05/26/2026	44.67	44.67	Open
MCPA Conference Meal *2	CHOYT				
101-42120-50331	MCPA Conference Meal *2		44.67		
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J.P. MORGAN CHASE BANK NA	04/15/2026	05/26/2026	86.65	86.65	Open
MCPA Conference Meal *2	CHOYT				
101-42120-50331	MCPA Conference Meal *2		86.65		
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J.P. MORGAN CHASE BANK NA	04/19/2026	05/26/2026	48.54	48.54	Open
Office Supplies	CHOYT				
101-42120-50200	Office Supplies		48.54		
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J.P. MORGAN CHASE BANK NA	04/22/2026	05/26/2026	195.00	195.00	Open
I-94 Awards Ceremony	CHOYT				
101-42120-50205	I-94 Awards Ceremony		195.00		
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J.P. MORGAN CHASE BANK NA	04/24/2026	05/26/2026	46.99	46.99	Open
Office Supplies	CHOYT				
101-42120-50200	Office Supplies		46.99		
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J.P. MORGAN CHASE BANK NA	04/24/2026	05/26/2026	100.00	100.00	Open
UNIFORM ALLOWANCE-MURPHY	CHOYT				
101-42120-50217	Uniform Allowance		100.00		
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J.P. MORGAN CHASE BANK NA	04/26/2026	05/26/2026	160.76	160.76	Open
Office Supplies	CHOYT				
101-42120-50200	Office Supplies		160.76		
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J.P. MORGAN CHASE BANK NA	04/29/2026	05/26/2026	144.47	144.47	Open

Armory Equipment		CHOYT				
101-42120-50580	Armory Equipment				144.47	
J.P. MORGAN CHASE BANK NA		04/07/2026	05/26/2026		38.89	38.89 Open
Windshield Nozzle		CHOYT				
101-42260-50220	Windshield Nozzle				38.89	
J.P. MORGAN CHASE BANK NA		04/07/2026	05/26/2026		41.56	41.56 Open
Windshield NOzzle		CHOYT				
101-42260-50220	Windshield NOzzle				41.56	
J.P. MORGAN CHASE BANK NA		04/07/2026	05/26/2026		(41.56)	(41.56) Open
Credit for Windshield Nozzle		CHOYT				
101-42260-50220	Credit for Windshield Nozzle				(41.56)	
J.P. MORGAN CHASE BANK NA		04/08/2026	05/26/2026		124.67	124.67 Open
Screws and Sheetrock for Training Burn		CHOYT				
101-42260-50208	Screws and Sheetrock for Training Burn				124.67	
J.P. MORGAN CHASE BANK NA		04/13/2026	05/26/2026		869.24	869.24 Open
Lucht SCBA Lenses		CHOYT				
101-42260-50217	Lucht SCBA Lenses				869.24	
J.P. MORGAN CHASE BANK NA		04/14/2026	05/26/2026		(843.92)	(843.92) Open
Credit for Mark Lucht Spectacle Kit		CHOYT				
101-42260-50217	Credit for Mark Lucht Spectacle Kit				(843.92)	
J.P. MORGAN CHASE BANK NA		04/24/2026	05/26/2026		1,480.05	1,480.05 Open
FDIC Hotel		CHOYT				
101-42260-50208	FDIC Hotel				1,480.05	
J.P. MORGAN CHASE BANK NA		04/28/2026	05/26/2026		71.05	71.05 Open
Cake and Juice for Badge Pinning		CHOYT				
101-42260-50207	Cake and Juice for Badge Pinning				71.05	

J.P. MORGAN CHASE BANK NA Food Licence 101-41910-50210	Food Licence	03/31/2026 CHOYT	05/26/2026	102.00 102.00	102.00	Open
J.P. MORGAN CHASE BANK NA Cups for Open House 101-41910-50210	Cups for Open House	04/01/2026 CHOYT	05/26/2026	745.15 745.15	745.15	Open
J.P. MORGAN CHASE BANK NA Event Supplies 101-41910-50213	Event Supplies	04/08/2026 CHOYT	05/26/2026	257.91 257.91	257.91	Open
J.P. MORGAN CHASE BANK NA EMPLOYEE BREAKFAST 101-41910-50213	EMPLOYEE BREAKFAST	04/08/2026 CHOYT	05/26/2026	82.20 82.20	82.20	Open
J.P. MORGAN CHASE BANK NA OPEN HOUSE SUPPLIES 101-41910-50210	OPEN HOUSE SUPPLIES	04/19/2026 CHOYT	05/26/2026	196.23 196.23	196.23	Open
J.P. MORGAN CHASE BANK NA OPERATING SUPPLIES PLAYPLACE 101-41910-50210	OPERATING SUPPLIES PLAYPLACE	04/19/2026 CHOYT	05/26/2026	40.79 40.79	40.79	Open
J.P. MORGAN CHASE BANK NA FLUSHING SIGNS 601-49400-50210	FLUSHING SIGNS	04/20/2026 CHOYT	05/26/2026	675.00 675.00	675.00	Open
J.P. MORGAN CHASE BANK NA EASTER EGG SIGNAGE 101-41910-50210	EASTER EGG SIGNAGE	04/20/2026 CHOYT	05/26/2026	198.00 198.00	198.00	Open
J.P. MORGAN CHASE BANK NA		04/20/2026	05/26/2026	120.00	120.00	Open

LEPRACHAUN ON THE LOOSE SIGNS	CHOYT				
101-41910-50210	LEPRACHAUN ON THE LOOSE SIGNS			120.00	
J.P. MORGAN CHASE BANK NA	04/28/2026	05/26/2026		267.81	267.81 Open
MOSQUITO SPRAYING	CHOYT				
101-41910-50210	MOSQUITO SPRAYING			267.81	
J.P. MORGAN CHASE BANK NA	04/28/2026	05/26/2026		267.81	267.81 Open
MOSQUITO SPRAYING	CHOYT				
101-41910-50210	MOSQUITO SPRAYING			267.81	
J.P. MORGAN CHASE BANK NA	04/01/2026	05/26/2026		1,583.94	1,583.94 Open
Elk River Utilities-Feb 2026	CHOYT				
101-41810-50381	Elk River Utilities-Feb 2026			1,583.94	
J.P. MORGAN CHASE BANK NA	04/02/2026	05/26/2026		149.45	149.45 Open
Adam's Pest Control- 12260	CHOYT				
101-41810-50308	Adam's Pest Control- 12260			149.45	
J.P. MORGAN CHASE BANK NA	04/08/2026	05/26/2026		950.40	950.40 Open
ICMA-Z.Doud	CHOYT				
101-41310-50205	ICMA-Z.Doud			950.40	
J.P. MORGAN CHASE BANK NA	04/20/2026	05/26/2026		47.00	47.00 Open
CULLIGAN-12260 APR	CHOYT				
101-41810-50220	CULLIGAN-12260 APR			47.00	
J.P. MORGAN CHASE BANK NA	04/20/2026	05/26/2026		110.10	110.10 Open
CULLIGAN-18461 APR 2026	CHOYT				
101-41910-50220	CULLIGAN-18461 APR 2026			110.10	
J.P. MORGAN CHASE BANK NA	04/20/2026	05/26/2026		42.00	42.00 Open
CULLIGAN-16471 APR 2026	CHOYT				
101-43100-50220	CULLIGAN-16471 APR 2026			42.00	

J.P. MORGAN CHASE BANK NA CULLIGAN-13700 APR 101-41810-50220	CULLIGAN-13700 APR	04/20/2026 CHOYT	05/26/2026	118.00 118.00	118.00	Open
J.P. MORGAN CHASE BANK NA Republic-13700 Mar-Apr 2026 101-42120-50384	Republic-13700 Mar-Apr 2026	04/20/2026 CHOYT	05/26/2026	420.59 420.59	420.59	Open
J.P. MORGAN CHASE BANK NA Republic-13700 Mar-Apr 2026 101-43100-50384	Republic-13700 Mar-Apr 2026	04/20/2026 CHOYT	05/26/2026	420.58 420.58	420.58	Open
J.P. MORGAN CHASE BANK NA Republic-16471 Apr 2026 101-43100-50384	Republic-16471 Apr 2026	04/20/2026 CHOYT	05/26/2026	227.34 227.34	227.34	Open
J.P. MORGAN CHASE BANK NA Republic-18461 Apr 2026 101-41910-50384	Republic-18461 Apr 2026	04/20/2026 CHOYT	05/26/2026	174.58 174.58	174.58	Open
J.P. MORGAN CHASE BANK NA Republic12260 -Apr 2026 101-41810-50384	Republic12260 -Apr 2026	04/20/2026 CHOYT	05/26/2026	426.45 426.45	426.45	Open
JEFFERSON FIRE & SAFETY, INC SUPPLIES-ENFORCER FIRE BULL CLASS A FOAM 101-42260-50200 101-42260-50200	SUPPLIES-ENFORCER FIRE BULL CLASS A FOAM SUPPLIES-IN336922 CREDIT	05/11/2026 CHOYT	05/26/2026	539.70 690.00 (150.30)	0.00	Paid
JESSICA HARTFIEL THURSTON EMBEDDED SOCIAL WORKER- APR 2026 101-42120-50300	EMBEDDED SOCIAL WORKER- APR 2026	05/13/2026 CHOYT	05/26/2026	2,334.00 2,334.00	2,334.00	Open

KWIK TRIP INC	05/13/2026	05/26/2026	150.80	0.00	Paid
PD; 514204/ CARWASH APR 2026	CHOYT				
101-42120-50220	PD; 514204/ CARWASH APR 2026		150.80		

LANDFORM PROFESSIONAL SVCS, LLC	05/13/2026	05/26/2026	8,855.00	8,855.00	Open
PLANNING; CITY BUSINESS APR 2026	CHOYT				
101-41710-50300	PLANNING; CITY BUSINESS APR 2026		8,855.00		

LEAGUE OF MN CITIES-INSURANCE	05/13/2026	05/26/2026	674.36	674.36	Open
GENERAL & WKR COMP CLAIM #00530025	CHOYT				
101-43100-50361	GENERAL & WKR COMP CLAIM #00530025		674.36		

LENNAR	05/11/2026	05/26/2026	3,000.00	3,000.00	Open
11288 KINGSVIEW LN N LANDSCAPE ESCROW RE	CHOYT				
420-00000-22100	11288 KINGSVIEW LN N LANDSCAPE ESCROW RE		3,000.00		

LENNAR	05/11/2026	05/26/2026	3,000.00	3,000.00	Open
11268 KINGSVIEW LN N LANDSCAPE ESCROW RE	CHOYT				
420-00000-22100	11268 KINGSVIEW LN N LANDSCAPE ESCROW RE		3,000.00		

LENNAR	05/11/2026	05/26/2026	3,000.00	3,000.00	Open
11278 KINGSVIEW LN N LANDSCAPE ESCROW RE	CHOYT				
420-00000-22100	11278 KINGSVIEW LN N LANDSCAPE ESCROW RE		3,000.00		

LENNAR	05/11/2026	05/26/2026	3,000.00	3,000.00	Open
11077 HARBOR LN N LANDSCAPE ESCROW RE	LE CHOYT				
420-00000-22100	11077 HARBOR LN N LANDSCAPE ESCROW RE		3,000.00		

LENNAR	05/11/2026	05/26/2026	3,000.00	3,000.00	Open
11248 KINGSVIEW LN N LANDSCAPE ESCROW RE	CHOYT				
420-00000-22100	11248 KINGSVIEW LN N LANDSCAPE ESCROW RE		3,000.00		

LENNAR	05/11/2026	05/26/2026	3,000.00	3,000.00	Open
11258 KINGSVIEW LN N LANDSCAPE ESCROW RE	CHOYT				

420-00000-22100	11258 KINGSVIEW LN N LANDSCAPE ESCROW RE			3,000.00		
M/I HOMES OF MPLS		05/18/2026	05/26/2026	3,000.00	3,000.00	Open
	14367 KINGSVIEW LN N LANDSCAPE ESCROW RE CHOYT					
420-00000-22100	14367 KINGSVIEW LN N LANDSCAPE ESCROW RE			3,000.00		
M/I HOMES OF MPLS		05/18/2026	05/26/2026	3,000.00	3,000.00	Open
	14371 KINGSVIEW LN N LANDSCAPE ESCROW RE CHOYT					
420-00000-22100	14371 KINGSVIEW LN N LANDSCAPE ESCROW RE			3,000.00		
M/I HOMES OF MPLS		05/18/2026	05/26/2026	3,000.00	3,000.00	Open
	14375 KINGSVIEW LN N LANDSCAPE ESCROW RE CHOYT					
420-00000-22100	14375 KINGSVIEW LN N LANDSCAPE ESCROW RE			3,000.00		
M/I HOMES OF MPLS		05/18/2026	05/26/2026	3,000.00	3,000.00	Open
	14383 KINGSVIEW LN N LANDSCAPE ESCROW RE CHOYT					
420-00000-22100	14383 KINGSVIEW LN N LANDSCAPE ESCROW RE			3,000.00		
M/I HOMES OF MPLS		05/18/2026	05/26/2026	3,000.00	3,000.00	Open
	14418 KINGSVIEW LN N LANDSCAPE ESCROW RE CHOYT					
420-00000-22100	14418 KINGSVIEW LN N LANDSCAPE ESCROW RE			3,000.00		
M/I HOMES OF MPLS		05/18/2026	05/26/2026	3,000.00	3,000.00	Open
	14887 144TH AVE N LANDSCAPE ESCROW RELEA CHOYT					
420-00000-22100	14887 144TH AVE N LANDSCAPE ESCROW RELEA			3,000.00		
M/I HOMES OF MPLS		05/18/2026	05/26/2026	3,000.00	3,000.00	Open
	14937 144TH AVE N LANDSCAPE ESCROW RELEA CHOYT					
420-00000-22100	14937 144TH AVE N LANDSCAPE ESCROW RELEA			3,000.00		
M/I HOMES OF MPLS		05/18/2026	05/26/2026	3,000.00	3,000.00	Open
	14957 144TH AVE N LANDSCAPE ESCROW RELEA CHOYT					
420-00000-22100	14957 144TH AVE N LANDSCAPE ESCROW RELEA			3,000.00		

M/I HOMES OF MPLS	05/18/2026	05/26/2026	3,000.00	3,000.00	Open
14987 144TH AVE N LANDSCAPE ESCROW RELEA CHOYT					
420-00000-22100	14987 144TH AVE N LANDSCAPE ESCROW RELEA		3,000.00		

M/I HOMES OF MPLS	05/18/2026	05/26/2026	3,000.00	3,000.00	Open
14947 144TH AVE N LANDSCAPE ESCROW RELEA CHOYT					
420-00000-22100	14947 144TH AVE N LANDSCAPE ESCROW RELEA		3,000.00		

M/I HOMES OF MPLS	05/18/2026	05/26/2026	3,000.00	3,000.00	Open
14940 144TH AVE N LANDSCAPE ESCROW RELEA CHOYT					
420-00000-22100	14940 144TH AVE N LANDSCAPE ESCROW RELEA		3,000.00		

M/I HOMES OF MPLS	05/18/2026	05/26/2026	3,000.00	3,000.00	Open
14915 RIVERVIEW LN N LANDSCAPE ESCROW RE CHOYT					
420-00000-22100	14915 RIVERVIEW LN N LANDSCAPE ESCROW RE		3,000.00		

M/I HOMES OF MPLS	05/18/2026	05/26/2026	3,000.00	3,000.00	Open
14996 RIVERVIEW LN N LANDSCAPE ESCROW RE CHOYT					
420-00000-22100	14996 RIVERVIEW LN N LANDSCAPE ESCROW RE		3,000.00		

M/I HOMES OF MPLS	05/18/2026	05/26/2026	3,000.00	3,000.00	Open
14986 RIVERVIEW LN N LANDSCAPE ESCROW RE CHOYT					
420-00000-22100	14986 RIVERVIEW LN N LANDSCAPE ESCROW RE		3,000.00		

M/I HOMES OF MPLS	05/18/2026	05/26/2026	3,000.00	3,000.00	Open
14976 RIVERVIEW LN N LANDSCAPE ESCROW RE CHOYT					
420-00000-22100	14976 RIVERVIEW LN N LANDSCAPE ESCROW RE		3,000.00		

M/I HOMES OF MPLS	05/18/2026	05/26/2026	3,000.00	3,000.00	Open
14926 RIVERVIEW LN N LANDSCAPE ESCROW RE CHOYT					
420-00000-22100	14926 RIVERVIEW LN N LANDSCAPE ESCROW RE		3,000.00		

METROPOLITAN COUNCIL	05/18/2026	05/26/2026	68,884.20	0.00	Paid
SAC FEE; APR 2026	CHOYT				

602-00000-20801	SAC FEE; APR 2026			69,580.00		
602-49450-37270	LESS PROMPT PAYMENT FEE; APR			(695.80)		
MHSRC/RANGE		05/11/2026	05/26/2026	1,310.00	1,310.00	Open
PD; PROFESSIONAL DEVELOPMENT X2		CHOYT				
101-42120-50208	PROFESSIONAL DEVELOPMENT X2			1,310.00		
MINUTEMAN PRESS		05/13/2026	05/26/2026	2,553.94	2,553.94	Open
UTILITY BILLING; MAY 2026		CHOYT				
601-49400-50200	UTILITY BILLING; MAY 2026			1,276.97		
602-49400-50200	UTILITY BILLING; MAY 2026			1,276.97		
MINUTEMAN PRESS		05/13/2026	05/26/2026	59.50	59.50	Open
PW; OPERATING SUPPLIES-M FARRELL CARDS		CHOYT				
101-43100-50210	PW; OPERATING SUPPLIES M FARRELL CARDS			59.50		
MONTICELLO ANIMAL CONTROL		05/13/2026	06/02/2026	55.00	55.00	Open
PD; ANIMAL CONTROL		CHOYT				
101-42140-50308	PD; ANIMAL CONTROL			55.00		
MOTOROLA, INC		05/13/2026	05/26/2026	1,500.00	1,500.00	Open
PD; CONTRACT SERVICES OCT 2026 -OCT 2027		CHOYT				
101-42120-50308	PD; CONTRACT SERVICES OCT -OCT 2027			1,500.00		
PACHAL, JEREMY		05/11/2026	05/26/2026	144.62	144.62	Open
UB refund for account: 1097		CHOYT				
601-00000-15550	CREDIT FORWARD			144.62		
RUM RIVER COUNSELING, INC		05/18/2026	05/26/2026	2,125.00	2,125.00	Open
WELLNESS PROGRAM		CHOYT				
101-42120-50392	WELLNESS PROGRAM			2,125.00		
SIMMER BROTHER HOMES		05/18/2026	05/26/2026	3,000.00	3,000.00	Open
14905 142ND AVE N LANDSCAPE ESCROW RELEA		CHOYT				

420-00000-22100	14905 142ND AVE N LANDSCAPE ESCROW RELEA			3,000.00		
SITE ONE LANDSCAPE SUPPLY PW; SUPPLIES		05/13/2026	05/26/2026	207.18	207.18	Open
101-45200-50220	PW; SUPPLIES	CHOYT		207.18		
TRANSPORT GRAPHICS OTHER EQUIPMENT-GRAPHICS		05/18/2026	05/26/2026	967.00	967.00	Open
401-42120-50580	OTHER EQUIPMENT-GRAPHICS	CHOYT		967.00		
USPS POSTAGE ORDER 2026		05/20/2026	05/26/2026	3,200.00	3,200.00	Open
601-49400-50322	UB; POSTAGE	CHOYT		400.00		
602-49400-50322	UB; POSTAGE			300.00		
101-41810-50322	CH; POSTAGE			2,500.00		
VEIT & COMPANY INC. CLEAN-UP DAY 2026		05/19/2026	05/26/2026	4,493.78	4,493.78	Open
101-41650-50388	CLEAN-UP DAY 2026	CHOYT		4,493.78		
VERIZON WIRELESS PW;CELL SERVICE;MCM SEWER APR-MAY 2026		05/20/2026	05/26/2026	150.12	150.12	Open
602-49400-50321	PW;CELL SERVICE;MCM SEWER	CHOYT		150.12		
WATER LABORATORIES, INC WATER TESTING; APR 2026		05/13/2026	05/26/2026	685.20	685.20	Open
601-49400-50300	WATER TESTING; APR 2026	CHOYT		685.20		
XCEL ENERGY 51-0015605920-3 11301 DAYTON PKWY TRAF. SK		05/13/2026	05/26/2026	34.42	0.00	Paid
101-41810-50381	51-0015605920-3 11301 DAYTON PKWY	CHOYT		34.42		
XCEL ENERGY 51-0013923150-3;HOLLY LN APR 2026		05/14/2026	05/26/2026	38.39	0.00	Paid
		CHOYT				

101-43100-50230	51-0013923150-3;HOLLY LN APR 2026			38.39	
XCEL ENERGY		05/14/2026	05/26/2026	56.20	0.00 Paid
51-0013433451-8;BROCKTON LGT APR 2026	CHOYT				
101-43100-50230	51-0013433451-8;BROCKTON LGT			56.20	
XCEL ENERGY		05/14/2026	05/26/2026	33.24	0.00 Paid
51-0014297205-1;14641 U.PASS W/RH PKWY	APR CHOYT				
101-43100-50230	51-0014297205-1;14641 U.PASS W/RH PKWY			33.24	
XCEL ENERGY		05/14/2026	05/26/2026	23.99	0.00 Paid
51-0013433188-8; 18432 UNIT SIGNAL APR 2026	CHOYT				
101-43100-50230	51-0013433188-8; 18432 UNIT SIGNAL			23.99	
XCEL ENERGY		05/18/2026	05/26/2026	26.03	0.00 Paid
51-0014444653-6;14666 146TH AVE S.L. APR 202	CHOYT				
101-43100-50230	51-0014444653-6;14666 146TH AVE S.L.			26.03	
XCEL ENERGY		05/18/2026	05/26/2026	26.03	0.00 Paid
51-0014444656-9;14748 CHESHIRE CT S.L. APR 2	CHOYT				
101-43100-50230	51-0014444656-9;14748 CHESHIRE CT S.L.			26.03	
XCEL ENERGY		05/18/2026	05/26/2026	22.77	0.00 Paid
51-6970693-8;17320 DAYTON SHED APR 2026	CHOYT				
101-41810-50381	51-6970693-8;17320 DAYTON SHED			22.77	
XCEL ENERGY		05/18/2026	05/26/2026	33.63	0.00 Paid
51-0013985527-8; CHESHIRE LGT APR 2026	CHOYT				
101-43100-50230	51-0013985527-8; CHESHIRE LGT			33.63	
XCEL ENERGY		05/18/2026	05/26/2026	3,732.69	0.00 Paid
51-0011857801-8;PD/PW BLDG APR 2026	CHOYT				
101-41810-50381	51-0011857801-8;PD/PW BLDG			3,732.69	

XCEL ENERGY	05/18/2026	05/26/2026	0.17	0.00	Paid
51-0014712973-2; 18160 SIREN APR 2026	CHOYT				
101-41810-50381	51-0014712973-2; 18160 SIREN		0.17		

XCEL ENERGY	05/18/2026	05/26/2026	2,774.45	0.00	Paid
51-0013565432-4; 14695 RIVER/WELLHOUSE API	CHOYT				
601-49400-50381	51-0013565432-4; 14695 RIVER/WELLHOUSE		2,774.45		

XCEL ENERGY	05/18/2026	05/26/2026	31.49	0.00	Paid
51-0013211437-0;SDL TRAIL LIFT APR 2026	CHOYT				
601-49400-50381	51-0013211437-0;SDL TRAIL LIFT		31.49		

XCEL ENERGY	05/18/2026	05/26/2026	50.57	0.00	Paid
51-0014473382-9 12000 1/2 W FRENCH APR-MA`	CHOYT				
101-43100-50230	51-0014473382-9 12000 1/2 W FRENCH		50.57		

XCEL ENERGY	05/19/2026	05/26/2026	54.61	0.00	Paid
51-0013348079-5;14430 DAYTON RIVER APR-MA`	CHOYT				
101-45200-50381	51-0013348079-5;14430 DAYTON RIVER		54.61		

XCEL ENERGY	05/19/2026	05/26/2026	27.81	0.00	Paid
51-0015591826-7; 14642 146TH/IRR APR-MAY 20	CHOYT				
601-49400-50381	51-0015591826-7; 14642 146TH/IRR		27.81		

# of Invoices:	219	# Due: 188	Totals:	292,480.22	206,248.13
# of Credit Memos:	2	# Due: 2	Totals:	(885.48)	(885.48)
Net of Invoices and Credit Memos:				291,594.74	205,362.65
* 2 Net Invoices have Credits Totalling:				(846.10)	

--- TOTALS BY PAYMENT CARD ACCOUNT ---					
0843			2,536.61		
0983			2,129.45		
1860			440.10		
2363			581.95		

2499	15.17
3028	4,670.43
3212	470.92
3240	2,952.90
3356	3,632.46
3926	6,579.03
4473	183.28
4971	302.50
4983	1,071.63
5639	1,778.28
8767	1,392.05
9053	1,739.98

--- TOTALS BY FUND ---

101 - GENERAL FUND	89,780.95	75,397.25
225 - EDA	470.00	470.00
226 - CABLE	1,000.00	1,000.00
401 - CAPITAL EQUIPMENT	967.00	967.00
420 - LANDSCAPE ESCROWS	81,000.00	81,000.00
601 - WATER FUND	47,765.50	44,801.31
602 - SEWER FUND	70,611.29	1,727.09

--- TOTALS BY DEPT/ACTIVITY ---

00000 -	151,549.62	81,969.62
41110 - Council	134.54	134.54
41310 - Administration	1,063.68	1,063.68
41410 - Elections	105.00	105.00
41420 - City Clerk	540.00	540.00
41500 - Finance	52.09	52.09
41640 - Legal Services	319.50	319.50

41650 - Recycling Services	5,869.03	5,869.03
41710 - Plannning & Economic Dev	9,627.50	9,627.50
41810 - Central Services	16,293.11	8,549.88
41820 - Information Technology	2,716.33	2,116.35
41900 - General Govt	1,000.00	1,000.00
41910 - Activity Center	6,226.87	6,226.87
42120 - Patrol and Investigate	18,241.31	14,945.73
42140 - Animal Control	55.00	55.00
42260 - Fire Suppression	13,490.60	11,885.76
43100 - Public Works	15,396.60	14,311.14
45200 - Parks	261.79	207.18
49400 - Utilities	49,347.97	46,383.78
49450 - Sewer	(695.80)	0.00

ITEM:

2026 Dayton Survey Results

PREPARED BY:

Zach Doud, City Administrator

POLICY DECISION / ACTION TO BE CONSIDERED:

The City Council approved a community survey on April 14, 2026. The results have been compiled and a presentation by the Morris Leatherman Company will occur at this meeting. There were 400 residents surveyed during this process which is a statistical number used by numerous communities and has a plus/minus 5% percent error. This survey is a professional company who has completed surveys for numerous cities, school districts, and counties for years and has a proven track record of success based on their scientific process that is used. More information will be provided on the presentation.

Additionally, the presentation will have comparisons to other communities so we can compare where we are at versus other communities that have had this survey completed.

CRITICAL ISSUES:

There are no outstanding issues.

RELATIONSHIP TO COUNCIL GOALS:

Promote a Safe and Welcoming Community

RECOMMENDATION:

Staff recommends City Council accept the results of the Community Survey.

ATTACHMENT(S):

2026 City of Dayton Survey Results

THE MORRIS LEATHERMAN COMPANY
3128 Dean Court
Minneapolis, Minnesota 55416

CITY OF DAYTON
RESIDENTIAL SURVEY
FINAL APRIL 2026

Hello, I'm _____ of the Morris Leatherman Company, a polling firm located in Minneapolis. We've been hired by the City of Dayton to speak with a random sample of residents about issues facing the City. The survey is being taken because your local elected leaders and city staff are interested in your opinions and suggestions. I want to assure you that all individual responses will be held strictly confidential; only summaries of the entire sample will be reported. (DO NOT PAUSE)

- | | |
|--|-------------------------------------|
| 1. Approximately, how many years have you lived in the City of Dayton? | FIVE YEARS OR LESS....17% |
| | SIX TO TEN YEARS.....27% |
| | ELEVEN TO TWENTY YRS..27% |
| | 21 TO 30 YEARS.....15% |
| | OVER THIRTY YEARS.....14% |
| | DON'T KNOW/REFUSED.....0% |
| 2. Thinking back to when you moved to Dayton, what factors were most important to you in selecting the City? | DON'T KNOW/REFUSED.....0% |
| | HOUSING/LAND.....20% |
| | SCHOOLS.....11% |
| | SAFE.....8% |
| | CLOSE TO FAMILY/
FRIENDS.....18% |
| | SMALL COMMUNITY.....7% |
| | PARKS/TRAILS.....3% |
| | CONVENIENT LOCATION....6% |
| | QUIET AND PEACEFUL....12% |
| | RURAL/OPEN SPACE.....5% |
| | CLOSE TO JOB.....10% |
| | SCATTERED.....1% |
| 3. How would you rate the quality of Life in the City of Dayton - excellent, good, only fair, or poor? | EXCELLENT.....33% |
| | GOOD.....61% |
| | ONLY FAIR.....5% |
| | POOR.....0% |
| | DON'T KNOW/REFUSED.....1% |

- | | | |
|----|---|--|
| 4. | What do you like most about living the City of Dayton? | DON'T KNOW/REFUSED.....0%
HOUSING/LAND.....12%
SCHOOLS.....11%
SAFE.....9%
CLOSE TO FAMILY
/FRIENDS.....13%
SMALL COMMUNITY.....7%
PARKS/TRAILS.....6%
CONVENIENT LOCATION....4%
QUIET/PEACEFUL.....14%
RURAL/OPEN SPACE.....4%
CLOSE TO JOB.....3%
SMALL TOWN FEEL.....8%
FRIENDLY PEOPLE.....8%
SCATTERED.....2% |
| 5. | What do you think is the most serious issue facing the City of Dayton today? | DON'T KNOW/REFUSED.....3%
NOTHING.....28%
TOO MUCH GROWTH.....21%
HIGH TAXES.....14%
LACK OF JOBS.....10%
SPEEDING/RECKLESS
DRIVING.....9%
STREET MAINTENANCE.....6%
RISING CRIME.....3%
LACK OF CITY SERVICES..2%
SCATTERED.....4% |
| 6. | All in all, do you think things in the City of Dayton are headed in the right direction, or do you think things are off on the wrong track? | RIGHT DIRECTION.....74%
WRONG TRACK.....25%
DON'T KNOW/REFUSED.....2% |

IF "WRONG TRACK," ASK: (n=99)

- | | | |
|----|---|--|
| 7. | Why do you think things have gotten off on the wrong track? | DON'T KNOW/REFUSED.....0%
TOO MUCH GROWTH.....34%
HIGH TAXES.....13%
RISING CRIME.....13%
ECONOMY/INFLATION.....28%
POOR DEVELOPMENT
PLANNING.....8%
ECONOMY/INFLATION.....2%
SCATTERED.....2% |
|----|---|--|

- | | | |
|-----|---|--|
| 8. | Do you see the City of Dayton as a small town or a suburb? | SMALL TOWN.....49%
SUBURB.....24%
BOTH (VOL.).....27%
DON'T KNOW/REFUSED.....0% |
| 9. | How would you rate the sense of community identity among residents in Dayton - would you say it is very strong, somewhat strong, not too strong, or not at all strong? | VERY STRONG.....35%
SOMEWHAT STRONG.....56%
NOT TOO STRONG.....9%
NOT AT ALL STRONG.....1%
DON'T KNOW/REFUSED.....0% |
| 10. | Please tell me which of the following do you feel the closest connection - the City of Dayton as a whole, your neighborhood, your school district, or something else? (IF "SOMETHING ELSE," ASK:) What would that be? | DON'T KNOW/REFUSED.....1%
CITY OF DAYTON.....30%
NEIGHBORHOOD.....59%
SCHOOL DISTRICT.....9%
SCATTERED.....2% |

Let's discuss the future of the City of Dayton...

- | | | |
|-----|---|--|
| 11. | When thinking about a city's quality of life, what do you think is the most important aspect of that quality? | DON'T KNOW/REFUSED.....0%
SMALL TOWN FEEL.....19%
SAFETY.....32%
RURAL/OPEN SPACES.....11%
PARKS/TRAILS.....5%
SCHOOLS.....7%
SENSE OF COMMUNITY.....18%
AFFORDABLE LIVING.....7%
SCATTERED.....2% |
| 12. | What, if anything, is currently missing from the City of Dayton which, if present, would greatly improve the quality of life for residents? | DON'T KNOW/REFUSED.....3%
NOTHING.....26%
MORE CITY SERVICES....11%
BETTER STREET
MAINTENANCE.....13%
JOBS.....10%
LOWER TAXES.....13%
PARK/TRAILS.....4%
SHOPPING/
ENTERTAINMENT.....10%
RESTAURANTS.....4%
SCATTERED.....6% |

I would now like to read a list of characteristics which are a part of the overall quality of life in a community. First, for each one tell me if it is a very important aspect of quality of life, a somewhat important aspect, a not too important aspect or not at all important aspect of quality of life.

	VIM	SIM	NTI	NAA	DKR
13. City parks?	51%	42%	6%	1%	0%
14. City trails?	50%	44%	5%	1%	0%
15. Recreational programs?	26%	58%	14%	2%	0%
16. Schools?	65%	26%	5%	4%	0%
17. Public open spaces?	46%	46%	9%	0%	0%
18. Public safety?	90%	10%	0%	0%	0%
19. Community events?	32%	58%	10%	0%	0%
20. Public transportation?	23%	45%	25%	8%	0%
21. Retail shopping opportunities?	38%	45%	15%	2%	0%
22. Dining and entertainment opportunities?	41%	47%	11%	1%	0%

Now for each one, please rate the City of Dayton on that characteristic as excellent, good, only fair, or poor.

	EXC	GOO	FAI	POO	DKR
23. City parks?	32%	59%	9%	0%	0%
24. City trails?	35%	53%	11%	0%	1%
25. Recreational programs?	19%	59%	17%	1%	5%
26. Schools?	36%	54%	6%	1%	4%
27. Public open spaces?	30%	61%	10%	0%	0%
28. Public safety?	44%	49%	7%	0%	0%
29. Community events?	20%	62%	18%	0%	1%
30. Public transportation?	8%	31%	50%	9%	3%
31. Retail shopping opportunities?	9%	45%	32%	14%	0%
32. Dining and entertainment opportunities?	9%	46%	31%	15%	0%

Turning to city services....

33. Do you think the quality of city services has been able to keep pace with growth in Dayton?	YES.....60%
	NO.....37%
	DON'T KNOW/REFUSED.....3%

IF "NO," ASK: (n=148)

34. What services, in particular, have not been able to keep pace?	DON'T KNOW/REFUSED.....1%
	STREET MAINTENANCE....33%
	PARKS/TRAILS.....5%
	POLICE.....9%
	FIRE.....10%
	DEVELOPMENT/PLANNING..36%
	TRANSIT.....2%
	SCATTERED.....4%

I would like to read you a list of a few city services. For each one, please tell me whether you would rate the quality of the service as excellent, good, only fair, or poor?

	EXC	GOO	FAI	POO	DKR
35. Police protection?	46%	48%	6%	0%	0%
36. Fire protection?	47%	44%	9%	0%	1%
37. Storm water run-off and drainage?	14%	54%	30%	1%	1%
38. Park maintenance?	25%	55%	20%	1%	0%
39. Park facilities?	29%	58%	13%	0%	0%
40. Administration?	24%	60%	14%	2%	0%
41. Utility billing?	20%	61%	16%	4%	0%
42. Building inspections?	11%	58%	23%	1%	8%
43. City drinking water?	21%	51%	21%	7%	1%
44. City planning?	24%	35%	34%	5%	3%
45. Communications?	19%	62%	14%	3%	2%

For the next two city services, please consider only their job on city-maintained streets and roads in neighborhoods. That means you should exclude state and county roads, such as Dayton River Road and Fernbrook Lane, that are taken care of by other levels of government and private roads in neighborhoods. Keeping that in mind, would you rate each of the following as excellent, good, only fair, or poor.....

	EXC	GOO	FAI	POO	DKR
46. Snowplowing of city streets?	26%	52%	21%	2%	0%
47. City street maintenance?	19%	43%	34%	5%	0%

I am going to re-read the list of city services. For each service, please tell me if you would support an increase in funding for the service, keep the funding for the service at its current level, make cuts in the funding for the service, or eliminate funding for the service. (ROTATE)

	INC	KEE	CUT	ELM	DKR
48. Police protection?	38%	62%	0%	0%	0%
49. Fire protection?	40%	60%	1%	0%	0%
50. Storm water run-off and drainage?	19%	68%	12%	0%	1%
51. Park maintenance?	20%	74%	7%	0%	0%
52. Park facilities?	17%	76%	7%	0%	0%
53. Administration?	9%	79%	12%	0%	1%
54. Utility billing?	5%	80%	13%	0%	2%
55. Building inspections?	7%	69%	19%	1%	5%
56. City drinking water?	47%	52%	1%	0%	0%
57. City planning?	38%	55%	6%	1%	0%

	INC	KEE	CUT	ELM	DKR	
58. Communications?	17%	72%	10%	2%	0%	
59. Snowplowing of city streets?	38%	61%	1%	0%	0%	
60. City street maintenance?	55%	44%	2%	0%	0%	
61. Do you consider city property taxes in the City of Dayton to be very high, somewhat high, about average, somewhat low, or very low?	VERY HIGH.....	18%	SOMEWHAT HIGH.....	45%	ABOUT AVERAGE.....	33%
	SOMEWHAT LOW.....	2%	VERY LOW.....	0%	DON'T KNOW/REFUSED.....	3%
62. When you consider the property taxes you pay and the quality of city services you receive, would you rate the general value of city services as excellent, good, only fair, or poor?	EXCELLENT.....	12%	GOOD.....	70%	ONLY FAIR.....	15%
	POOR.....	1%	DON'T KNOW/REFUSED.....	2%		

Let's discuss police and fire service in the City of Dayton....

63. Over the past few years, have you had any interaction with the Dayton Police Department?	YES.....	25%	NO.....	75%	DON'T KNOW/REFUSED.....	0%
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IF "YES," ASK: (n=98)

64. How would you rate your experience with the police department - excellent, good, only fair, or poor?	EXCELLENT.....	35%	GOOD.....	56%	ONLY FAIR.....	9%
	POOR.....	0%	DON'T KNOW/REFUSED.....	0%		
65. Over the past few years, have you had any interaction with the Dayton Fire Department?	YES.....	18%	NO.....	82%	DON'T KNOW/REFUSED.....	1%

IF "YES," ASK: (n=71)

66. How would you rate your experience with the fire department - excellent, good, only fair, or poor?	EXCELLENT.....	55%	GOOD.....	41%	ONLY FAIR.....	4%
	POOR.....	0%	DON'T KNOW/REFUSED.....	0%		

As residential growth continues, a study done for the City determined there will be a need in the near future to relocate a fire station to the southern area of the city. This station would

allow space to grow the department and lower response times across the city.

67. How much would you be willing to see your property taxes increase to fund the relocation of a fire station, would you be willing to see your property taxes increase by \$__ a month or \$__ a year? (CHOOSE RANDOM STARTING POINT; MOVE UP OR DOWN DEPENDING ON RESPONSE) How about \$__ per month or \$__ a year?
- | | |
|-------------------------|-----|
| NOTHING..... | 21% |
| \$10/\$120..... | 47% |
| \$20/\$240..... | 21% |
| \$30/\$360..... | 4% |
| \$40/\$480..... | 1% |
| \$50/\$600..... | 0% |
| OVER \$50/\$600..... | 0% |
| DON'T KNOW/REFUSED..... | 7% |

Most communities have one of two systems for trash collection. Dayton has an open collection system, where residents choose any of the haulers licensed by the City. Some cities use a collection system in which the City manages the system, negotiates prices and standardizes services for residential trash collection.

68. Would you support or oppose the City of Dayton changing from the current system in which residents choose their trash hauler to a system where the City manages collection? (WAIT FOR RESPONSE) Do you feel strongly that way?
- | | |
|-------------------------|-----|
| STRONGLY SUPPORT..... | 9% |
| SUPPORT..... | 38% |
| OPPOSE..... | 32% |
| STRONGLY OPPOSE..... | 13% |
| DON'T KNOW/REFUSED..... | 8% |

IF A RESPONSE IS GIVEN, ASK: (n=367)

69. Could you tell me one or two reasons for your decision?
- | | |
|--------------------------|-----|
| DON'T KNOW/REFUSED..... | 0% |
| SUPPORT/LOWER COST.... | 23% |
| SUPPORT/BETTER SERVICE.. | 8% |
| SUPPORT/BETTER FOR | |
| STREET MAINTENANCE.. | 11% |
| SUPPORT/LESS TRUCK | |
| TRAFFIC..... | 10% |
| OPPOSE/WANT CHOICE.... | 26% |
| OPPOSE/LIKE CURRENT | |
| HAULER..... | 15% |
| OPPOSE/LOWER COST..... | 2% |
| OPPOSE/BETTER SERVICE.. | 4% |
| SCATTERED..... | 2% |

I would like to read a list of aspects of trash collection. For each one, please tell me if you think it is better under an open collection system or a City-managed collection system. (WAIT FOR RESPONSE) Do you feel strongly that way? If you have no opinion, just say so.... (ROTATE)

	STO	OPE	CIT	STC	DKR
70. Cost for trash collection?	20%	40%	24%	10%	7%
71. Pollution from trucks?	8%	27%	43%	14%	9%
72. Customer service?	23%	38%	24%	9%	7%
73. Pedestrian safety?	16%	29%	33%	12%	11%
74. Wear and tear on city streets?	9%	27%	40%	14%	10%

Changing topics....

75. Are there any types of development you would like to see in the city? (IF "YES," ASK:) What are they?	DON'T KNOW/REFUSED.....4%
	NO.....32%
	RETAIL/SERVICE
	BUSINESSES.....19%
	PARKS/TRAILS.....7%
	JOB-PRODUCING.....14%
	ENTERTAINMENT.....9%
	RESTAURANTS.....10%
	SCATTERED.....5%
76. Are there any types of development you would strongly oppose?	DON'T KNOW/REFUSED.....3%
	NO.....45%
	ANY HOUSING.....8%
	BIG-BOX RETAIL.....12%
	APARTMENTS.....8%
	AFFORDABLE HOUSING.....7%
	HIGH DENSITY HOUSING..14%
	SCATTERED.....3%

As the City of Dayton continues development....

77. Do you support or oppose the City providing financial incentives to attract specific types of development? (WAIT FOR RESPONSE) Do you feel strongly that way?	STRONGLY SUPPORT.....12%
	SUPPORT.....54%
	OPPOSE.....20%
	STRONGLY OPPOSE.....7%
	DON'T KNOW/REFUSED.....8%

I would like to read you a list of characteristics of a community. For each one, please tell me if you think City of Dayton currently has too many or too much, too few or too little, or about the right amount.

	MANY /MCH	FEW/ LITT	ABT RGHT	DK/ REFD
78. Manufactured or mobile homes?	11%	29%	60%	1%
79. Rental apartments?	17%	30%	53%	1%
80. Townhouses?	9%	29%	61%	1%
81. Starter homes under \$300,000?	6%	47%	46%	1%

	MANY /MCH	FEW/ LITT	ABT RGHT	DK/ REFD
82. "Move up" housing between \$300,000 to \$750,000?	21%	9%	70%	1%
83. High-end housing over \$750,000?	29%	13%	58%	1%
84. Hobby farms and large lots over one and a half acres?	3%	41%	55%	1%
85. Assisted living?	3%	46%	48%	4%
86. One-level senior housing maintained by an association?	3%	57%	36%	4%
87. Parks and open spaces?	14%	12%	74%	0%
88. Trails and bikeways?	12%	18%	70%	0%
89. Job producing businesses?	2%	47%	51%	1%
90. Service and retail establishments?	1%	44%	55%	0%
91. Entertainment establishments?	2%	45%	53%	0%
92. Restaurants?	2%	45%	54%	0%

Increased housing density involves building more housing units in a specific area designed to accommodate a higher number of people.

93. How would you rate the importance of increasing housing density through- out the city - very important, some- what important, not too important, or not at all important?	VERY IMPORTANT.....5%	SOMEWHAT IMPORTANT....42%	NOT TOO IMPORTANT.....31%	NOT AT ALL IMPORTANT..19%	DON'T KNOW/REFUSED.....3%
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Changing topics....

94. How would you rate park and recreation amenities in the City of Dayton - excellent, good, only fair, or poor?	EXCELLENT.....19%	GOOD.....73%	ONLY FAIR.....7%	POOR.....1%	DON'T KNOW/REFUSED.....1%
95. Does the current mix of park and recreational amenities in the city adequately meet the needs of your household?	YES.....85%	NO.....11%	DON'T KNOW/REFUSED.....4%		

IF "NO," ASK: (n=44)

96.	What additional park and recreational amenities would you like to see offered?	DON'T KNOW/REFUSED.....0% MORE PARKS.....14% MORE TRAILS.....50% COMMUNITY CENTER.....18% SPLASH PAD.....9% ATHLETIC FIELDS.....7% FISHING PIER.....2%
97.	How important are park and recreational facilities to your overall quality of life in Dayton - is it very important, somewhat important, not too important, or not at all important?	VERY IMPORTANT.....39% SOMEWHAT IMPORTANT....47% NOT TOO IMPORTANT.....13% NOT AT ALL IMPORTANT...2% DON'T KNOW/REFUSED.....0%
98.	How important is the quality and appearance of city park and recreational facilities to the value of your home - very important, somewhat important, not too important, or not at all important?	VERY IMPORTANT.....34% SOMEWHAT IMPORTANT....54% NOT TOO IMPORTANT.....10% NOT AT ALL IMPORTANT...1% DON'T KNOW/REFUSED.....1%
99.	During the past year, have you or members of your household visited Elsie Stephens Park on the Mississippi River? (IF "YES," ASK:) How would you rate Elsie Stephens Park - excellent, good, only fair, or poor?	NO.....46% YES/EXCELLENT.....13% YES/GOOD.....37% YES/ONLY FAIR.....4% YES/POOR.....0% DON'T KNOW/REFUSED.....0%
100.	During the past year, have you or members of your household visited the Elm Creek Park Reserve?	YES.....53% NO.....46% DON'T KNOW/REFUSED.....1%

IF "YES," ASK: (n=213)

101.	Was the City of Dayton's proximity to the Elm Creek Park Preserve a major factor in your decision to move to Dayton, a minor factor, or was it not a factor all?	MAJOR FACTOR.....15% MINOR FACTOR.....25% NOT A FACTOR.....60% DON'T KNOW/REFUSED.....0%
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Over the past few years, the City of Dayton has conducted a system-wide review of the city's parks and trails system. The purpose of this review was to determine trends and community priorities for making improvements to the parks and trails system.

For each of the following, please tell me if you would strongly support a property tax increase for that purpose, somewhat support, somewhat oppose, or strongly oppose a property tax increase for that purpose. (ROTATE)

	STS	SMS	SMO	STO	DKR
102. Additional pedestrian and bicycling trails?	30%	38%	23%	9%	1%
103. Additional horseback riding trails?	14%	36%	29%	18%	3%
104. Additional snowmobiling trails?	16%	33%	29%	20%	3%
105. Construction of a splash pad?	27%	34%	25%	13%	1%
106. Construction of an indoor sports complex, including a fieldhouse or dome with artificial turf athletic fields?	19%	39%	26%	15%	2%
107. Pickleball courts?	20%	46%	23%	9%	2%
108. Construction of a large outdoor community park with athletic fields and playground?	24%	44%	21%	8%	3%
109. A Veteran's Memorial?	38%	46%	12%	4%	1%
110. A bandshell for concerts?	24%	47%	21%	7%	2%
111. Docks and fishing pier?	37%	41%	17%	4%	0%
112. Picnic shelters?	32%	43%	18%	7%	0%
113. An off-leash dog park?	35%	37%	19%	8%	1%

Suppose the City of Dayton proposed a parks and recreational facilities referendum which you considered to be a reasonable approach. The proposal would be placed on a referendum ballot for approval by the voters. In order to fund construction, residents would be asked to approve a property tax increase for a twenty year period.

114. How much would you be willing to see your property taxes increase to fund these improvements? Let's say, would you be willing to see your property taxes increase by \$__ a month or \$__ a year? (CHOOSE RANDOM STARTING POINT; MOVE UP OR DOWN DEPENDING ON RESPONSE) How about \$__ a month or \$__ a year?	NOTHING.....	19%
	\$6/\$72.....	39%
	\$12/\$144.....	22%
	\$18/\$216.....	9%
	\$24/\$288.....	4%
	\$30/\$360.....	1%
	OVER \$30/\$360.....	0%
	DON'T KNOW/REFUSED.....	5%
115. Have you or members of your household attended programs at the Activities Center? (IF "YES," ASK:) How would you rate your experience - excellent, good, only fair, or poor?	NO.....	58%
	YES/EXCELLENT.....	9%
	YES/GOOD.....	32%
	YES/ONLY FAIR.....	2%
	YES/POOR.....	0%
	DON'T KNOW/REFUSED.....	0%

116. Are there any changes or improvements you would like to see to the programs offered at the Activities Center?

UNSURE, 18%; NO, 76%; MORE CHILDREN ENRICHMENT PROGRAMS, 2%; SCATTERED, 4%.

The City of Dayton hosts many city events, including Holly Dayton, the Easter Egg Hunt, Jack-O-Lantern Trail, and Family Fun Night.

117. Have you or members of your household participated in any city events? (IF "YES," ASK:) How would you rate your experience - excellent, good, only fair, or poor?	NO.....46%
	YES/EXCELLENT.....14%
	YES/GOOD.....35%
	YES/ONLY FAIR.....5%
	YES/POOR.....0%
	DON'T KNOW/REFUSED.....0%

118. Are there any changes or improvements you would like to see to city events?

UNSURE, 17%; NO, 78%; MORE PARKING, 2%; MORE VARIETY OF EVENTS, 2%; SCATTERED, 2%.

Moving on...

119. Other than voting, do you feel that if you wanted to, you could have a say about the way things are run in the City of Dayton?	YES.....70%
	NO.....21%
	DON'T KNOW/REFUSED.....9%

120. How much do you know about the work of the Mayor and City Council - a great deal, a fair amount, very little, or none at all?	A GREAT DEAL.....11%
	A FAIR AMOUNT.....48%
	VERY LITTLE.....33%
	NONE AT ALL.....9%
	DON'T KNOW/REFUSED.....0%

121. From what you know, do you approve or disapprove of the job of the Mayor or City Council are doing? (WAIT FOR RESPONSE) And do you feel strongly that way?	STRONGLY APPROVE.....23%
	SOMEWHAT APPROVE.....64%
	SOMEWHAT DISAPPROVE.....9%
	STRONGLY DISAPPROVE.....1%
	DON'T KNOW/REFUSED.....4%

122. How much first hand contact have you had with the Dayton City staff - quite a lot, some, very little, or none?	QUITE A LOT.....7%
	SOME.....41%
	VERY LITTLE.....38%
	NONE.....14%
	DON'T KNOW/REFUSED.....0%

123. From what you have heard or seen, how would you rate the job performance of the Dayton City staff - excellent, good, only fair, or poor?	EXCELLENT.....11%
	GOOD.....75%
	ONLY FAIR.....12%
	POOR.....0%
	DON'T KNOW/REFUSED.....2%
124. During the past year, have you Visited or contacted Dayton City Hall in-person, on the telephone, or using the website?	YES.....39%
	NO.....61%
	DON'T KNOW/REFUSED.....0%

IF "YES," ASK: (n=155)

Thinking about your last contact with the City, for each of the following characteristics, please rate the service as excellent, good, only fair, or poor....

	EXC	GOO	FAI	POO	DKR
125. Wait time for city staff to respond?	14%	61%	22%	3%	1%
126. Ease of reaching a city staff member who could help you?	28%	56%	13%	3%	1%
127. Courtesy of the city staff?	32%	63%	3%	1%	1%
128. Efficiency of the city staff?	30%	60%	10%	1%	0%

Turning to city communications....

129. What is your principal source of information about Dayton City Government and its activities?	DON'T KNOW/REFUSED.....0%
	NONE.....2%
	CITY NEWSLETTER.....40%
	CITY SOCIAL MEDIA.....15%
	WORD OF MOUTH.....15%
	LOCAL NEWSPAPER.....7%
	MAILINGS.....11%
	COMMUNITY SOCIAL MEDIA.8%
	SCATTERED.....2%
130. If you could choose the best way for you to receive information about your City Government and the issues facing the community, what would it be?	DON'T KNOW/REFUSED.....0%
	NONE.....2%
	CITY NEWSLETTER.....45%
	CITY SOCIAL MEDIA.....14%
	WORD OF MOUTH.....6%
	LOCAL NEWSPAPER.....7%
	MAILINGS.....15%
	COMMUNITY SOCIAL MEDIA.8%
	SCATTERED.....3%

The City publishes a newsletter called the *Dayton Communicator*, which is mailed to all residents quarterly.

131. Do you read this newsletter? YES.....72%
 NO.....28%
 DON'T KNOW/REFUSED.....0%

IF "YES," ASK: (n=287)

132. How would you evaluate its content and format - excellent, good, only fair, or poor? EXCELLENT.....17%
 GOOD.....75%
 ONLY FAIR.....8%
 POOR.....0%
 DON'T KNOW/REFUSED.....0%

133. Do you have access to the Internet? YES.....91%
 NO.....9%
 DON'T KNOW/REFUSED.....0%

IF INTERNET ACCESS, ASK: (n=364)

134. Have you or any member of your household accessed the City of Dayton website? YES.....60%
 NO.....40%
 DON'T KNOW/REFUSED.....0%

IF "YES," ASK: (n=218)

135. How often do you visit the City's website - frequently, occasionally, rarely, or whenever needed? FREQUENTLY.....15%
 OCCASIONALLY.....51%
 RARELY.....21%
 WHENEVER NEEDED.....14%
 DON'T KNOW/REFUSED.....0%

136. How would you evaluate the content of the City's website - excellent, good, only fair, or poor? EXCELLENT.....9%
 GOOD.....84%
 ONLY FAIR.....7%
 POOR.....1%
 DON'T KNOW/REFUSED.....0%

137. How would you rate the ease of navigating the City's website and finding information you were looking for - excellent, good, only fair, or poor? EXCELLENT.....13%
 GOOD.....81%
 ONLY FAIR.....6%
 POOR.....0%
 DON'T KNOW/REFUSED.....0%

138. Have you used the City of Dayton Government Facebook or Instagram to receive information from the City?	YES.....44%
	NO.....56%
	DON'T KNOW/REFUSED.....0%

IF "YES," ASK: (n=161)

139. How would you rate the City's social media - excellent, good, only fair, or poor?	EXCELLENT.....22%
	GOOD.....73%
	ONLY FAIR.....5%
	POOR.....0%
	DON'T KNOW/REFUSED.....0%

140. Have you used the City of Dayton app?	YES.....34%
	NO.....66%
	DON'T KNOW/REFUSED.....0%

IF "YES," ASK: (n=87)

141. How would you rate the City's app - excellent, good, only fair, or poor?	EXCELLENT.....9%
	GOOD.....83%
	ONLY FAIR.....8%
	POOR.....0%
	DON'T KNOW/REFUSED.....0%

Now, just a few more questions for demographic purposes....

Could you please tell me how many people in each of the following age groups live in your household. Let's start with the oldest. Be sure to include yourself.

142. First, persons 65 or over?	NONE.....81%
	ONE.....7%
	TWO OR MORE.....12%

143. Adults under 65?	NONE.....13%
	ONE.....18%
	TWO.....65%
	THREE OR MORE.....5%

144. School-aged or preschool children?	NONE.....65%
	ONE.....14%
	TWO.....15%
	THREE OF MORE.....6%

145. What is your age, please?	18-34.....30%
	35-54.....40%
	55 AND OVER.....30%
	REFUSED.....0%

IF "55 AND OVER," ASK: (n=120)

146. Are there any recreation programs or activities that you would be interested in the City of Dayton offering?

UNSURE, 27%; NO, 58%; ART CLASSES, 2%; FITNESS CLASSES, 3%; SENIOR TECHNOLOGY SKILLS, 3%; CARDS/GAME CLUBS, 2%; SCATTERED, 5%.

- | | |
|---|---------------------------|
| 147. Finally, thinking about your household finances, how would you describe your financial situation, would you say that - | STATEMENT A.....2% |
| A) Your monthly expenses are exceeding your income; | STATEMENT B.....31% |
| B) You are meeting your monthly expenses but are putting aside little or no savings; | STATEMENT C.....52% |
| C) You are managing comfortably while putting some money aside; | STATEMENT D.....14% |
| D) Managing very well? | DON'T KNOW/REFUSED.....2% |
| | |
| 148. Gender (BY OBSERVATION) | MALE.....48% |
| | FEMALE.....52% |
| | |
| 149. Precinct (FROM LIST) | ONE.....32% |
| | TWO.....26% |
| | THREE.....43% |

ITEM:

Resolution 22-2026, Approving a Variance to increase the maximum front yard building setback; and Site Plan Review for Dayton Assisted Living, 136XX Balsam Lane N., PID: 14-120-22-41-0005

APPLICANT/PRESENTERS:

Chad Caza, BJ Baas Builders, Inc.
Frank Orito Angwenyi, Triad Holdings, LLC

PREPARED BY:

Jon Sevald, Community Development Director

POLICY DECISION / ACTION TO BE CONSIDERED:

1. Motion to Approve
2. Motion to Deny with Findings
3. Table for more information (define what information is needed).

BACKGROUND/OVERVIEW:

BJ Baas Builders is proposing a one-story 22-unit Assisted Living and Memory Care facility to be located on vacant land south of Raintree Plaza. The facility will be operated by Triad Holdings. This will be their first brick & mortar operation. Triad has experience with home hospice care.

A Concept Plan consisting of three-stories and 58-units was reviewed by the Planning Commission and City Council in December 2025.

City Council comments:

- Opposition to 3-stories (Dec 9, 2025).
- Support for 2-stories next to single-family and 3 stories along Balsam Ln.

Planning Commission comments (Dec 3, 2025):

- Support for Variance to increase maximum building setback
- Opposition to 3-stories next to single-family
- Support for shared access with Raintree Plaza
- Recommend pervious Fire Lane behind building

CRITICAL ISSUES:

Front Setback	A Variance is required to increase the maximum front building setback, from 30' to 180'. The practical difficulty is the wedge shape of the property. Findings supporting the Variance are in the attached Resolution.
Access	A right-in/right-out is proposed. Staff recommends a shared access through Raintree Plaza. If Raintree opposes, then Staff recommends the applicant remove the Balsam Ln center median creating a full-access.

ANALYSIS:

Zoning¹

GMU-4 General Mixed Use – Balsam Lane. Assisted Living is a Permitted Use.

Comprehensive Plan Mixed Use. Minimum density is 12-units per net acre. Proposed is 14-units per net acre.

Architecture²

All new building fronts (single story or multi-story) shall include a minimum of 3 of the following elements:		
1.	Architectural detailing, such as cornice, awning, parapet, or columns;	X
2.	A visually pleasing primary front entrance that, in addition to doors, shall be accented a minimum of 150 square feet around the door entrance for single occupancy buildings and a minimum of 300 square feet total for the front of multi-tenant buildings (this area shall be counted as one element). Entrances shall be clearly articulated and obvious from the street;	X
3.	A minimum of 25% window coverage on each front that faces a street;	
4.	Contrasting, yet complementary material colors;	
5.	A combination of horizontal and vertical design features;	X
6.	Irregular building shapes;	X
7.	Horizontal offsets of at least 4 feet in depth;	X
8.	Vertical offsets in the roofline of at least 4 feet;	
9.	Fenestration at the first floor level which is recessed horizontally at least 1 foot into the facade;	
10.	Other architectural features in the overall architectural concept.	
Large, uninterrupted exterior wall surfaces are not permitted. No wall shall have an uninterrupted length exceeding 100 feet, without including at least 2 of the following: changes in roof plane, changes in color, texture, materials or masonry pattern, windows, or an equivalent element that visually subdivides the wall. In addition to these aforementioned elements, additional landscaping may be required.		X
Other design requirements.		
1.	Buildings shall provide a base and top to their architecture.	X
2.	The tops of buildings shall be articulated to minimize the "box" like images.	X
3.	Standard corporate style architecture shall be minimized at the discretion of the City Council.	N/A
Roof-mounted mechanical equipment, vents, and stacks shall be minimized and positioned so that they will not be seen from public rights-of-way or adjacent properties. If that is not possible, and the equipment is visible from public rights-of-way or adjacent properties, the equipment shall be screened with parapet walls or encasements colored similar to the building in a manner that eliminates reflections.		X
Major exterior materials of all walls including face brick, stone, glass, stucco, synthetic stucco, fiber cement vertical, horizontal or lap panel siding architectural concrete and precast panels shall be acceptable as the major exterior wall surface when they are incorporated into an		X

¹ [City Code 1001.065, Subd 4\(2\)](#) (Permitted Uses; Dwelling, multiple family)

² [City Code 1001.065, Subd 4\(8\)](#) (Building design requirements)

overall design of the building. Major materials must cover at least 60% of the exterior.	
Fiber cement seam lines shall be architecturally integrated into the building design so that they are not visible. Seam lines can be filled, covered by other accent material or other method thereby making the seam lines invisible. Color impregnated decorative block shall also be allowed as a major exterior wall material and shall be required to be sealed. All materials shall be color impregnated with the exception of architectural concrete precast panel systems and fiber cement siding that may be painted.	X

Rooftop mechanical equipment on the north (rear) roof is screened by a parapet wall.

Exterior materials include four styles of fiber cement (LP siding) and manufactured stone.

Building Setbacks³

	Prop.
Front-build-to-line. 15 feet minimum with a 30-foot maximum.	179'-4"
Side. Non-residential uses: 10 feet; residential uses: 20 feet.	21'-4"
Rear. Non-residential uses: 10 feet; residential uses: 20 feet.	22'

A Variance is required to increase the maximum front yard building setback from 30' to 180'.

Parking^{4 5}

11 minimum (1/2 stall per unit), whereas 18 is proposed.

Parking is prohibited from the required front yard (0'-15').⁶ Proposed parking is in the front yard, 95' set back. Due to the shape of the property, the parking lots are reasonable.

10' X 20' stall required (10' X 18' with overhang). 9' X 20' proposed. Plans are to be amended.

Parking Setback^{7 8}

5' minimum. Proposed is 4'-6" between property line and back of curb. Plans to be revised to 5' to back of curb.

Driveway

A right-in/right-out is proposed. Staff recommends the Applicant obtain an easement from Raintree Plaza to share their southerly driveway (full access). If Raintree is not agreeable, Staff recommends the Applicant remove the Balsam Ln landscaped median to create a full access.

³ [City Code 1001.065, Subd 4\(6\)\(a\)\(2\)](#) (Setbacks)

⁴ [City Code 1001.19, Subd 7\(j\)\(1\)\(f\)\(i\)](#) (Special Residential; Assisted living facility)

⁵ [City Code 1001.15, Subd 9\(1\)](#) (Minimum Parking Space and Aisle Dimensions; Table)

⁶ [City Code 1001.19, Subd 3\(1\)\(b\)\(1\)](#) (Front Yard)

⁷ [City Code 1001.065, Subd 4\(7\)\(e\)](#) (Parking requirements)

⁸ [City Code 1001.19, Subd 2\(8\)](#) (Curbing)

A Fire Lane (gravel/sod) is proposed along the rear of the building. Since the building was reduced from 3-stories to 1-story, this Fire Lane is not necessary.

Landscaping⁹

...provide at least 3 of the following required numbers of trees and shrubs in addition to any trees and shrubs required for screening...		
	Req.	Prop.
One over-story tree per 3,000 square feet of open area. ¹⁰	8	6
One ornamental tree per 1,500 square feet of open space.	17	9
One evergreen tree per 3,000 square feet of open area.	8	18
One deciduous or evergreen shrub per 300 square feet of open area.	84	123

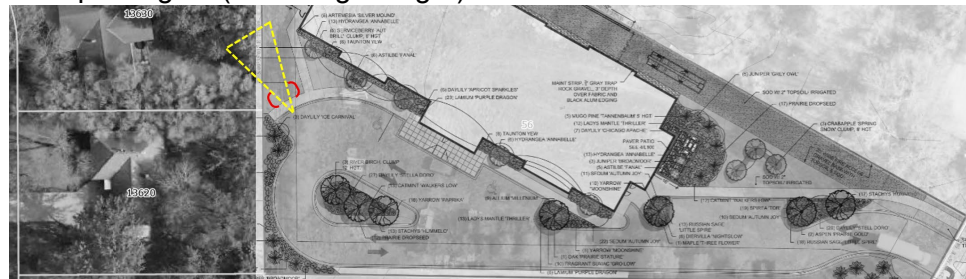
The number of plantings meets the intent of the City Code. If the Commission/Council recommends additional trees, there is space along the north property line in place of the Fire Lane.

Plans to be amended to add a 5' strip of ground cover along the building edge (in place of the north Fire Lane).¹¹

Screening^{12 13 14}

Dumpster will be stored inside the building.

Staff recommends additional landscape screening of car headlights at the west parking lot (3½' hedge height).



Stormwater

Stormwater storage will be under the parking lot.

Lighting¹⁵

Maximum of 0.5fc at the property line of single-family, whereas 0.2fc is proposed.

⁹ [City Code 1001.24, Subd 4\(3\)](#) (Landscaping requirements in multiple family residential, and all non-residential...)

¹⁰ 25,162sf of greenspace, per Landscape plan, p. L100

¹¹ [City Code 1001.24, Subd\(3\)\(b\)](#) (Building ground cover)

¹² [City Code 1001.065, Subd 4\(9\)](#) (Screening)

¹³ [City Code 1001.14, Subd 2\(2\)\(b\)](#) (Noise, odors, smoke, dust, fumes, water and waste)

¹⁴ [City Code 1001.24, Subd 4\(b\)](#) (Parking lot screening standards)

¹⁵ [City Code 1001.065, Subd 4\(10\)](#) (Lighting)

Signage¹⁶ One free standing sign is permitted, 16’ height, 64sf in area. The proposed sign is 5’ in height, 18.5sf in area.

Standard Conditions¹⁷ City Code provides conditions of approval for Site Plans. Many of these conditions are redundant with Building and Fire codes, and elsewhere within the City Code.

60/120-DAY RULE (IF APPLICABLE):

	60-Days	120-Days	
Variance	May 26, 2026	Jul 24, 2026	
Site Plan	May 26, 2026	Jul 24, 2026	

RELATIONSHIP TO COUNCIL GOALS:

Strategic Initiative	Goal	Key Outcome Indicator	Target	Action Item
Encourage Diversity and Manage Thoughtful Development	Create a variety of housing options	<ul style="list-style-type: none"> Review housing type and lot size by %'s 	<ul style="list-style-type: none"> Proportionate housing types available 	A) Begin work on Comp Plan – Create Timeline for Completion B) Develop Rental Housing Ordinance C) Seek out businesses more often D) Work with EDA to find niche businesses that are not in surrounding communities E) Complete Large Area Plan – Breakdown of Comp Plan Decades F) Review Parking Code Requirements
	Encourage healthy lifespan of both residential and commercial operations	<ul style="list-style-type: none"> Total amount of Funding provided. Number of rentals available and where they are located. 	<ul style="list-style-type: none"> Maintain grant program. Manage number of rentals 	
	Healthy Commercial Sector with services and job growth	<ul style="list-style-type: none"> Net difference of businesses movement including their employment 	<ul style="list-style-type: none"> Maintain a positive difference in business movement 	

PLANNING COMMISSION RECOMMENDATION:

The Planning Commission conducted a Public Hearing at its May 7, 2026 meeting, recommending Approval, conditioned upon (1) the Applicant installing a 6’ - 8’ tall opaque fence along the western property line, (2) if shared driveway easement is dissolved, then requiring the property owner to construct a direct access onto Balsam Lane.

During the Public Hearing, three residents spoke in opposition to the project because of anticipated traffic, noise (emergency vehicles), and exterior lighting.

STAFF RECOMMENDATION:

Staff recommends Approval.

ATTACHMENT(S):

- Aerial Photo
- Site Photos
- Public Hearing Map

¹⁶ [City Code 1001.20, Subd 5\(2\)](#) (Freestanding sign standards)

¹⁷ [City Code 1001.28, Subd 3\(5\)](#) (Standard conditions)

Zoning Map
Comprehensive Plan Map
City Engineer's letter, April 30, 2026
Plan Set
Correspondence, Ashley Winters, May 7, 2026

SITE PHOTOS



View looking west toward project (Apr 30, 2026)



View from Raintree Plaza looking south toward project (Apr 30, 2026)



View looking east toward Balsam Apartments (Apr 30, 2026)



Panoramic view from near SW corner of property, looking NW to SE (Apr 30, 2026)



Panoramic view from near NW corner of property, looking SW to SE (Apr 30, 2026)



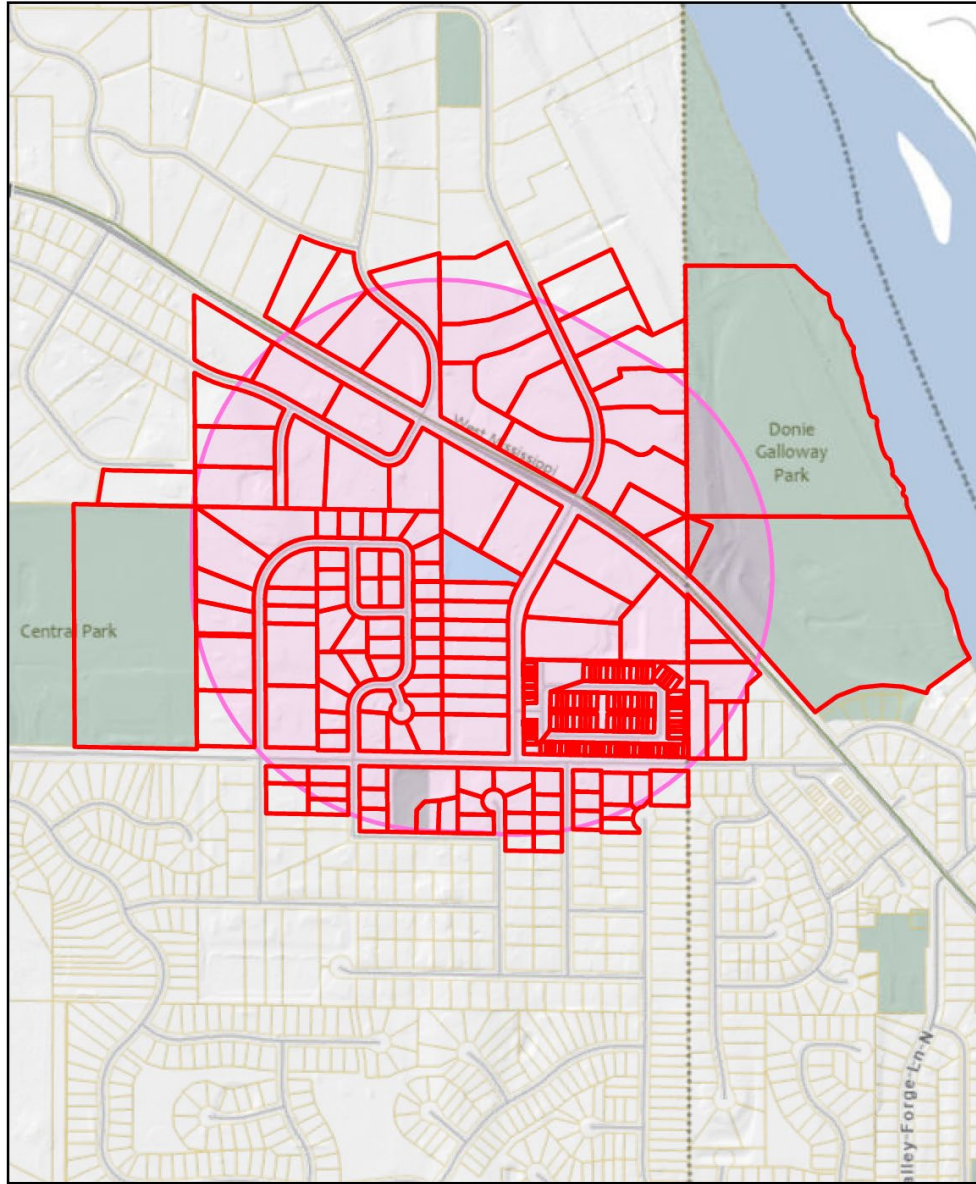
Panoramic view from south property line looking West to east (Apr 30, 2026)

PUBLIC HEARING MAP



Hennepin County Locate & Notify Map

Date: 4/14/2026



Buffer Size: 1320

Map Comments:

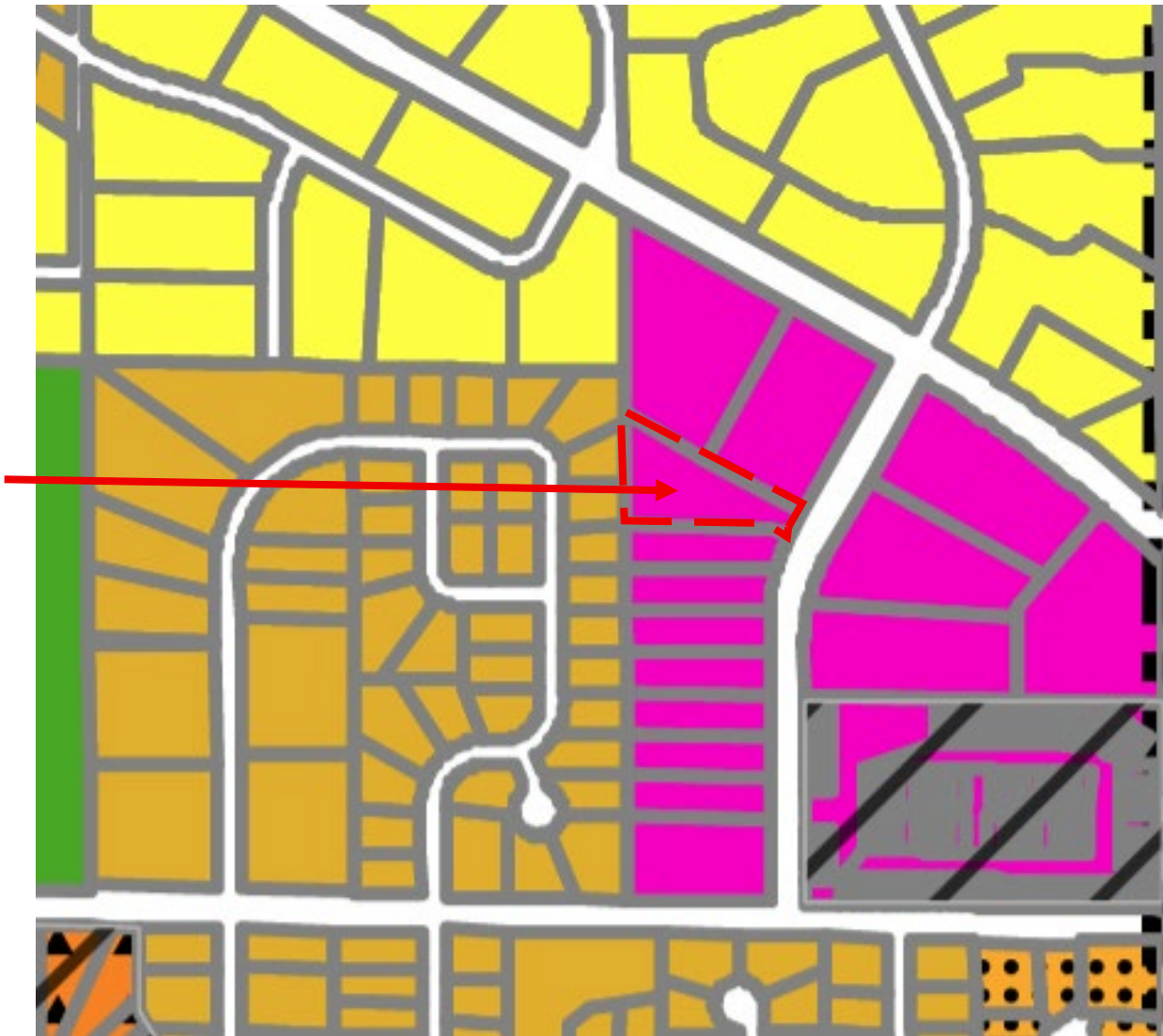
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
























This data (i) is furnished 'AS IS' with no representation as to completeness or accuracy; (ii) is furnished with no warranty of any kind; and (iii) is not suitable for legal, engineering or surveying purposes. Hennepin County shall not be liable for any damage, injury or loss resulting from this data.

For more information, contact Hennepin County GIS Office
300 6th Street South, Minneapolis, MN 55487 / gis.info@hennepin.us

ZONING MAP



Legend

- | | |
|---|---|
|  A-1 Agricultural District |  R-1A Single Family Residential |
|  A-2 Agricultural District |  R-2 Single Family District (90,000 Sf, Unsewered) |
|  A-3 Agricultural District |  R-3 Single Family and Attached Residential |
|  B-2 Neighborhood Business District |  R-E Single Family District (5 Ac, Unsewered) |
|  B-3 General Business District |  R-M Medium Density Residential District |
|  B-4 Commercial/ Industrial District |  R-MH Mobile Home District |
|  B-P Business Park District |  R-O Old Village Residential |
|  ES Essential Service District |  S-A Special Agriculture District |
|  GMU-4 Balsam Lane |  GMU-3 Historic Village |
|  GMU-5 Southwest Mixed-Use |  City Boundary |
|  I-1 Light Industrial District |  PUD |
|  P-R Public Recreation District |  County Parcels |
|  R-1 Single Family District | |

COMPREHENSIVE PLAN



- | | | | | |
|--|----------------------------|----------------------------|-------------------|----------------------|
| Greenway Overlay | Rural Estate | Existing Mobile Home Park | Mixed Use | Public/Institutional |
| Agricultural Preserve | Low Density Residential | Master Planned Development | Business Park | Open Water |
| Existing Unsewered Low Density Residential | Medium Density Residential | Neighborhood Commercial | Industrial | Right-of-Way |
| Existing Sewered Low Density Residential | High Density Residential | Commercial | Park & Open Space | |
| | | | Golf Course | |

RESOLUTION 22-2026

**CITY OF DAYTON
COUNTIES OF HENNEPIN AND WRIGHT
STATE OF MINNESOTA**

**RESOLUTION APPROVING A VARIANCE TO INCREASE THE MAXIMUM FRONT
YARD BUILDING SETBACK, AND APPROVING A SITE PLAN REVIEW FOR
DAYTON ASSISTED LIVING**

WHEREAS, Chad Caza, BJ Baas Builders (Applicant) on behalf of Triad Holdings LLC (Owner) applied for a Variance to increase the maximum Building Front Yard Setback from 30’ to 179’-4”, AND a Site Plan Review for Dayton Assisted Living, to be located on a vacant unaddressed property, 136XX Balsam Lane, legally described as:

PID: 14-120-22-41-0005

Lot 3, Block 1, Raintree 2nd Addition

WHEREAS, the property is zoned GMU-4 Balsam Lane; and,

WHEREAS, the property is guided Mixed Use in the 2040 Comprehensive Plan; and,

VARIANCE

WHEREAS, City Code 1001.065, Subd 4(6)(a)(2)(a) (General Mixed Use District-4 (GMU-4) (Setbacks); requires a Front-build-to-line, 15’ minimum with a 30’ maximum; and,

WHEREAS, the applicant proposes a front yard building setback of 179’-4””; and,

WHEREAS, City Code 1001.29, Subd 3 (Criteria) states; *All requests for a variance shall be subject to a determination that the variance request complies with the requirements of Minn. Stat. § 462.357 and any amendments thereto, which include, but are not limited to [Findings];*

VARIANCE FINDINGS

(1) A variance shall only be permitted when it is in harmony with the general purpose and intent of City Code and consistent with the Comprehensive Plan.

Finding: In relation to the Variance, the purpose of the GMU-4 district is ...“*The placement of buildings and the relationship of the building, parking, landscaping, and pedestrian spaces is essential to*

creating the pedestrian-friendly environment envisioned for the GMU-4 District.”

Due to the wedge-like shape of the property, the proposed building is reasonably as close as possible to the maximum 30’ front yard setback. “*The pedestrian-friendly environment*” can be obtained via enhanced landscaping along Balsam Lane.

(2) A variance may only be permitted when the applicant establishes that there are practical difficulties in complying with the zoning ordinance, meaning:

a. The property owner proposes to use the lot or parcel in a reasonable manner not permitted by the zoning code.

Finding: The placement of the building is reasonable. The lot is approximately 70’ wide measured at the 30’ maximum front yard building setback. Considering the minimum 20’ side yard setbacks, this allows for a 30’ wide building, whereas the proposed building is 91’ wide. The proposed building is a reasonable size.

b. The plight of the property owner must be due to non-economic circumstances that are unique to the lot or parcel and not created by the property owner.

Finding: The wedge-like shape of the property is unique, not created by the property owner.

c. The variance must not alter the essential character of the locality.

Finding: The proposed building will not alter the essential character of the neighborhood. The adjacent buildings have front yard setbacks of approximately:

90’ 11311 Dayton River Rd (Marathon Gas - 1973)
88’ 13691 Balsam (multi-tenant - 1998)

d. Practical difficulties include, but are not limited to, inadequate access to direct sunlight for solar energy systems.

Finding: Not applicable.

VARIANCE DECISION

WHEREAS, the Planning Commission held a Public Hearing on May 7, 2026, recommending Approval; and,

NOW, THEREFORE, in consideration of the Staff Report, Planning Commission recommendation, and public testimony, the City of Dayton City Council approves the Variance to increase the maximum front yard building setback from 30' to 175' with the following conditions:

1. The applicant shall install landscaping in the front yard consistent with the Landscaping Plan included in the May 26, 2026 Staff Report, with the intent of creating a pedestrian-friendly environment. Landscaping shall be installed prior to the city issuing a Certificate of Occupancy.
2. The applicant shall install a 6' – 8' tall opaque fence along the western property line prior to the city issuing a Certificate of Occupancy.

SITE PLAN REVIEW

WHEREAS, City Code 1001.28, Subd 3(1)(a) states; “*Final site and building plans shall be approved by the City Council prior to the issuance of any permits for new development or building construction/expansion in any Non-Residential Zoning District*”; and,

WHEREAS, the applicant proposes an Assisted Living and Memory Care Facility on land zoned GMU-4 Balsam Lane; and,

WHEREAS, Staff reviewed the plan set for consistency with City Code, and Engineering Design Guidelines and Standard Detail Plates; and,

WHEREAS, City Code 1001.28, Subd 3(3) states; *The City Council shall find the following prior to the approval of final site and building plans.*

SITE PLAN FINDINGS

a. *The proposed development is not in conflict with the Comprehensive Plan;*

Finding: The property is guided *Mixed-Use* in the 2040 Comprehensive Plan, with a minimum residential density of 12-units per net acre. The proposed project is 14-units per net acre.

b. *The proposed development is not in conflict with the zoning district provisions;*

Finding: The proposed Assisted Living Facility is a Permitted Use in the GMU-4 district. The development is consistent with zoning district provisions, pending approval of a Variance to front yard building setback, and minor revisions to the plan set.

c. *The proposed development is compatible with existing and anticipated future development; and*

Finding: The proposed Assisted Living Facility is compatible with existing land uses to the north (gas station), east (multi-family), south (mixed-use office), and west (single-family).

d. Conform to the exterior building material requirements of the Zoning Code.

Finding: The proposed building material is consistent with the Zoning Code.

SITE PLAN DECISION

WHEREAS, the Planning Commission held a Public Hearing on May 7, 2026, recommending Approval; and,

WHEREAS, in consideration of the Staff Report, Planning Commission recommendation, and public testimony, the City of Dayton City Council approves the Site Plan with the following conditions:

1. Prior to issuance of a Building Permit, plans shall be revised to comply with the City Engineer's Review letter, dated April 30, 2026, and marked up plan sheet (email, May 20, 2026).
2. Prior to issuance of a Building Permit, plans shall be revised to comply with a minimum 5' setback, property line to curb (west and south property line), and property line to patio (west property line). The Fire Lane behind the building may be removed. Parking stalls shall be 10' X 20' (10' X 18' if overhang).
3. Prior to issuance of a Building Permit, the Applicant shall revise plans to comply with minimum parking stall dimensions, and minimum of 5' setback between property line and back of curb.
4. Plans and construction shall be consistent with City Code 1001.28, Subd 3 (Standard conditions).
5. Prior to issuance of a Building Permit, the Applicant shall obtain and record an Ingress/Egress Easement through Raintree Plaza's south driveway. If Raintree Plaza is not agreeable to the easement, the Applicant shall construct a full access driveway entrance, and remove the Balsam Lane center median, at the Applicant's expense, prior to the City issuing a Certificate of Occupancy.
6. Upon dissolution of the Ingress/Egress Easement, the property owner shall install a driveway access onto Balsam Lane.
7. Prior to issuance of a Building Permit, the Applicant shall submit a Landscape Escrow, 125% the value of trees & shrubs. Trees & shrubs shall be guaranteed for 12-months from time of planting. The city will inspect at the expiration of guarantee period. The Applicant shall replace dead and diseased trees & shrubs within the guarantee period. The city shall refund the escrow at the end of the guarantee period (proportionate amount based on survivability).
8. The Applicant shall install a 6' – 8' opaque wall along the western property line, prior to the city issuing a Certificate of Occupancy.

Adopted by the City Council of the City of Dayton on this 26th day of May, 2026

Dennis Fisher, Mayor

ATTEST:

Amy Benting, City Clerk

Motion by Councilmember, _____, Second by Councilmember _____

Ayes:

Nays:

Resolution Approved

To:	Jon Sevald, Planning	From:	Jason Quisberg, Engineering Nick Findley, Engineering Ben Otto, Engineering
Project:	Dayton Assisted Living	Date:	4/30/2026

Exhibits:

This Memorandum is based on a review of the following documents:

1. Dayton Assisted Living Plans, by Schultz Engineering & Site Design, undated, 15 sheets
2. Certificate of Survey, by O'Malley & Kron Land Surveyors, Inc., dated 10/16/2025, 1 sheet
3. Stormwater Calculations Report, by Schultz Engineering & Site Design, dated 3/10/2026, 90 sheets
4. Geotechnical Report, By Chosen Valley testing, Inc., dated 3/11/2026, 22 sheets
5. Electrical Site Plan, By Berd Electric, LLC, dated 2/9/2026, 8 sheets

Comments:General

1. Consistent with the review process, a comment response letter shall be provided in response to the following comments provided in this Memorandum in which the applicant provides a written response to each item.
2. In addition to engineering related comments per these plans, the proposed plans are subject to additional planning, zoning, land-use, and other applicable codes of the City of Dayton.
3. Final approval by the Elm Creek Watershed Management Commission must be attained before any site grading or activity may commence.
4. For any site activity (demo, grading, utilities, etc.) no closures or restrictions of any kind shall be imposed upon the public use of Balsam Lane without the City's permission. Should any lane restrictions be necessary, the Contractor shall notify the City at least 48 hours in advance and provide a Traffic Control Plan.
5. The City of Dayton Standard Details provided are not the current set. Please see the PDFs located on the City website or request CAD files from the Engineering Department.
6. Fill is shown with the existing power line easement along the western edge of the property. Provide confirmation from the utility company that the required vertical allowance is maintained, and pole replacements are not required.

Plat

7. It appears storm water improvements are to be private, and a maintenance access agreement will be required.

Erosion Control/SWPPP

8. The SWPPP Contact information is to be updated and provide prior to the start of work onsite.
9. A MPCA/NPDES construction stormwater permit is required for the site. Sediment and erosion control plans shall be consistent with the general criteria set forth by the most recent versions of the Minnesota Stormwater Manual and the NPDES Construction site permit.

Wetlands

10. The National Wetland Inventory does not show any wetlands within the area. A qualified person is to complete a wetland delineation or provide documentation meeting LGUs requirements confirming there are no wetlands.
 - o Verification is a requirement of the wetland review agencies (TEP).

Site Plans

11. Based on discussions with emergency services, a right-in/right-out connection will not be acceptable. If shared access with the northern property owner cannot be obtained, a full-access connection must be provided. This will require removal of the existing median to accommodate all turning movements at the proposed entrance. Given the median's length and its proximity to the taper, full removal of the median will be required. Any impacted irrigation or landscaping must be removed or modified without affecting the remaining system. Pavement patching shall match the existing roadway.
12. The proposed entrance is to conform to the standards shown within the commercial driveway apron detail (STR-14).
 - o Per City detail STR-14 curb radii on commercial driveway aprons need to be a minimum of 30-feet.
13. For future submittals provide turning movements showing a fire truck can navigate the site.
 - o Current plans submitted on April 9th, 2026 do not include fire truck turning movements. Turning movements are required to be shown to ensure emergency services are able to access the site.
14. The proposed fire lane is shown within close proximity to the adjacent property. Access rights are to be obtained from the adjacent property if encroachment is required. Written authorization from the property owner should be provided if encroachment is necessary.
15. Retaining wall is shown within 3.5 ft of the property line. Access rights are to be obtained from the adjacent property if encroachment is required. Written authorization from the property owner should be provided if encroachment is necessary.
16. Provide top and bottom retaining wall elevations at tie ins and slope changes for all proposed retaining walls.
17. The existing storm sewer structure located in the proposed entrance currently has a R-3067 casting. A drive-over casting will need to be installed with the proposed entrance.

18. A power pole and electrical box are shown within proposed sidewalk. Provide clarification on if the utilities are to be relocated.

Grading /Stormwater

19. The City of Dayton's Local Surface Water Management plans requires that the storm sewer system must be designed to handle a 10-year event. Provide rational method calculations.
20. Provide calculations show that existing City's ROW storm sewer system is not affected by the pipe leaving the OCS structure.
21. Time of concentrations for the sub-catchments in the existing HydroCAD model do not appear to align with existing conditions and when calculated, TC's were substantially longer than 5 minutes. Update the time of concentrations for the sub-catchments in the existing HydroCAD model using sheet (100' max) and shallow concentrated flow Show TC flow paths on existing drainage map.
22. Update the Manning's "n" coefficients in all proposed HydroCAD model outlet pipes to 0.011 to reflect the HDPE/PVC piping that is proposed throughout the projects stormwater system.
23. It appears that area directly south of the proposed project site drains onto the project site based on lidar and aerial imagery. Update the existing and proposed drainage maps and HydroCAD models to include offsite area.
24. Provide further details or shop drawings with dimensions and key elevation labels for the StormTech underground infiltration system to confirm HydroCAD modeling and treatment volume provided.
25. Update OCS 1 detail on sheet C3 to match information on sheet C5. Update plan sheets to be consistent throughout.
26. Update Device #3 for pond node MC-4500 in the proposed HydroCAD model to model a broad crested weir instead of a sharp crested weir.
27. Update pond node MC-4500 in the proposed HydroCAD model to utilize a starting WSE at the lowest constructed outlet per Section 3.6.2.a.viii of the Dayton Engineering Design Guidelines document.
28. Some of the elevation information listed under the "Elevations" annotation in Detail 1 - "Underground Storm Water Detention System Elev" on sheet C5 do not appear to match the detail depicted. Update elevation information to ensure consistency throughout.
29. It appears that a MIDS water quality model was only provided for the proposed conditions of the project. Provide a MIDS water quality model for the existing site conditions so that pollutant reductions can be quantified and confirmed.
 - o Provide the existing and proposed electronic MIDS files so that parameters of the site and the proposed underground infiltration BMP can be verified in detail.
 - o Ensure all project areas are accounted for, whether captured or uncaptured by the proposed BMP.
 - o Verify that BMP bypass percentage is utilized per stormwater manual.

30. Provide 48-hour drawdown calculations for the underground detention system. Isolator row surface area of proposed underground infiltration chamber shall not be considered part of the infiltration area in water quality calculations.
31. Provide maintenance access hatch on header row that is sized for standard jetter truck maintenance. Additionally provide inspection ports on the underground chamber.
32. Provide design information for the Underground Storm Detention System showing the system can withstand the weight of a fire truck.
33. All EOFs on and adjacent to the project site need to be shown and labeled. This includes the EOF for all depressions, including low point catchbasins.
34. Per City of Dayton standards drainage swales are required to be a minimum of 2%.
35. Provide spot elevations for the swale running along the northeast property line to ensure drainage is not trapped and the 2% requirement is met.
36. Storm sewer structures (48" diameter) approximately 7' in depth are shown within 5.5' of the property line. Access rights are to be obtained from the adjacent property owner if encroachment is required for the installation of these utilities. Written authorization from the property owner should be provided if encroachment is necessary.

Watermain/Sanitary Sewer

37. City of Dayton requires separate fire and domestic lines prior to entering commercial buildings, see City Detail Plate SER-07 for further information. Revise plans to incorporate this requirement into the utility plan.
38. Provide City Detail Plate WAT-01B to be used for the proposed fire hydrant within the site.
39. Records indicate that there is an existing valve located on the stub used for connection to this site. If the valve is not in working order or not in place a valve will be required at the connection.
40. The sanitary sewer and watermain services are within close proximity to the adjacent property. Access rights are to be obtained from the adjacent property owner if encroachment is required outside of the existing easement for the installation of these utilities. Written authorization from the property owner should be provided if encroachment is necessary.

Fire Department

41. The fire department has requested the proposed fire hydrant be relocated to the southwest side of the building facing the parking lot. It was also requested an additional hydrant be added southeast corner of the building with a Fire Department Connection added in close proximity. Coordinate final location with the Fire Department.

Landscaping

42. The landscaping plan conflicts with the site plan, specifically related to the fire access route. Ensure consistency between the two plans.

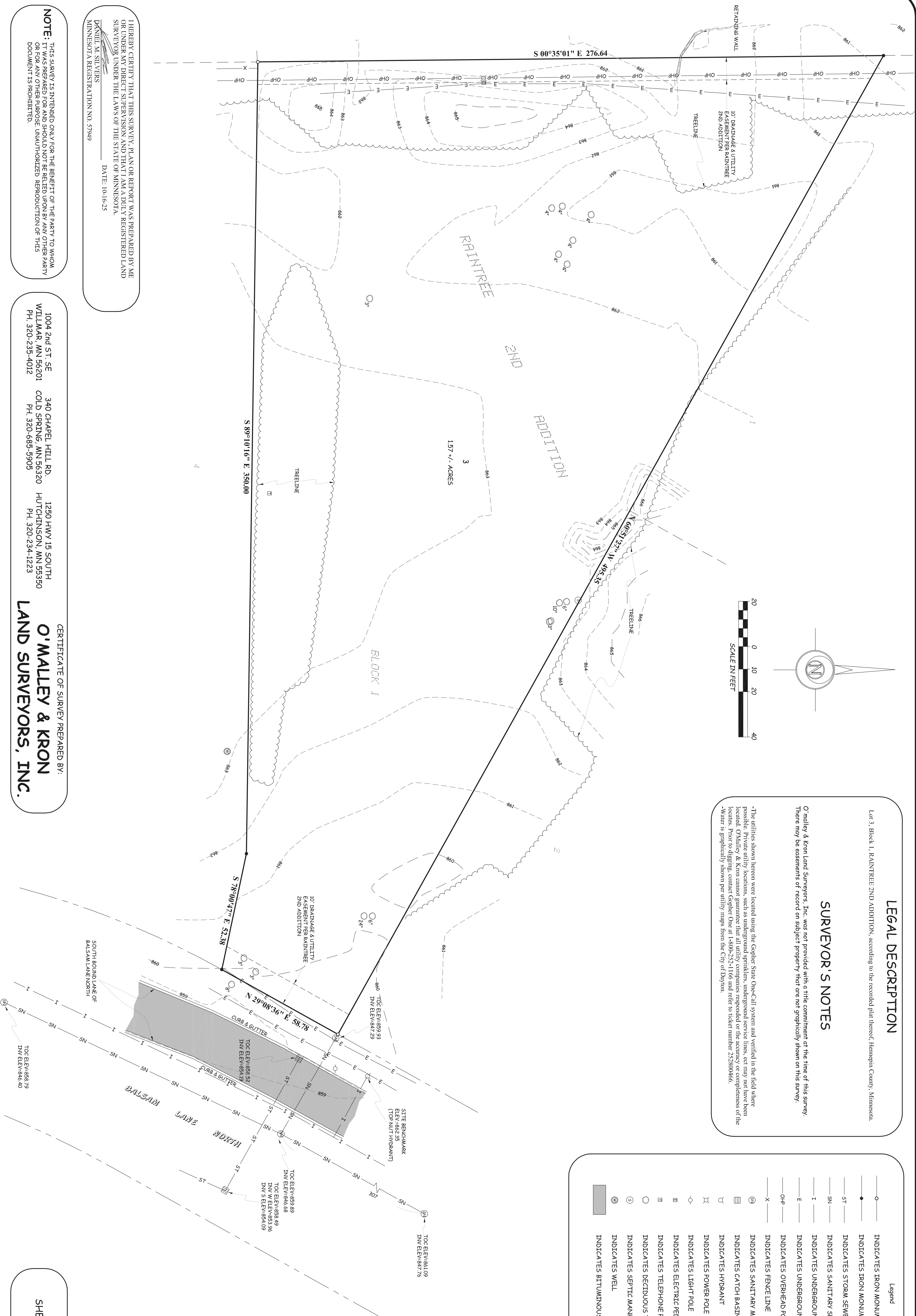
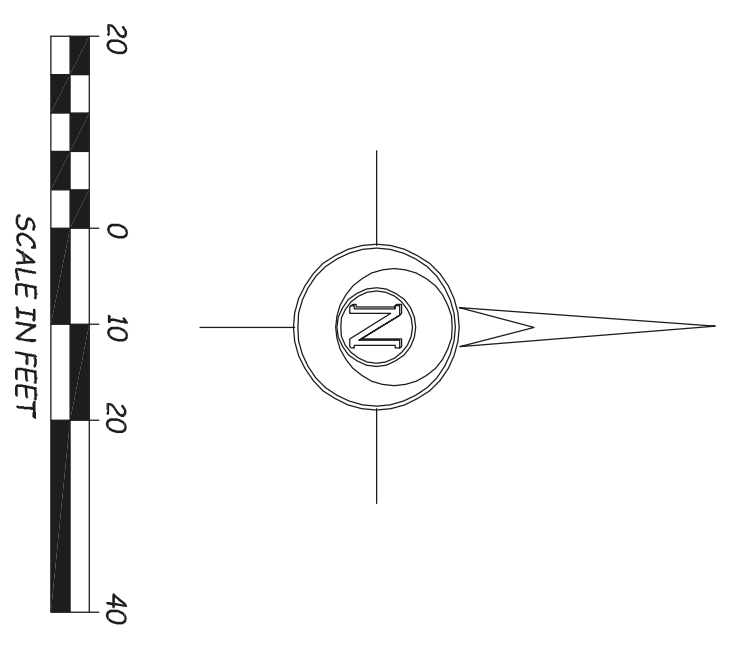
End of Comments

LEGAL DESCRIPTION
 Lot 3, Block 1, RAIN TREE 2ND ADDITION, according to the recorded plat thereof, Hennepin County, Minnesota.

SURVEYOR'S NOTES
 O'Malley & Kron Land Surveyors, Inc. was not provided with a title commitment at the time of this survey. There may be easements of record on subject property that are not graphically shown on this survey.

-The utilities shown hereon were located using the Greater State One-Call system and verified in the field where possible. Private utilities, such as irrigation systems, underground fire lines, etc may not have been located. O'Malley & Kron Land Surveyors, Inc. is not responsible for the accuracy of the utility locations. Prior to designing, contact Greater One at 1-800-252-1166 and refer to ticket number 252800466.
 -Water is graphically shown per utility maps from the City of Dayton.

- Legend
- INDICATES IRON MONUMENT PLACED
 - INDICATES IRON MONUMENT FOUND
 - INDICATES STORM SEWER LINE
 - ST— INDICATES SANITARY SEWER LINE
 - SN— INDICATES UNDERGROUND WATER
 - E— INDICATES UNDERGROUND ELECTRIC
 - OHP— INDICATES OVERHEAD POWER
 - X— INDICATES FENCE LINE
 - ⊙ INDICATES SANITARY MANHOLE
 - ⊞ INDICATES CATCH BASIN
 - ⊠ INDICATES HYDRANT
 - ⊡ INDICATES POWER POLE
 - ⊢ INDICATES LIGHT POLE
 - ⊣ INDICATES ELECTRIC PEDESTAL
 - ⊤ INDICATES TELEPHONE PEDESTAL
 - ⊥ INDICATES DECIDUOUS TREE
 - ⊦ INDICATES SEPTIC MANHOLE
 - ⊧ INDICATES WELL
 - INDICATES BITUMINOUS SURFACE



HEREBY CERTIFY THAT THIS SURVEY, PLAN OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY REGISTERED LAND SURVEYOR UNDER THE LAWS OF THE STATE OF MINNESOTA.

KARL M. SILVERS
 MINNESOTA REGISTRATION NO. 57949
 DATE: 10-16-23

NOTE: THIS SURVEY IS INTENDED ONLY FOR THE BENEFIT OF THE PARTY TO WHOM OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY REGISTERED LAND SURVEYOR UNDER THE LAWS OF THE STATE OF MINNESOTA. REPRODUCTION OF THIS DOCUMENT IS PROHIBITED.

1004 2nd ST. SE
 WILLMAR, MN 56201
 PH. 320-235-4012

340 CHAPEL HILL RD.
 COLD SPRING, MN 56320
 PH. 320-685-5905

1250 HWY 15 SOUTH
 HUTCHINSON, MN 55350
 PH. 320-234-1223

CERTIFICATE OF SURVEY PREPARED BY:
O'MALLEY & KRON
LAND SURVEYORS, INC.

JOB NO:2025-611
 FILE NAME:2025-611.DWG
 LOCATION:14-120-22

CERTIFICATE OF SURVEY PREPARED FOR:
MAHLER ARCHITECTURE

SHEET 1 OF 1

DAYTON ASSISTED LIVING

BALSAM LANE NORTH
DAYTON, MINNESOTA

CIVIL CHECK SET DRAWINGS



VICINITY MAP

NTS

EXISTING LINE WORK		PROPOSED LINE WORK		PROPOSED HATCHING	
---#85---	INDICATES EXISTING MAJOR CONTOURS	---937---	INDICATES PROPOSED CONTOUR LINE	[Hatch Pattern]	INDICATES PROPOSED CONCRETE PAVEMENT, APRONS, VALLEY GUTTER, & SIDEWALK
---#06---	INDICATES EXISTING MINOR CONTOURS	---[Symbol]---	INDICATES SANITARY SERVICE LINE	[Hatch Pattern]	INDICATES PROPOSED BITUMINOUS PAVEMENT
---ST---	INDICATES STORM SEWER LINE	---[Symbol]---	INDICATES SANITARY SEWER LINE	[Hatch Pattern]	INDICATES PROPOSED GRAVEL SURFACING
---SN---	INDICATES SANITARY SEWER LINE	---[Symbol]---	INDICATES DRAIN TILE LINE	[Hatch Pattern]	INDICATES TEMPORARY ROCK CONSTRUCTION ENTRANCE
---I---	INDICATES UNDERGROUND WATER	---[Symbol]---	INDICATES STORM SEWER LINE	[Hatch Pattern]	INDICATES TYPE III RIP RAP OVERLAYING TYPE 4 GEOTEXTILE FABRIC
---G---	INDICATES UNDERGROUND GAS	---[Symbol]---	INDICATES WATER MAIN LINE	[Hatch Pattern]	INDICATES CATEGORY 20 EROSION CONTROL BLANKET
---E---	INDICATES UNDERGROUND ELECTRIC	---[Symbol]---	INDICATES WATER SERVICE LINE	[Hatch Pattern]	INDICATES SANITARY SEWER INSULATION
---T---	INDICATES UNDERGROUND TELEPHONE	---[Symbol]---	INDICATES SILT FENCE LINE	[Hatch Pattern]	
---C---	INDICATES UNDERGROUND CABLE	---[Symbol]---	INDICATES SPOT ELEVATION	[Hatch Pattern]	
---X---	INDICATES FENCE LINE	---[Symbol]---	INDICATES STORM CATCH BASIN	[Hatch Pattern]	
⊙	INDICATES STORM MANHOLE	⊙	INDICATES STORM SEWER CATCH BASIN	[Hatch Pattern]	
⊙	INDICATES SANITARY MANHOLE	⊙	INDICATES FLARED END SECTION	[Hatch Pattern]	
[Symbol]	INDICATES CATCH BASIN	⊙	INDICATES SANITARY/STORM CLEANOUT	[Hatch Pattern]	
[Symbol]	INDICATES WATER VALVE	⊙	INDICATES SANITARY/STORM MANHOLE	[Hatch Pattern]	
[Symbol]	INDICATES HYDRANT	⊙	INDICATES NYLOPLAST CATCH BASIN	[Hatch Pattern]	
[Symbol]	INDICATES ELECTRIC PEDESTAL	⊙	INDICATES NYLOPLAST CLEANOUT	[Hatch Pattern]	
[Symbol]	INDICATES TELEPHONE PEDESTAL	⊙	INDICATES NYLOPLAST MANHOLE	[Hatch Pattern]	
[Symbol]	INDICATES CABLE PEDESTAL	⊙	INDICATES OUTLET STRUCTURE	[Hatch Pattern]	
[Symbol]	INDICATES SIGN	⊙	INDICATES RAIN GUARDIAN STRUCTURE	[Hatch Pattern]	
[Symbol]	INDICATES POST	⊙	INDICATES CURB STOP & BOX	[Hatch Pattern]	
[Symbol]	INDICATES ELECTRIC BOX	⊙	INDICATES GATE VALVE	[Hatch Pattern]	
[Symbol]	INDICATES ELECTRIC BOX	⊙	INDICATES HYDRANT	[Hatch Pattern]	
[Symbol]	INDICATES LIGHT/POWER POLE	⊙	INDICATES FLOW ARROW	[Hatch Pattern]	
[Symbol]	INDICATES SEPTIC CLEANOUT	⊙	INDICATES SCOUR STOP PAD	[Hatch Pattern]	
[Symbol]	INDICATES SEPTIC MANHOLE	⊙	INDICATES FILTER LOG	[Hatch Pattern]	

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- C6 - SWPPP - STANDARD NOTES
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PRELIMINARY CIVIL CHECK SET - 03/18/2026

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly licensed Engineer under the laws of the state of Minnesota

Norman E. Stutz, PE
Date: xx/xx/2026 License No.: 63480

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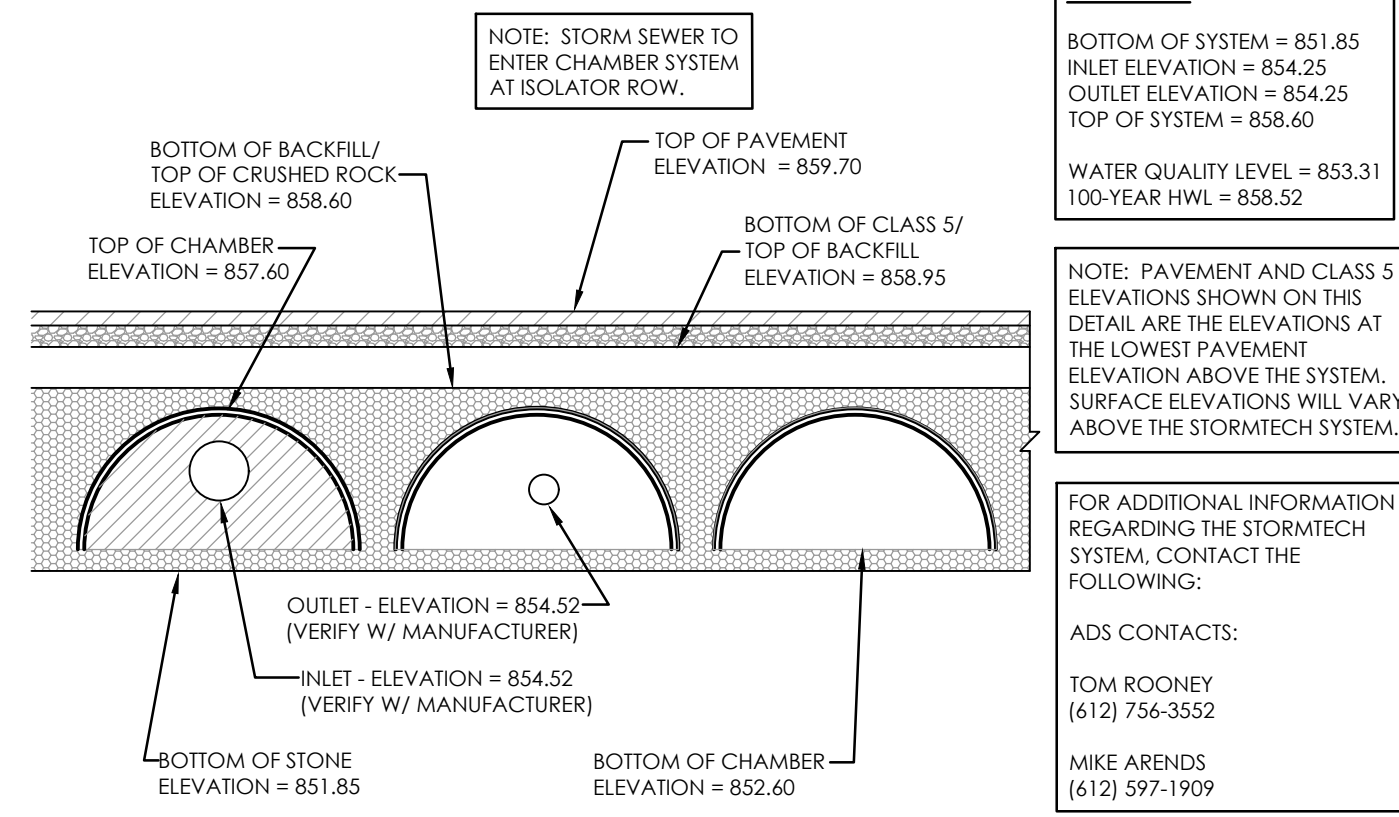
No.	Revision/Issue	Date

Project Name and Address
DAYTON ASSISTED LIVING
DAYTON, MN
C/O BJ BASS
CHAD CAZA
763-691-0444

Project: 26006
Sheet: **TITLE SHEET**
C1 of 9

STORMTECH SYSTEM NOTES:

- TRAFFIC FROM CONSTRUCTION EQUIPMENT SHALL BE LIMITED AS MUCH AS POSSIBLE ACROSS AREA DESIGNATED FOR STORMTECH SYSTEM. AND BE ONLY LOW IMPACT TRACK EQUIPMENT. DESIGNATED AREA SHALL BE EXCAVATED WITH A BACKHOE STATIONED OUTSIDE OF THE AREA AS MUCH AS POSSIBLE.
- THE BOTTOM OF THE STORMTECH EXCAVATION SHALL BE SCARIFIED TO A MINIMUM DEPTH OF 24 INCHES WITH THE USE OF APPROPRIATE EQUIPMENT (TILLER, RIPPER, ETC.) PRIOR TO PLACEMENT OF THE CRUSHED ROCK BASE.
- ONCE THE EXCAVATION AND SCARIFICATION IS COMPLETED, THE CONTRACTOR SHALL ARRANGE AND PAY FOR TESTING THE INFILTRATION RATE OF THE BOTTOM OF THE EXCAVATION PRIOR TO PLACEMENT OF THE CRUSHED ROCK BASE. THE TEST RESULTS SHALL BE SUBMITTED TO THE CITY AND THE ENGINEER.
- STORMTECH SYSTEM SHALL BE INSTALLED IN STRICT ACCORDANCE WITH THE MANUFACTURER'S SPECIFICATIONS AND RECOMMENDATIONS.



NOTE: THE PURPOSE OF THIS DETAIL IS TO SHOW THE RELEVANT ELEVATIONS OF THE STORMTECH MC-4500 UNDERGROUND STORM WATER SYSTEM FOR THIS PROJECT. THE CONTRACTOR SHALL INSTALL THE UNDERGROUND STORM WATER SYSTEM PER SHOP DRAWINGS TO BE PREPARED BY THE SUPPLIER AND APPROVED BY THE ENGINEER. THE SYSTEM SHALL ALSO BE INSTALLED IN STRICT ACCORDANCE WITH ALL OF THE MANUFACTURER'S REQUIREMENTS AND SPECIFICATIONS.

1 UNDERGROUND STORM WATER DETENTION SYSTEM ELEV

GRADING NOTES:

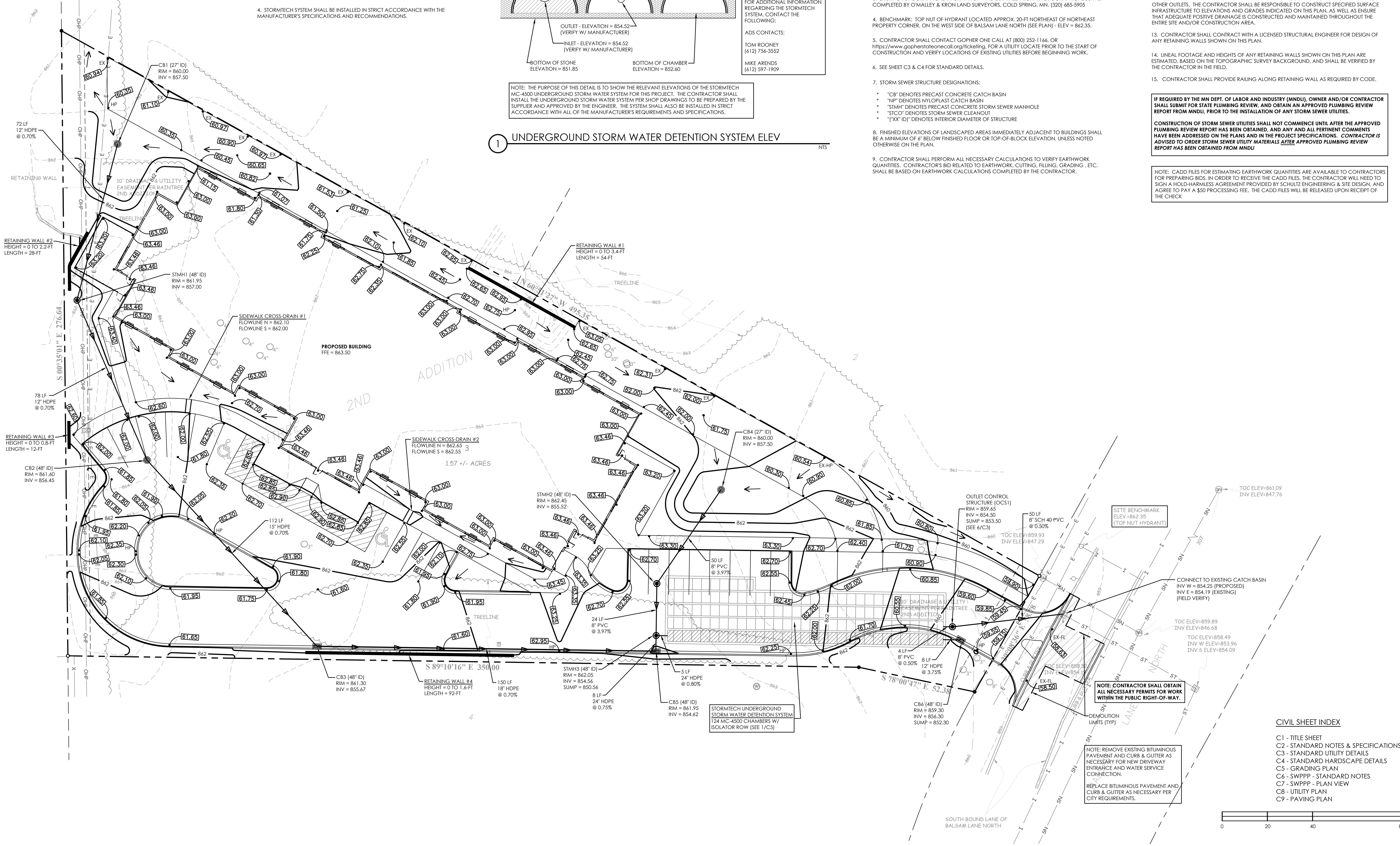
- CONTRACTOR SHALL FIELD VERIFY ALL EXISTING SITE CONDITIONS INCLUDING, BUT NOT LIMITED TO, THE FOLLOWING:
 - LOCATIONS, DEPTHS, SIZES, AND MATERIAL TYPES OF EXISTING SANITARY, WATER, AND STORM SEWER UTILITIES, INCLUDING EXISTING STUBS
 - RIM AND INVERT ELEVATIONS OF EXISTING SANITARY AND STORM SEWER UTILITIES, INCLUDING EXISTING STUBS
 - LOCATIONS AND DEPTHS OF EXISTING NON-SANITARY AND NON-WATER UTILITIES (IE: GAS, ELECTRIC, COMMUNICATIONS, ETC.)
 - EXISTING ABOVE-GROUND INFRASTRUCTURE AND SITE FEATURES
- NOTIFY ENGINEER IMMEDIATELY IF ANY INCONSISTENCIES ARE DISCOVERED BETWEEN ACTUAL SITE CONDITIONS AND WHAT IS SHOWN ON THE PLANS, WHICH ARE SIGNIFICANT ENOUGH TO ALTER THE INTENT OF THE DRAWINGS.
- EXISTING TOPOGRAPHICAL INFORMATION WAS OBTAINED FROM A TOPOGRAPHICAL SURVEY COMPLETED BY O'MALLEY & KRON LAND SURVEYORS, COLD SPRING, MN. (320) 485-5905
- BENCHMARK: TOP NUT OF HYDRANT LOCATED APPROX. 20-FT NORTHEAST OF NORTHEAST PROPERTY CORNER, ON THE WEST SIDE OF BALSAM LANE NORTH (SEE PLAN) - ELEV = 862.35.
- CONTRACTOR SHALL CONTACT GOPHER ONE CALL AT (800) 252-1166, OR <https://www.gopheronecall.org/ticketing>, FOR A UTILITY LOCATE PRIOR TO THE START OF CONSTRUCTION AND VERIFY LOCATIONS OF EXISTING UTILITIES BEFORE BEGINNING WORK.
- SEE SHEET C3 & C4 FOR STANDARD DETAILS.
- STORM SEWER STRUCTURE DESIGNATIONS:
 - "CB" DENOTES PRECAST CONCRETE CATCH BASIN
 - "NP" DENOTES NYLOPLAST CATCH BASIN
 - "STMH" DENOTES PRECAST CONCRETE STORM SEWER MANHOLE
 - "STCO" DENOTES STORM SEWER CLEANOUT
 - "TX" ID" DENOTES INTERIOR DIAMETER OF STRUCTURE
- FINISHED ELEVATIONS OF LANDSCAPED AREAS IMMEDIATELY ADJACENT TO BUILDINGS SHALL BE A MINIMUM OF 6" BELOW FINISHED FLOOR OR TOP-OF-BLOCK ELEVATION, UNLESS NOTED OTHERWISE ON THE PLAN.
- CONTRACTOR SHALL PERFORM ALL NECESSARY CALCULATIONS TO VERIFY EARTHWORK QUANTITIES. CONTRACTOR'S BID RELATED TO EARTHWORK, CUTTING, FILLING, GRADING, ETC. SHALL BE BASED ON EARTHWORK CALCULATIONS COMPLETED BY THE CONTRACTOR.

- SPOT ELEVATIONS INDICATED ON THIS PLAN REPRESENT FINISHED SURFACE ELEVATIONS, UNLESS NOTED OTHERWISE.
- SPOT ELEVATIONS LOCATED ALONG CURB & GUTTER OR THICKENED EDGE SIDEWALK REPRESENT FLOWLINE ELEVATIONS OF GUTTER SECTION OR ADJACENT PAVEMENT. TOP-OF-CURB OR TOP-OF-THICKENED EDGE SIDEWALK SURFACE ELEVATIONS ARE 6" ABOVE THE FLOW LINE SPOT ELEVATION SHOWN ON THE PLANS, UNLESS NOTED OTHERWISE.
- SPOT ELEVATION DESIGNATIONS:
 - "EX" DENOTES EXISTING SPOT ELEVATION
 - "HP" DENOTES HIGH POINT
 - "FL" DENOTES FLOWLINE
 - "BC" DENOTES BACK OF CURB
 - "EC" DENOTES EDGE OF CONCRETE
 - "EP" DENOTES EDGE OF PAVEMENT
- ALL SPOT ELEVATIONS AND CONTOURS INDICATED ON THIS PLAN ARE INTENDED TO PROVIDE ADEQUATE POSITIVE DRAINAGE TOWARDS CATCH BASINS, CURB CUTS, PONDS, BASINS AND/OR OTHER OUTLETS. THE CONTRACTOR SHALL BE RESPONSIBLE TO CONSTRUCT SPECIFIED SURFACE INFRASTRUCTURE TO ELEVATIONS AND GRADES INDICATED ON THIS PLAN, AS WELL AS ENSURE THAT ADEQUATE POSITIVE DRAINAGE IS CONSTRUCTED AND MAINTAINED THROUGHOUT THE ENTIRE SITE AND/OR CONSTRUCTION AREA.
- CONTRACTOR SHALL CONTRACT WITH A LICENSED STRUCTURAL ENGINEER FOR DESIGN OF ANY RETAINING WALLS SHOWN ON THIS PLAN.
- LINEAL FOOTAGE AND HEIGHTS OF ANY RETAINING WALLS SHOWN ON THIS PLAN ARE ESTIMATED, BASED ON THE TOPOGRAPHIC SURVEY BACKGROUND, AND SHALL BE VERIFIED BY THE CONTRACTOR IN THE FIELD.
- CONTRACTOR SHALL PROVIDE RAILING ALONG RETAINING WALL AS REQUIRED BY CODE.

IF REQUIRED BY THE MN DEPT. OF LABOR AND INDUSTRY (MNDLI), OWNER AND/OR CONTRACTOR SHALL SUBMIT FOR STATE PLUMBING REVIEW, AND OBTAIN AN APPROVED PLUMBING REVIEW REPORT FROM MNDLI, PRIOR TO THE INSTALLATION OF ANY STORM SEWER UTILITIES.

CONSTRUCTION OF STORM SEWER UTILITIES SHALL NOT COMMENCE UNTIL AFTER THE APPROVED PLUMBING REVIEW REPORT HAS BEEN OBTAINED, AND ANY AND ALL PERTINENT COMMENTS HAVE BEEN ADDRESSED ON THE PLANS AND IN THE PROJECT SPECIFICATIONS. CONTRACTOR IS ADVISED TO ORDER STORM SEWER UTILITY MATERIALS AFTER APPROVED PLUMBING REVIEW REPORT HAS BEEN OBTAINED FROM MNDLI.

NOTE: CADD FILES FOR ESTIMATING EARTHWORK QUANTITIES ARE AVAILABLE TO CONTRACTORS FOR PREPARING BIDS. IN ORDER TO RECEIVE THE CADD FILES, THE CONTRACTOR WILL NEED TO SIGN A HOLD-HARMLESS AGREEMENT PROVIDED BY SCHULTZ ENGINEERING & SITE DESIGN, AND AGREE TO PAY A \$50 PROCESSING FEE. THE CADD FILES WILL BE RELEASED UPON RECEIPT OF THE CHECK.



PRELIMINARY CIVIL CHECK SET - 03/18/2026

PRELIMINARY - NOT FOR CONSTRUCTION

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly licensed Engineer under the laws of the state of Minnesota

Norman E. Stutz, PE
 Date: xxx/xx/2026 License No.: 63480

SCHULTZ ENGINEERING & SITE DESIGN

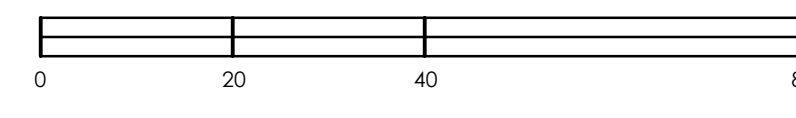
18 South Riverside Avenue
 Suite 230
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 Phone: (612) 339-0669
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 Email: nschultz@se.com
 www.schultzengineering.com

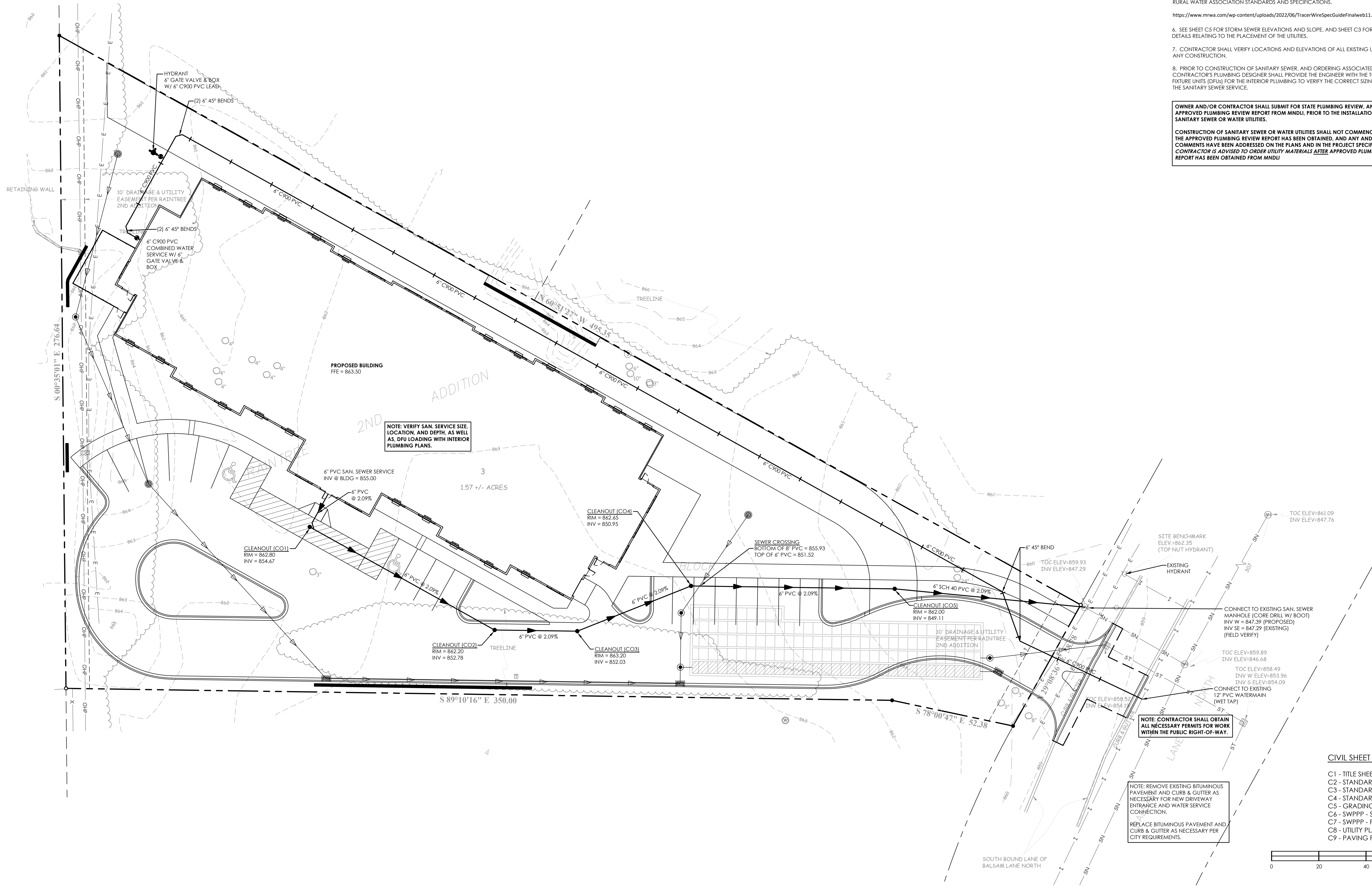
No.	Revision/Issue	Date

DAYTON ASSISTED LIVING
 DAYTON, MN
 C/O BU BASS
 CHAD CAZA
 763-691-0444

Project: 26006
 Scale: 1"=20'
 Sheet: GRADING PLAN
 C5 of 9

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NOTE: VERIFY SAN. SERVICE SIZE, LOCATION, AND DEPTH, AS WELL AS, DPU LOADING WITH INTERIOR PLUMBING PLANS.

NOTE: REMOVE EXISTING BITUMINOUS PAVEMENT AND CURB & GUTTER AS NECESSARY FOR NEW DRIVEWAY ENTRANCE AND WATER SERVICE CONNECTION.
 REPLACE BITUMINOUS PAVEMENT AND CURB & GUTTER AS NECESSARY PER CITY REQUIREMENTS.

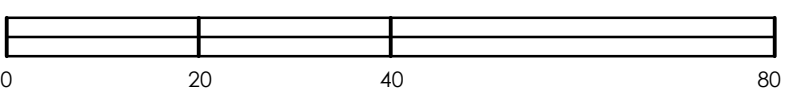
NOTE: CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS FOR WORK WITHIN THE PUBLIC RIGHT-OF-WAY.

- UTILITY NOTES:**
1. WATER MAIN AND ANY WATER SERVICE LINES SHALL BE PLACED AT A MINIMUM DEPTH OF 8 FEET BELOW FINISHED GRADE.
 2. IF CONFLICTS ARE DISCOVERED WHERE WATER MAIN OR SERVICES CROSS EXISTING OR PROPOSED SANITARY SEWER, SERVICE LINES, OR STORM SEWER, THE WATER MAIN SHALL BE RAISED OR LOWERED APPROPRIATELY WHILE STILL MAINTAINING A MINIMUM DEPTH OF 8 FEET BELOW FINISHED GRADE.
 3. INSULATION SHALL BE PLACED AT ALL LOCATIONS WHERE STORM SEWER CROSSES SANITARY SEWER, WATER MAIN, OR ASSOCIATED SERVICES. INSULATION SHALL CONSIST OF AN 8-FT X 8-FT SQUARE OF 3"-THICK RIGID INSULATION. INSULATION SHALL BE PLACED BETWEEN THE STORM SEWER AND PIPE CROSSING WITH THE EDGES OF THE INSULATION PARALLEL TO THE PIPING AS MUCH AS POSSIBLE.
 4. INSTALL SANITARY SEWER SERVICE LINE CLEANOUTS AS REQUIRED BY THE MINNESOTA PLUMBING CODE.
 5. INSTALL TRACER WIRE ALONG PROPOSED UNDERGROUND UTILITY PIPE AS PER MINNESOTA RURAL WATER ASSOCIATION STANDARDS AND SPECIFICATIONS.
<https://www.mrwa.com/wp-content/uploads/2022/06/TracerWireSpecGuideFinalweb11.pdf>
 6. SEE SHEET C5 FOR STORM SEWER ELEVATIONS AND SLOPE, AND SHEET C3 FOR STANDARD DETAILS RELATING TO THE PLACEMENT OF THE UTILITIES.
 7. CONTRACTOR SHALL VERIFY LOCATIONS AND ELEVATIONS OF ALL EXISTING UTILITIES PRIOR TO ANY CONSTRUCTION.
 8. PRIOR TO CONSTRUCTION OF SANITARY SEWER, AND ORDERING ASSOCIATED MATERIALS, THE CONTRACTOR'S PLUMBING DESIGNER SHALL PROVIDE THE ENGINEER WITH THE TOTAL DRAINAGE FIXTURE UNITS (DFU) FOR THE INTERIOR PLUMBING TO VERIFY THE CORRECT SIZING AND SLOPE OF THE SANITARY SEWER SERVICE.

OWNER AND/OR CONTRACTOR SHALL SUBMIT FOR STATE PLUMBING REVIEW, AND OBTAIN AN APPROVED PLUMBING REVIEW REPORT FROM MNDU, PRIOR TO THE INSTALLATION OF ANY SANITARY SEWER OR WATER UTILITIES.
 CONSTRUCTION OF SANITARY SEWER OR WATER UTILITIES SHALL NOT COMMENCE UNTIL AFTER THE APPROVED PLUMBING REVIEW REPORT HAS BEEN OBTAINED, AND ANY AND ALL PERTINENT COMMENTS HAVE BEEN ADDRESSED ON THE PLANS AND IN THE PROJECT SPECIFICATIONS.
 CONTRACTOR IS ADVISED TO ORDER UTILITY MATERIALS AFTER APPROVED PLUMBING REVIEW REPORT HAS BEEN OBTAINED FROM MNDU.

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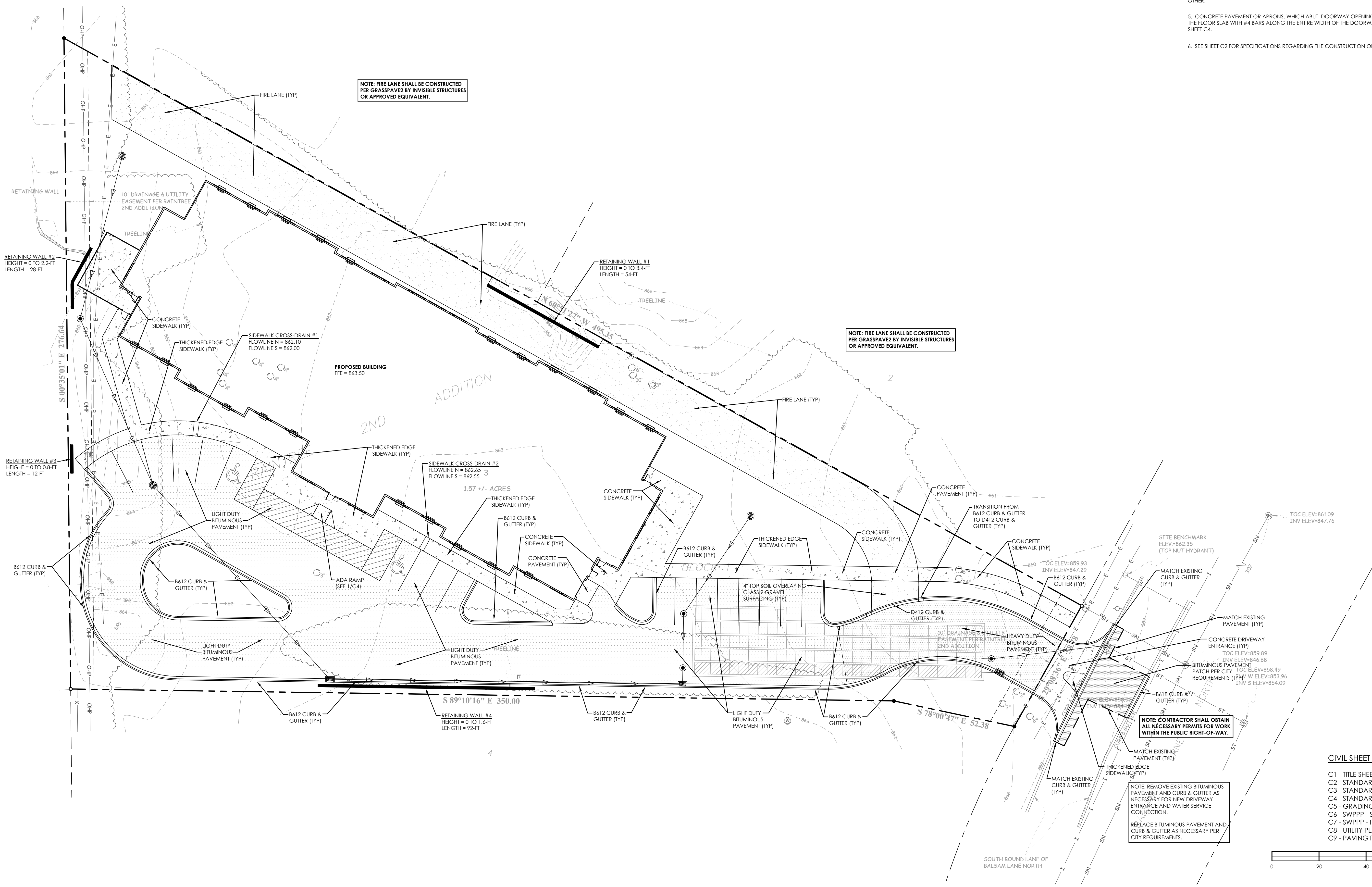
PRELIMINARY CIVIL CHECK SET - 03/18/2026
 I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly licensed Engineer under the laws of the state of Minnesota.
 Norman E. Stutz, PE
 Date: xxx/xx/2026 License No.: 63480

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 Website: www.schultzengineering.com

No.	Revision/Issue	Date

DAYTON ASSISTED LIVING
 DAYTON, MN
 C/O BU BASS
 CHAD CAZA
 763-691-0444

Project: 26006
 Scale: 1"=20'
 Sheet: UTILITY PLAN
 C8 of 9



- SURFACING NOTES:**
1. SUBGRADES SHALL BE SCARIFIED AND COMPACTED AS NECESSARY TO ATTAIN THE REQUIRED COMPACTION INDICATED IN THE GENERAL NOTES (SHEET C1). TEST ROLLING OF THE SUBGRADE SHALL BE OBSERVED BY A QUALIFIED GEOTECHNICAL ENGINEER OR TECHNICIAN. LOCATIONS EXHIBITING EXCESSIVE RUTTING (PER MNDOT SPEC. 2111) SHALL BE REPAIRED TO THE SATISFACTION OF THE ENGINEER PRIOR TO THE PLACEMENT OF AGGREGATE BASE. COMPACTION TESTING IN UTILITY TRENCHES SHALL BE PERFORMED BY AN INDEPENDENT TESTING FIRM.
 2. AGGREGATE BASE AND SURFACE COURSES SHALL BE ROLLED AND COMPACTED. TEST ROLLING OF THE AGGREGATE BASE AND/OR SURFACING SHALL BE OBSERVED BY A SOILS ENGINEER TO VERIFY STABILITY. MAXIMUM ALLOWED DEFLECTION DURING TEST ROLLING OF AGGREGATE BASE OR SURFACE COURSES SHALL BE 1/2".
 3. ALL EXISTING BITUMINOUS OR CONCRETE EDGES, WHICH WILL ABUT NEW BITUMINOUS OR CONCRETE SURFACING SHALL BE SAWCUT TO OBTAIN A VERTICAL EDGE.
 4. EXPANSION JOINTS SHALL BE PLACED AT ALL LOCATIONS WHERE NEW CONCRETE ABUTS EXISTING CONCRETE, AND AT ALL LOCATIONS WHERE SEPARATE CONCRETE POURS ABUT EACH OTHER.
 5. CONCRETE PAVEMENT OR APRONS, WHICH ABUT DOORWAY OPENINGS SHALL BE TIED TO THE FLOOR SLAB WITH #4 BARS ALONG THE ENTIRE WIDTH OF THE DOORWAY. SEE DETAIL ON SHEET C4.
 6. SEE SHEET C2 FOR SPECIFICATIONS REGARDING THE CONSTRUCTION OF HARDSCAPES.

PRELIMINARY - NOT FOR CONSTRUCTION PRELIMINARY CIVIL CHECK SET - 03/18/2026

I hereby certify that this plan, specification and report was prepared by me or under my direct supervision and that I am a duly licensed Engineer under the laws of the state of Minnesota

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St. Paul, MN 55107
www.schultzengdesign.com

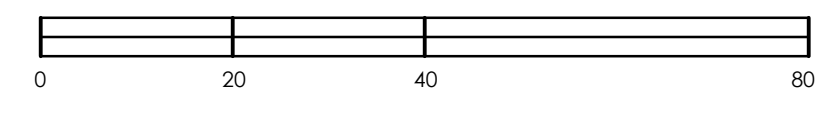
Ph: (620) 335-0669
Fax: (666) 633-1830
nschultzengdesign@gmail.com

No.	Revision/Issue	Date

DAYTON ASSISTED LIVING
DAYTON, MN
C/O BJ BASS
CHAD CAZA
763-691-0444

CIVIL SHEET INDEX

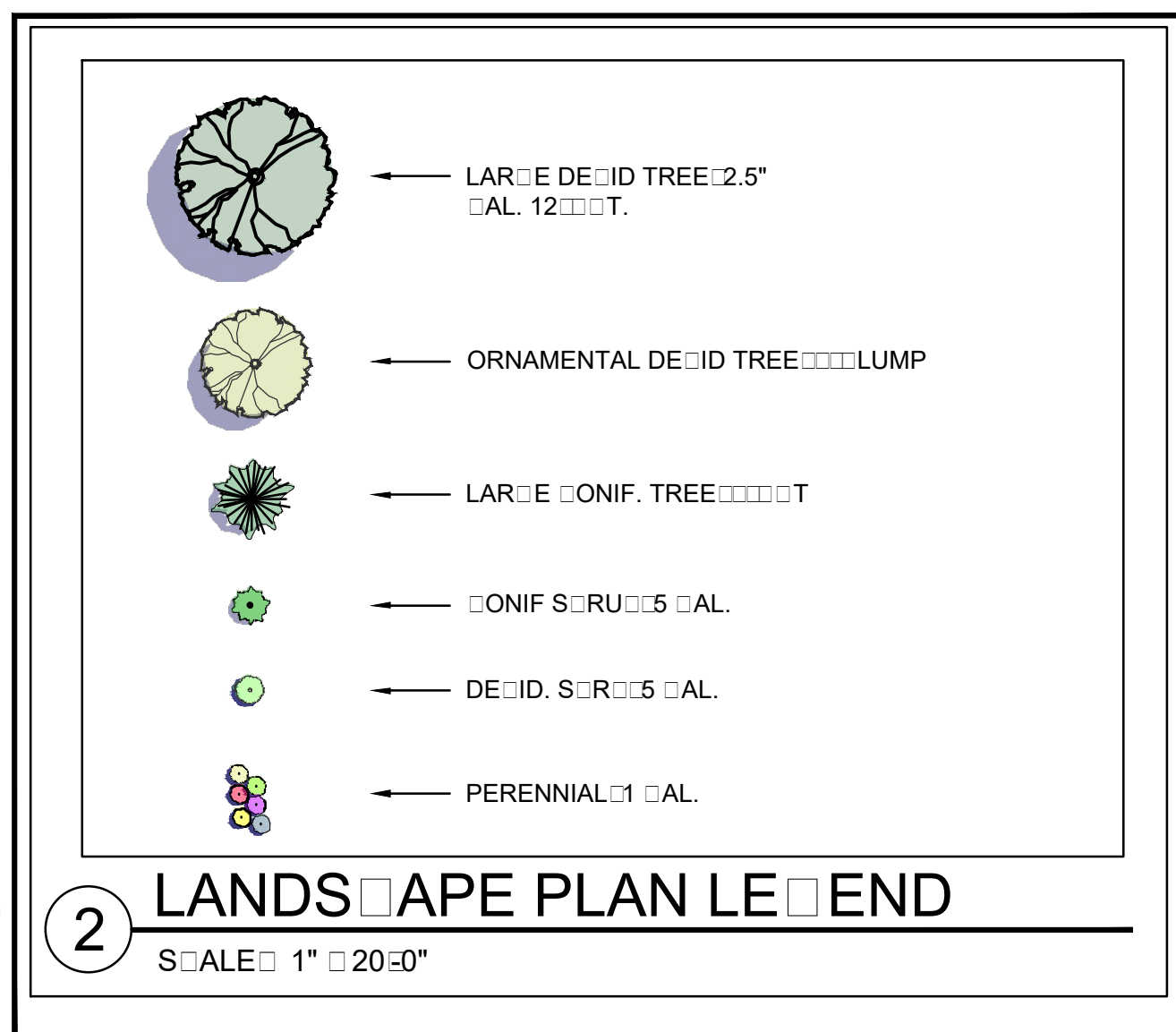
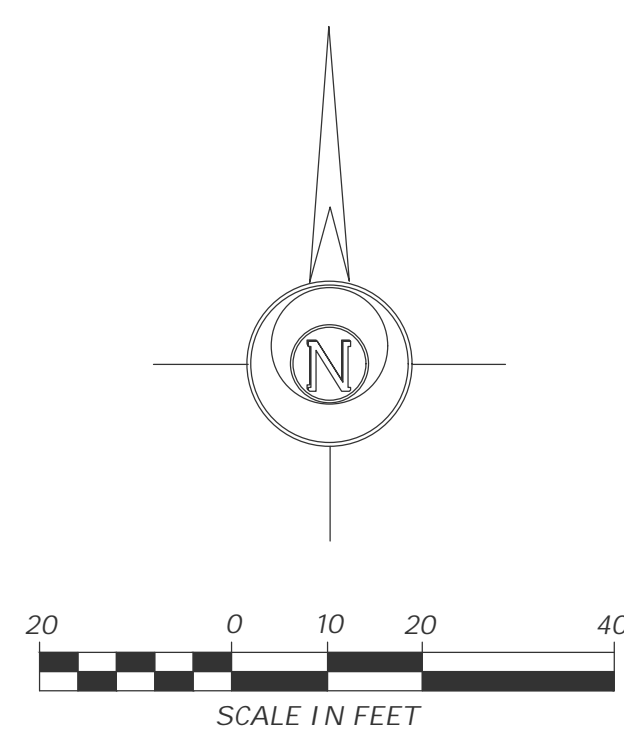
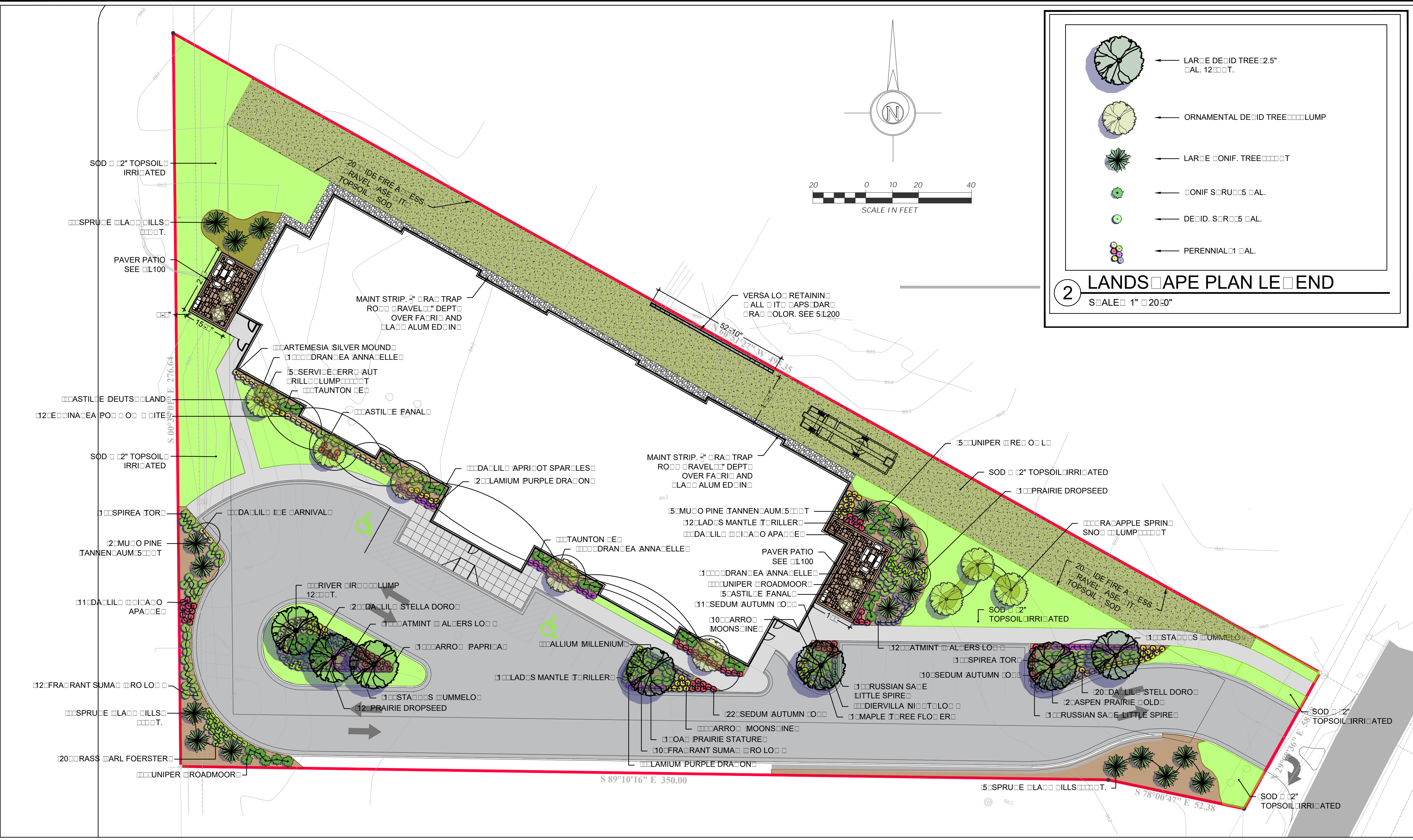
- C1 - TITLE SHEET
- C2 - STANDARD NOTES & SPECIFICATIONS
- C3 - STANDARD UTILITY DETAILS
- C4 - STANDARD HARDSCAPE DETAILS
- C5 - GRADING PLAN
- C6 - SWPPP - STANDARD NOTES
- C7 - SWPPP - PLAN VIEW
- C8 - UTILITY PLAN
- C9 - PAVING PLAN



Project: 26006
Scale: 1"=20'

Sheet: PAVING PLAN

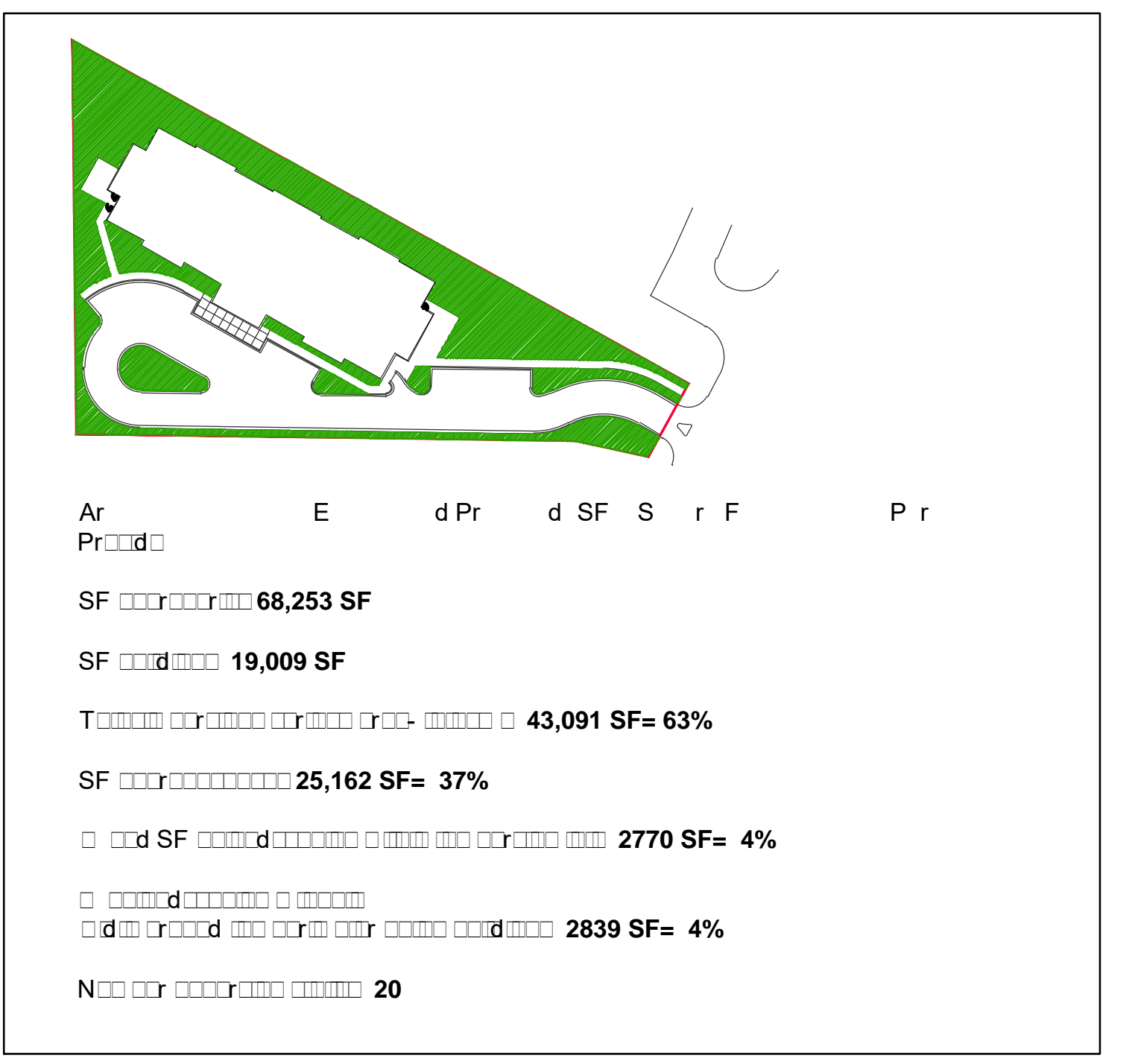
C9 of 9



ANOP TREES: 2.5" CALIPER: 12' T
RIVER IR LUMP
2 ASPEN PRAIRIE OLD LUMP
1 OAS PRAIRIE STATURE LUMP
ORNAMENTAL TREES: T LUMP
5 SERVICE BERRY AUT RILL LUMP T
2 RA APPLE SPRING SNO LUMP T
1 MAPLE TREE FLOWER T
EVERGREEN TREES: T
1 MUO PINE TANNEN AUM T
11 SPRUCE LADILLS T
SHRUBS: T
DIERVILLA NITLO
UNIPER ROADMOOR
5 UNIPER REO L
1 TAUNTON E
2 DRANEA ANNAELLE
2 FRAGRANT SUMA ORO L
5 SPIREA TOR
PERENNIALS: T
DA LIL APRICOT SPARLES
ARTEMESIA SILVER MOUND
DA LIL DE CARNIVAL
ASTILE DEUTS LAND
2 E INAEA PO O OITE
12 E INAEA PO O OITE
20 RASS ARL FOERSTER
2 PRAIRIE DROPSEED
1 LAMUM PURPLE DRAGON
DA LIL STELLA DORO
25 ATMINT ALTERS LO
1 ARRO PAPRI
ALLIUM MILLENIUM
0 STAGS UMMELO
1 LADS MANTLE RILLER
SEDUM AUTUM O
ARRO MOONSINE
ASTILE FANAL
12 LADS MANTLE RILLER
1 RUSSIAN SAE LITTLE SPIRE

PLANT SCHEDULE
 SCALE 1" = 20'-0"

LANDSCAPE PLAN
 SCALE 1" = 20'-0"



SITE CALCULATIONS
 SCALE IN TS

BEACON HILL FLAG

Inspired by natural flagstone, the interlocking and distinctive texture of Beacon Hill Flagstone has a natural look and feel, with all the benefits of a synthetic technology.

The surface texture is comfortable to walk on in bare feet, but appears more characteristic to the sight of natural stone. It is a modern take on the traditional flagstone, designed to be used in a variety of applications for patios, walkways, pool surrounds and walls.

www.belanddesigns.com

PAVERS
 SCALE NTS

BE
 LANDSCAPE DESIGNS

Ben Erickson, Landscape Architect
 www.belanddesigns.com
 ph 612-382-9802
 email: ben@belanddesigns.com

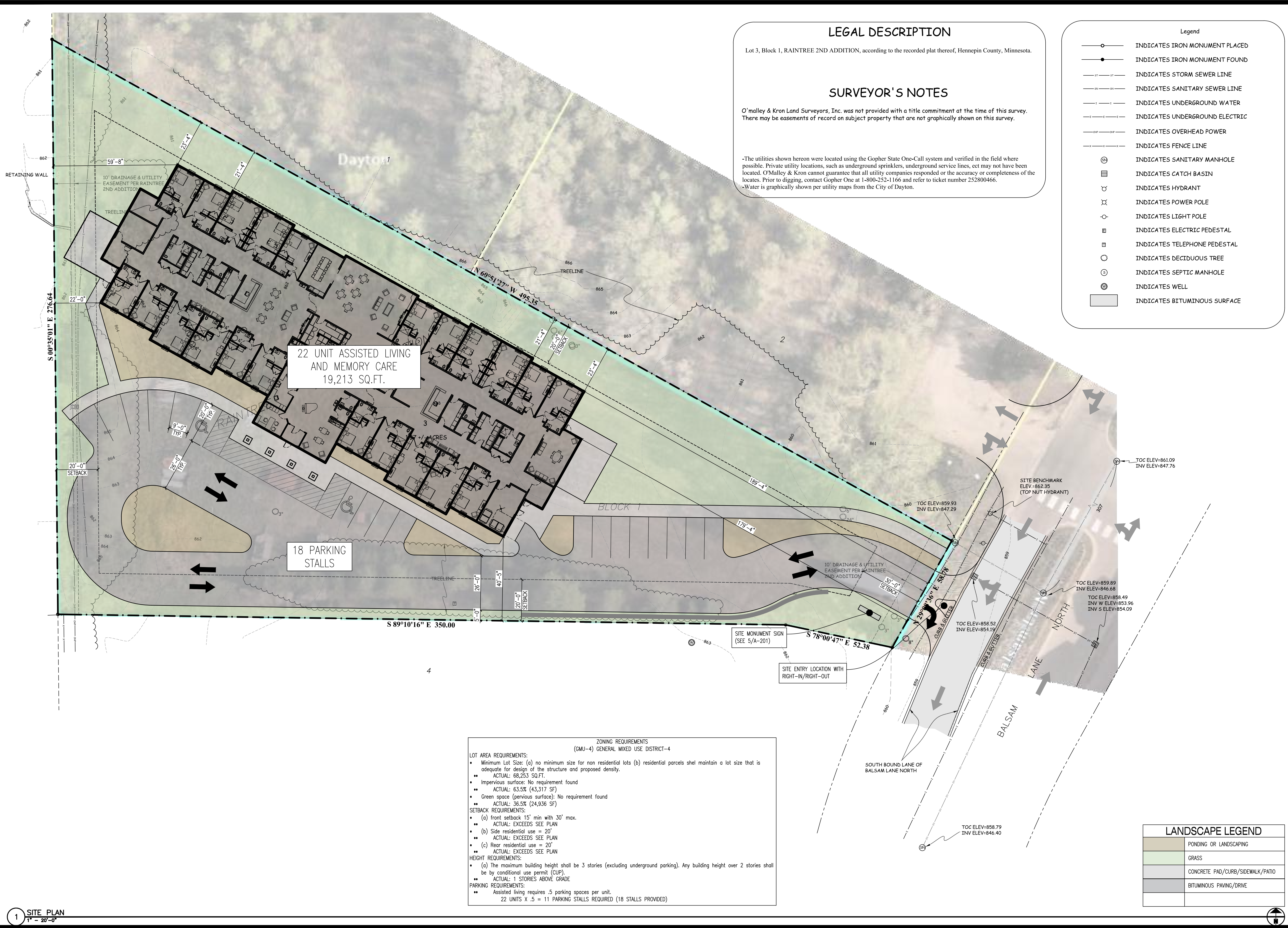
I hereby certify that this plan was prepared by me or under my direct supervision and that I am a duly registered Landscape Architect in the State of Minnesota.

FOR REVIEW

SIGNATURE _____
 REGISTRATION # 50130
 DATE _____

**L100
 LANDSCAPE
 PLAN**

**DATON ASSISTED LIVING
 DATON MN
 T 20-25-22**



LEGAL DESCRIPTION

Lot 3, Block 1, RAINTREE 2ND ADDITION, according to the recorded plat thereof, Hennepin County, Minnesota.

SURVEYOR'S NOTES

O'Malley & Kron Land Surveyors, Inc. was not provided with a title commitment at the time of this survey. There may be easements of record on subject property that are not graphically shown on this survey.

-The utilities shown hereon were located using the Gopher State One-Call system and verified in the field where possible. Private utility locations, such as underground sprinklers, underground service lines, ect may not have been located. O'Malley & Kron cannot guarantee that all utility companies responded or the accuracy or completeness of the locates. Prior to digging, contact Gopher One at 1-800-252-1166 and refer to ticket number 252800466.
 -Water is graphically shown per utility maps from the City of Dayton.

Legend

- INDICATES IRON MONUMENT PLACED
- INDICATES IRON MONUMENT FOUND
- |—|— INDICATES STORM SEWER LINE
- |—|— INDICATES SANITARY SEWER LINE
- |—|— INDICATES UNDERGROUND WATER
- |—|— INDICATES UNDERGROUND ELECTRIC
- |—|— INDICATES OVERHEAD POWER
- |—|— INDICATES FENCE LINE
- ⊕ INDICATES SANITARY MANHOLE
- ⊞ INDICATES CATCH BASIN
- ⊕ INDICATES HYDRANT
- ⊕ INDICATES POWER POLE
- ⊕ INDICATES LIGHT POLE
- ⊕ INDICATES ELECTRIC PEDESTAL
- ⊕ INDICATES TELEPHONE PEDESTAL
- ⊕ INDICATES DECIDUOUS TREE
- ⊕ INDICATES SEPTIC MANHOLE
- ⊕ INDICATES WELL
- INDICATES BITUMINOUS SURFACE

22 UNIT ASSISTED LIVING AND MEMORY CARE
19,213 SQ.FT.

18 PARKING STALLS

ZONING REQUIREMENTS
(GMU-4) GENERAL MIXED USE DISTRICT-4

LOT AREA REQUIREMENTS:

- Minimum Lot Size: (a) no minimum size for non residential lots (b) residential parcels shall maintain a lot size that is adequate for design of the structure and proposed density.
- ACTUAL: 68,253 SQ.FT.
- Impervious surface: No requirement found
- ACTUAL: 63.5% (43,317 SF)
- Green space (pervious surface): No requirement found
- ACTUAL: 36.5% (24,936 SF)

SETBACK REQUIREMENTS:

- (a) front setback 15' min with 30' max.
- ACTUAL: EXCEEDS SEE PLAN
- (b) Side residential use = 20'
- ACTUAL: EXCEEDS SEE PLAN
- (c) Rear residential use = 20'
- ACTUAL: EXCEEDS SEE PLAN

HEIGHT REQUIREMENTS:

- (a) The maximum building height shall be 3 stories (excluding underground parking). Any building height over 2 stories shall be by conditional use permit (CUP).
- ACTUAL: 1 STORIES ABOVE GRADE

PARKING REQUIREMENTS:

- Assisted living requires .5 parking spaces per unit.
- 22 UNITS X .5 = 11 PARKING STALLS REQUIRED (18 STALLS PROVIDED)

LANDSCAPE LEGEND

[Pattern]	PONDING OR LANDSCAPING
[Pattern]	GRASS
[Pattern]	CONCRETE PAD/CURB/SIDEWALK/PATIO
[Pattern]	BITUMINOUS PAVING/DRIVE

MAHLER & ASSOCIATES
ARCHITECTURE

Greene H.D. Mahler, AIA
Principal

5150 Marson Drive
Suite 101
Sauk Rapids, MN 56379

TEL: (320) 257-2724
EMAIL: gmahler@mahlerarchitecture.com

PRELIMINARY
NOT FOR
CONSTRUCTION

No.	Revision/Issue	Date

Project Name and Address
DAYTON ASSISTED LIVING
DAYTON, MN
C/O BJ BASS
CHAD CAZA
763-691-0444

Project: 2528
Date: 03/18/2026
Scale: AS INDICATED

Sheet: SITE PLAN

AS-101



UNIT MIX	18
SINGLE UNITS	4
DOUBLE UNITS	4
TOTAL	22

1 1ST FLOOR PLAN OVERALL
3/32" = 1'-0"

MAHLER & ASSOCIATES
ARCHITECTURE

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Principal

5150 Marston Drive
Suite 101
Sauk Rapids, MN 56379

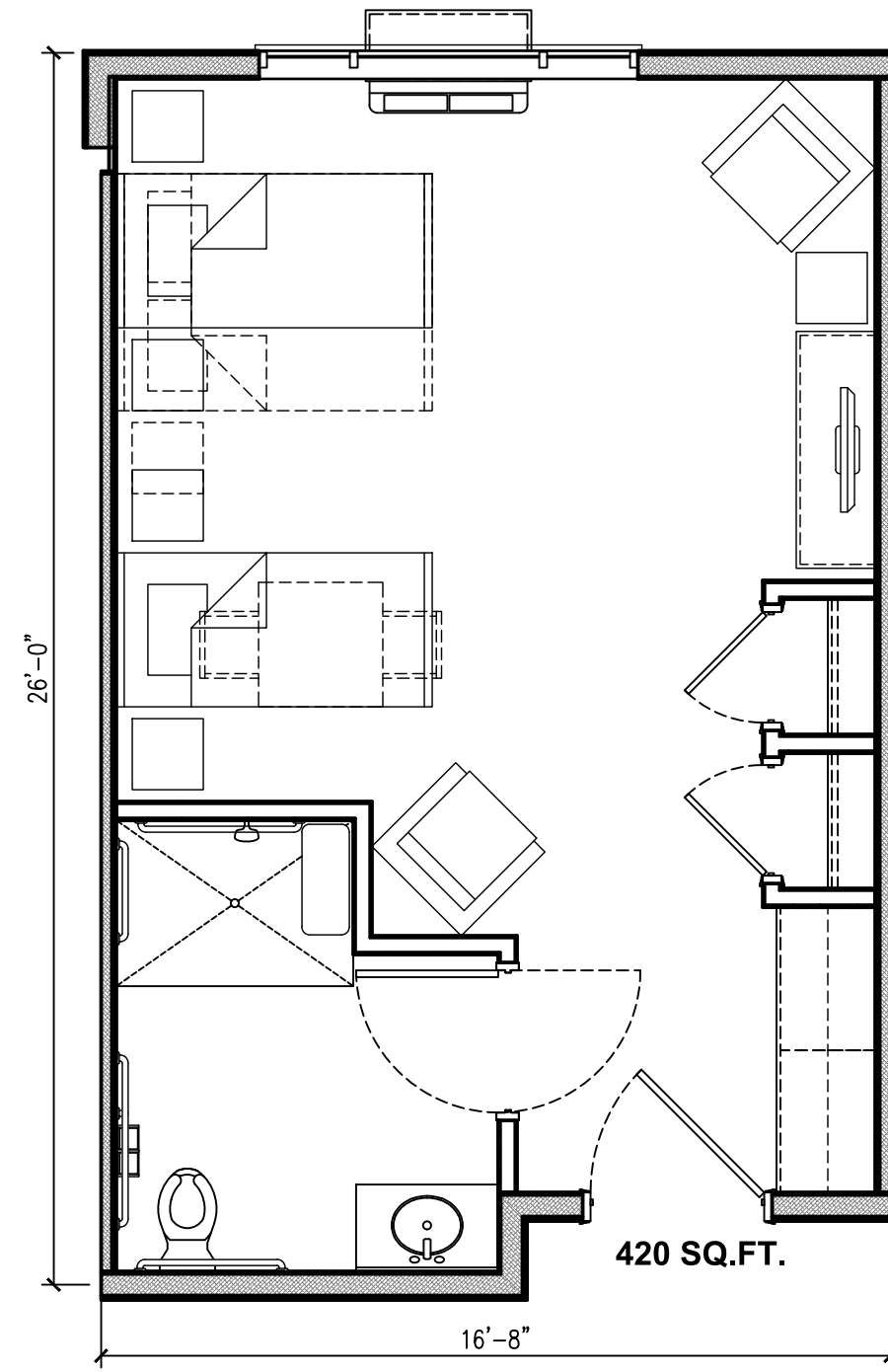
TEL: (320) 257-2724
EMAIL: gmahler@mahlerarchitecture.com

PRELIMINARY
NOT FOR
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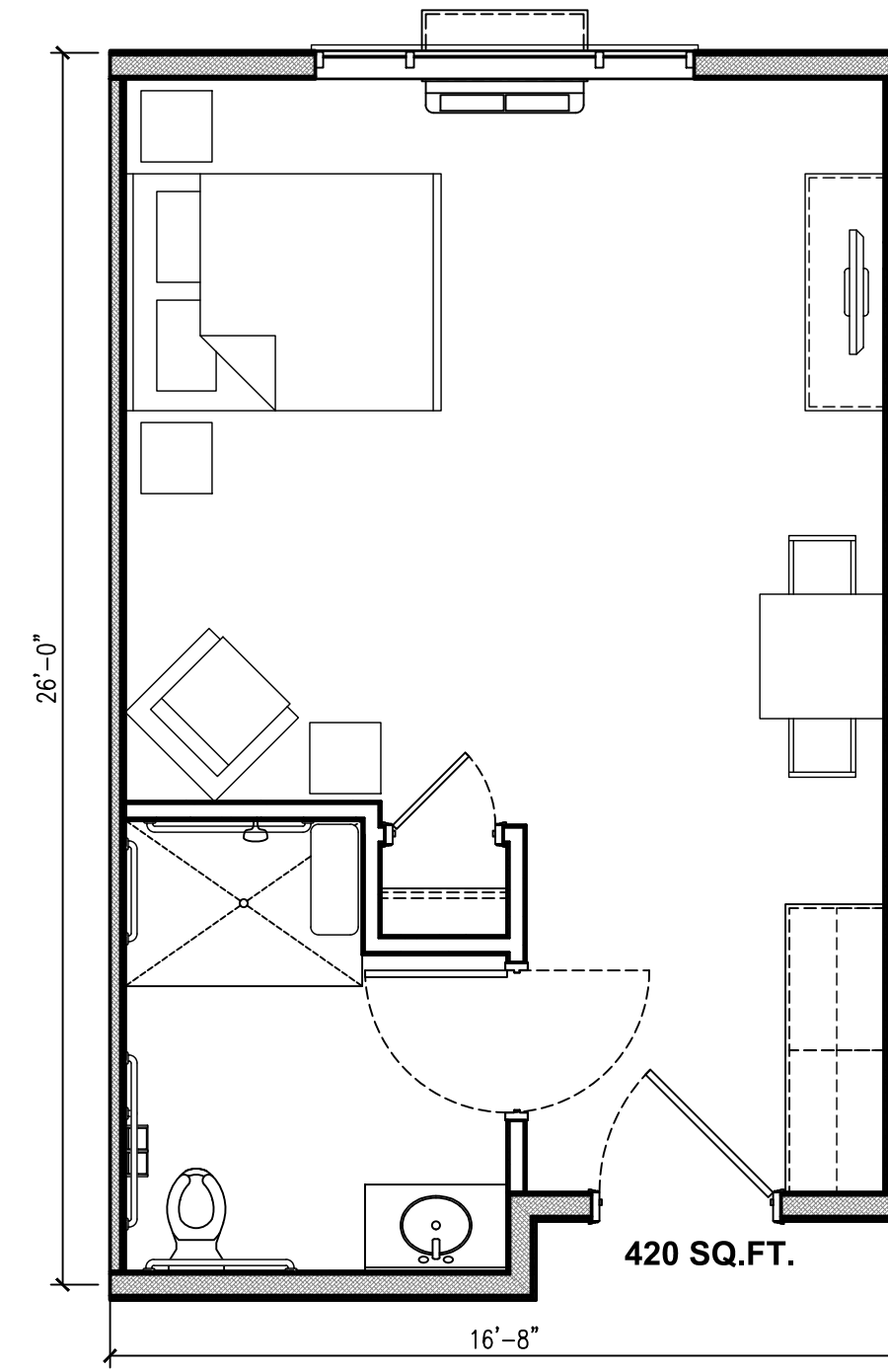
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Project Name and Address
DAYTON ASSISTED LIVING
DAYTON, MN
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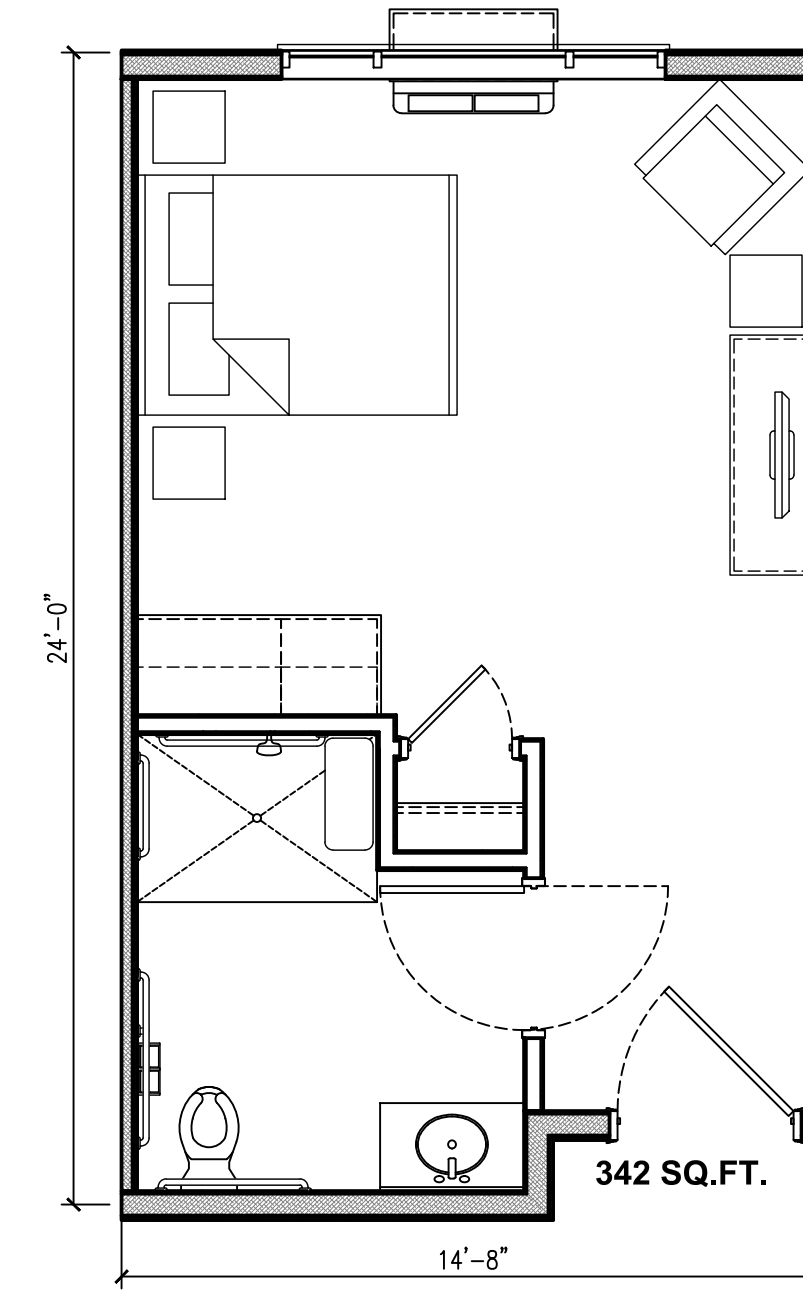
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Date	03/18/2026
Scale	AS INDICATED
Sheet	OVERALL FLOOR PLAN
A-101	



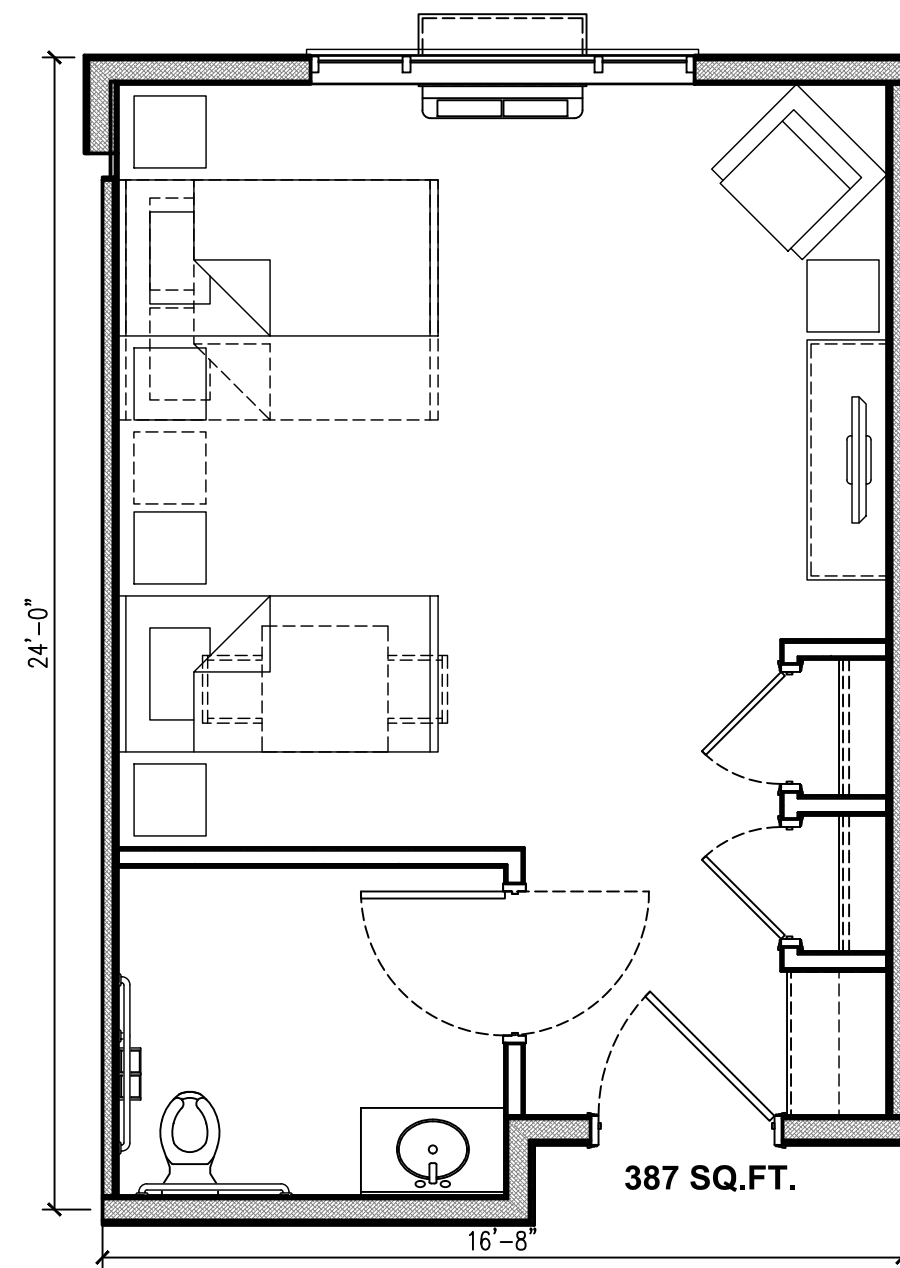
3 ASSISTED DOUBLE
1/4" = 1'-0"



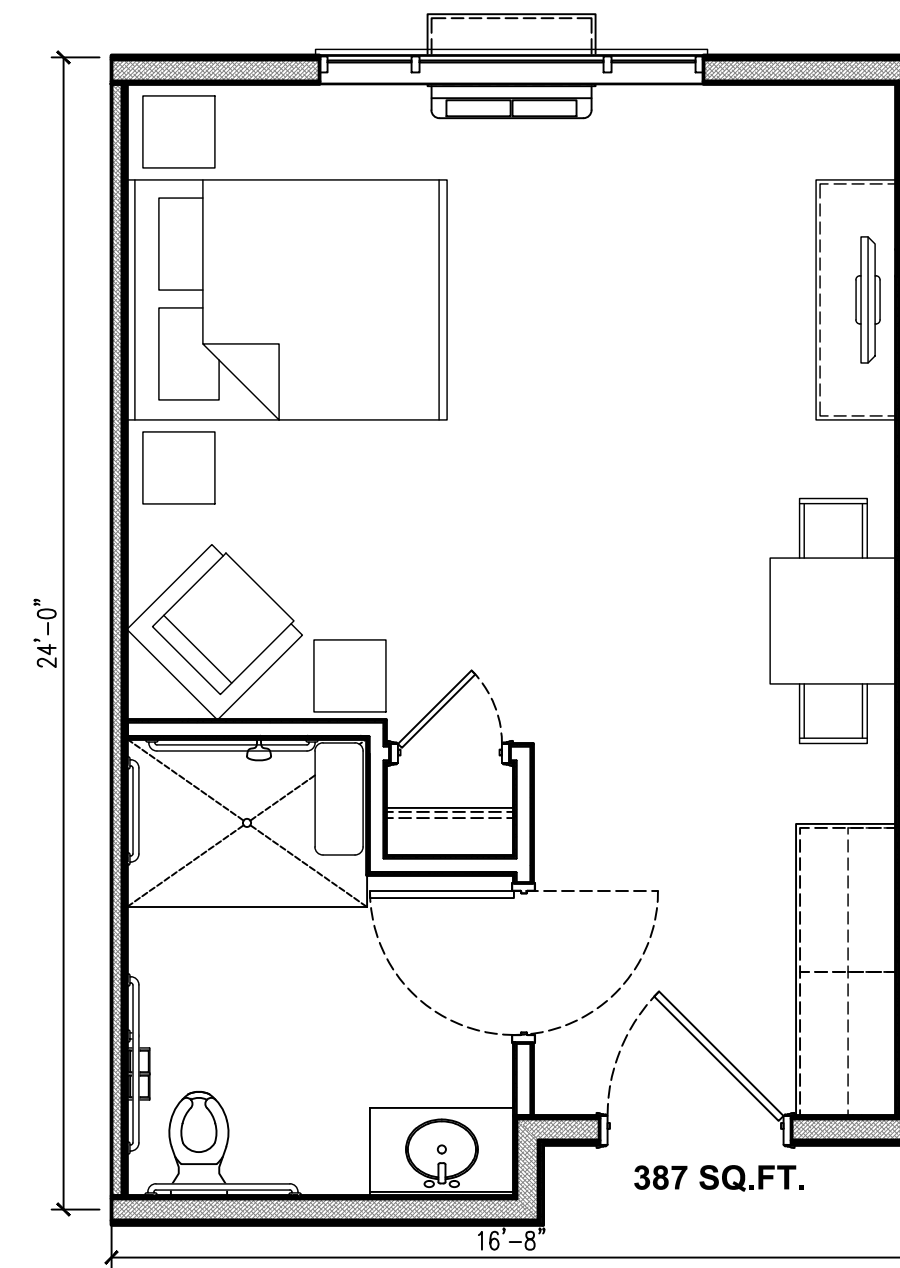
2 ASSISTED SUITE
1/4" = 1'-0"



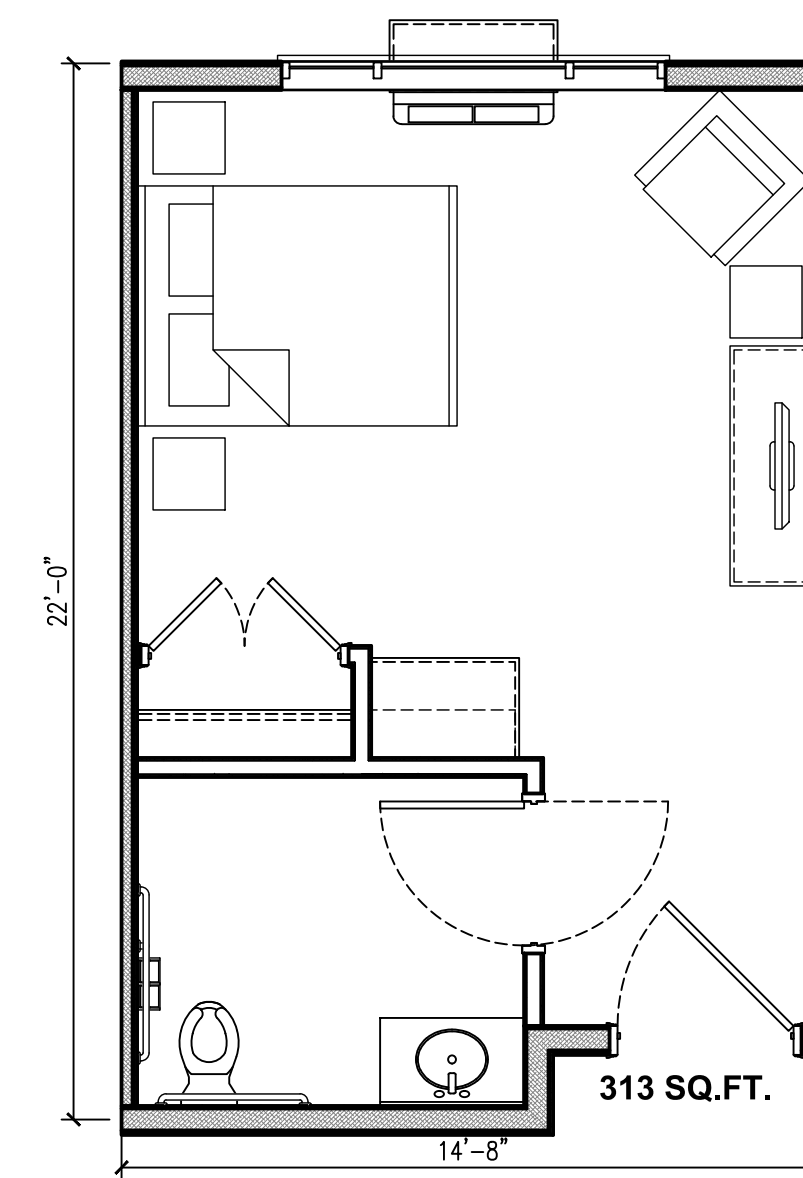
1 ASSISTED SINGLE
1/4" = 1'-0"



6 MEMORY DOUBLE
1/4" = 1'-0"



5 MEMORY SUITE
1/4" = 1'-0"



4 MEMORY SINGLE
1/4" = 1'-0"

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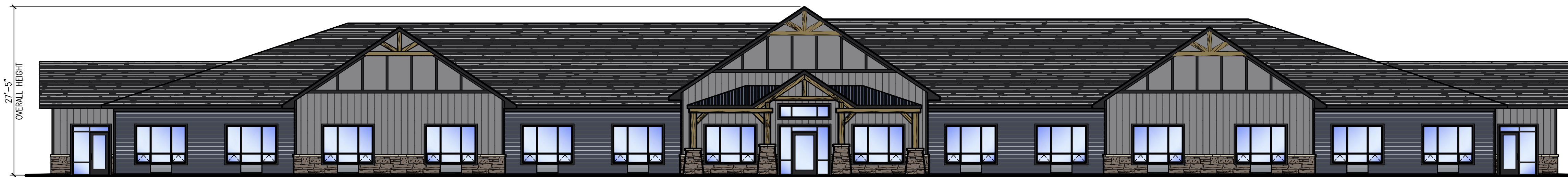
PRELIMINARY
NOT FOR
CONSTRUCTION

No.	Revision/Issue	Date

Project Name and Address
DAYTON ASSISTED LIVING
DAYTON, MN
C/O BJ BASS
CHAD CAZA
763-691-0444

Project: 2528
Date: 03/18/2026
Scale: AS INDICATED

Sheet: UNIT PLANS
A-105.1



1 SOUTH ELEVATION
3/32" = 1'-0"



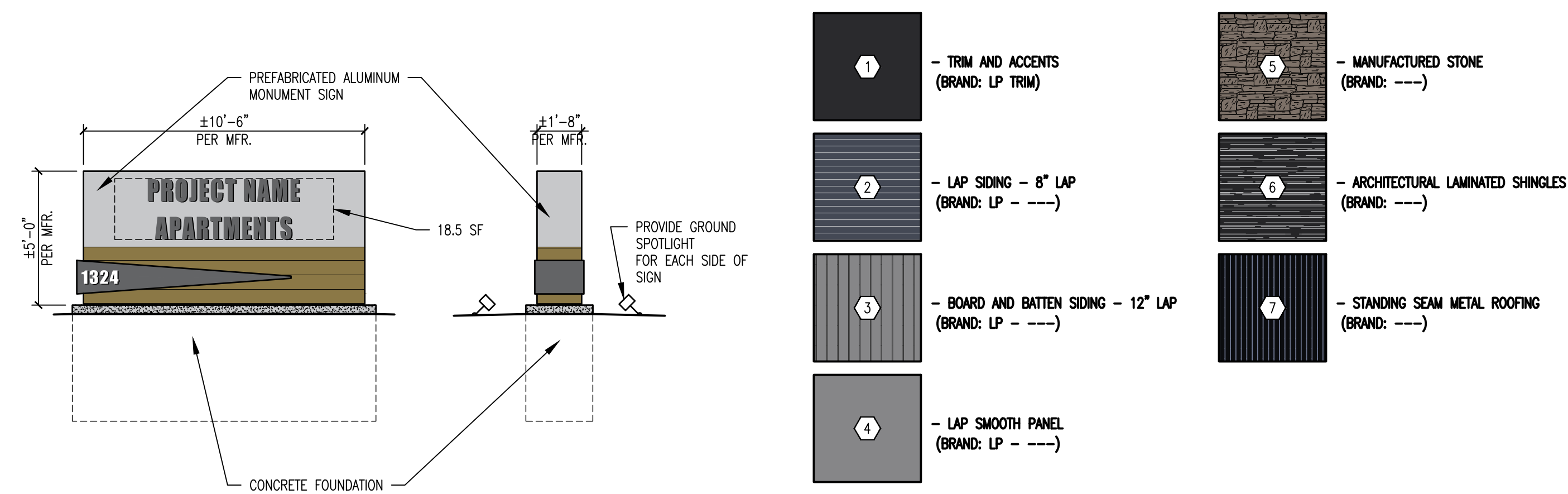
3 WEST ELEVATION
3/32" = 1'-0"



2 EAST ELEVATION
3/32" = 1'-0"



4 NORTH ELEVATION
3/32" = 1'-0"



5 MONUMENT SIGN ELEVATIONS
1/4" = 1'-0"

MAHLER & ASSOCIATES
ARCHITECTURE

Greene H.D. Mahler, AIA
Principal
5150 Marson Drive
Suite 101
Sauk Rapids, MN 56379
TEL: (320) 257-2724
EMAIL: gmahler@mahlerarchitecture.com

PRELIMINARY
NOT FOR
CONSTRUCTION

No.	Revision/Issue	Date

DAYTON ASSISTED LIVING
DAYTON, MN
C/O BJ BASS
CHAD CAZA
763-691-0444

Project: 2528	Date: 03/18/2026	Scale: AS INDICATED
Sheet: EXTERIOR ELEVATIONS		
A-201		

From: [Ashley Winters](#)
To: [Jon Sevald](#)
Subject: Re: Planning commission meeting
Date: Thursday, May 7, 2026 4:43:59 PM

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Thank you for this-

I'm reaching out about the Dayton Assisted Living project that's coming up on the agenda. I actually want to say upfront that I'm for having this kind of facility in town. We could use a facility like this and it is encouraging to see the prospects- I know I would want a loved one close to me.

My concern is really just making sure we do our due diligence before anything gets approved. Where I work at the we vet new clients and employees as a normal part of the process to make sure they fit our standards. I hope the city does the same with new businesses coming in.

I've been looking at some news articles and public records, and it seems like the person behind Dayton Assisted Living might be the same owner as Favored Hospice. I know Favored Hospice is currently dealing with allegations of fraud and misuse. (Kstp) reported on it. Those are just allegations and nothing's been proven, but if the two are connected, I think that's something we should know about before moving forward. Again I know they are allegations but I would be doing a disservice for not bringing this up.

There were a couple other things brought up at the last hearing that I'm still concerned about too. One is the issue with first responders. A lot of facilities now have policies where staff won't help a resident if they fall, and they just call 911 instead. Our police and fire departments are already small, and I worry about them being pulled away for lift assists when they could be needed for actual emergencies. Access to the building was also brought up last time, and I hope this new hearing is to address that.

The other thing is the business registration. From what I can see, not all of the business entities are listed with the Secretary of State. That's usually a pretty basic thing to check. If something goes wrong down the road, that kind of detail doesn't look good for the city.

I'm not against this project. I just want to make sure we've asked the right questions so we don't run into problems later.

Thanks for your time and for everything you do for this city.

> On May 6, 2026, at 1:08 PM, Ashley Winters <aewinters01@gmail.com> wrote:

>

> Thanks for the quick response Jon.

>

>> On May 6, 2026, at 1:04 PM, Jon Sevald <jsevald@daytonmn.gov> wrote:

>>

>> Ashley,

>>

>> If you email met prior to 5:00 Thursday, May 7th, I'll print it out for the Planning Commission meeting.

>>

>> If after, then your email will be provided to the City Council when they review this item.

>>

>> Thanks,

>> Jon

>>

>> -----Original Message-----

>> From: Ashley Winters <aewinters01@gmail.com>

>> Sent: Wednesday, May 6, 2026 12:59 PM

>> To: Jon Sevald <jsevald@daytonmn.gov>

>> Subject: Planning commission meeting

>>

>> CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

>>

>>

>> Hello, I am writing in regards to a public hearing that is to happen tomorrow regarding the assisted living and memory care. I understand the shorter notice and unfortunately I don't know if I can make it yet- with that said it says all written and verbal testimony will be taken. Are we able to send in a letter in what we wanted to state? Or how does the written piece work?

>>

>> Thank you!

ITEM:

Approval of Ordinance 2026-09 Amending City Code 1001.35 regarding Accessory Buildings

APPLICANT/PRESENTERS:

Jon Sevald, Community Development Director

PREPARED BY:

Jon Sevald, Community Development Director

POLICY DECISION/ ACTION TO BE CONSIDERED

1. Recommend **Approval**.
2. Recommend **Denial**, with reasoning.
3. **Table** for more information (define what information is needed).

BACKGROUND/OVERVIEW:

The use of Shipping Containers as permanent Accessory Buildings is prohibited.¹ The City Code allows Shipping Containers as a “Temporary Structure” up to 9-months (e.g. use of a storage pod during home renovation).²

Used shipping containers can be purchased for \$1,500 - \$3,000, whereas a comparatively sized wood shed may cost \$10,000+. The use of shipping containers is an affordable alternative to building stick-framed garages and sheds. In 2024, the city approved two Interim Use Permits (IUP) for Event Centers, both of which used shipping containers as Accessory Buildings. Both the Planning Commission and City Council were supportive of their use and directed Staff to prepare a Zoning Code amendment.

The attached DRAFT Ordinance amendment permits the use of shipping containers as permanent Accessory Buildings on Agricultural and Residential zoned parcels 2 acres or larger.

A quick survey of area city’s regulations:

Andover ³	Permits shipping containers as a temporary use (30-days).
Corcoran ⁴	Requires an Interim Use Permit (IUP) for up to one year for use of a shipping container as a “temporary structure”.
Elk River	Permits shipping containers in any residential district. But, in districts that prohibit metal panel siding (panel greater than 8”), the container must be re-sided.

¹ [City Code 1001.35, Subd 2\(5\)\(a\)\(1\)](#) and (b)

² [City Code 1001.41, Subd 3](#) (Temporary Structure; Special Requirements).

³ Andover City Code 12-6-6 (Temporary Structures)

⁴ Corcoran City Code 1030.040 (Temporary Structures)

Ramsey⁵ Permits shipping containers as a temporary use (30-days), and as a construction field office.

Rogers Has permitted two shipping containers for use as commercial outdoor bars.

CRITICAL ISSUES:

Enforcement The city does not have staff capacity to enforce property maintenance codes to assure shipping containers are “*complementary in color, materials and design with those of the principal dwelling.*”⁶ Staff’s preference is if permitted, that they not be visible from streets or neighbor’s, which means larger properties.

60/120-DAY RULE (IF APPLICABLE):

	60-Days (date)	120-Days (date)
N/A		

RELATIONSHIP TO COUNCIL GOALS:

N/A

PLANNING COMMISSION RECOMMENDATION:

The Planning Commission held a Public Hearing at its January 8, 2026 meeting, and continued at its April 2nd and May 7th meetings. The Commission recommends Approval. The Commission considered many factors, eventually settling on Shipping Containers being treated similar to any other accessory building, but that they are not appropriate on smaller lots (less than two acres).

STAFF RECOMMENDATION:

Staff recommend shipping containers be permitted in Agricultural and Residential zoned parcels 5-acres or larger.

ATTACHMENT(S):

- Photos
- Draft Ordinance
- Parcel Map, 2+ acre & 5+ acre parcels
- Comments, John Smith, March 27, 2026
- Comments, Eric Seppelt, May 7, 2026

⁵ Ramsey City Code 106-423, 106-455(9), 106-523, 106-307

⁶ [City Code 1001.35, Subd 2\(5\)\(a\)\(1\)](#) (Accessory building design requirements)

PHOTOS



2-acre parcel on Jonquil Ln, R-2 zoning.



2-acre parcel on Norwood Ln, R-2 zoning.



2-acre parcel on Jonquil Ln, R-2 zoning.



4-acre parcel on Kingsview Ln, A-1 zoning.



1-acre parcel on West French Lake Rd, A-1 zoning.



7-acre parcel on East French Lake Rd, A-1 zoning.



113th Ave, I-1 zoning.



Fernbrook Ln, I-1 zoning.



Troy Ln, B-3 zoning.

ORDINANCE 2026-09
CITY OF DAYTON
COUNTIES OF HENNEPIN AND WRIGHT
STATE OF MINNESOTA

AN ORDINANCE AMENDING DAYTON CITY CODE 1001 REGARDING ACCESORY BUILDINGS AND STRUCTURES

SECTION 1. AMENDMENT. Dayton City Code Section 1001 is hereby amended by adding the following underlined language and deleting the following ~~striketrough~~ language, which reads as follows:

SECTION 2. EFFECTIVE DATE. This Ordinance shall be in full force and effect from and after its passage and publication as required by law.

1001.03 RULES AND DEFINITIONS.
 Subd 2 Definitions.

Shipping Container

A six-sided steel unit originally constructed as a general cargo container used for the transport of goods and materials. Shipping containers do not include similar structures such as railroad cars, recreational vehicles, bus bodies, semi-trailers, and similar prefabricated items.

1001.35 ACCESSORY BUILDINGS AND STRUCTURES
 Subd 2 General Regulations

(5) Accessory building design requirements:

- a. The following requirements are for residential districts under 1 acre in size and commercial and industrial zoned districts:
 1. The exterior materials of the proposed accessory building shall be complementary in color, materials and design (e.g. orientation of siding) with those of the principal dwelling. No accessory building shall be constructed of canvas, plastic, fabric or other similar nonrigid materials, nor shall the use of a metal storage container be permitted as a permanent accessory structure.
 2. Architectural metal siding can be used as a permitted material provided it is complementary in color to the principal structure. Any exposed screws or fasteners shall match the color of the siding. Roof material shall be asphalt shingles or standing seam metal roof material.
 3. The accessory building shall include design elements that match the principal structure. This shall include additional accenting through the use of a porch, complementary building trim, window/door trim,

dormer, wainscoting, or other elements that are complementary to the principal building.

- b. Requirements are for residential districts over 1 acre in size. No accessory building shall be constructed of canvas, plastic, fabric or other similar nonrigid materials; ~~nor shall the use of a metal storage container shall be permitted as a permanent accessory structure.~~ Shipping Containers are permitted permanent accessory structures on parcels two or more acres in size, in A-1, A-2, A-3 S-A, R-1, R-1A, R-2, R-E zoning districts.

Adopted by the City Council of the City of Dayton, this 26th Day of May, 2026.

Dennis Fisher, Mayor

ATTEST:

Amy Benting, City Clerk

Motion by _____. Second by _____.

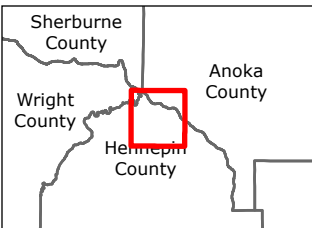
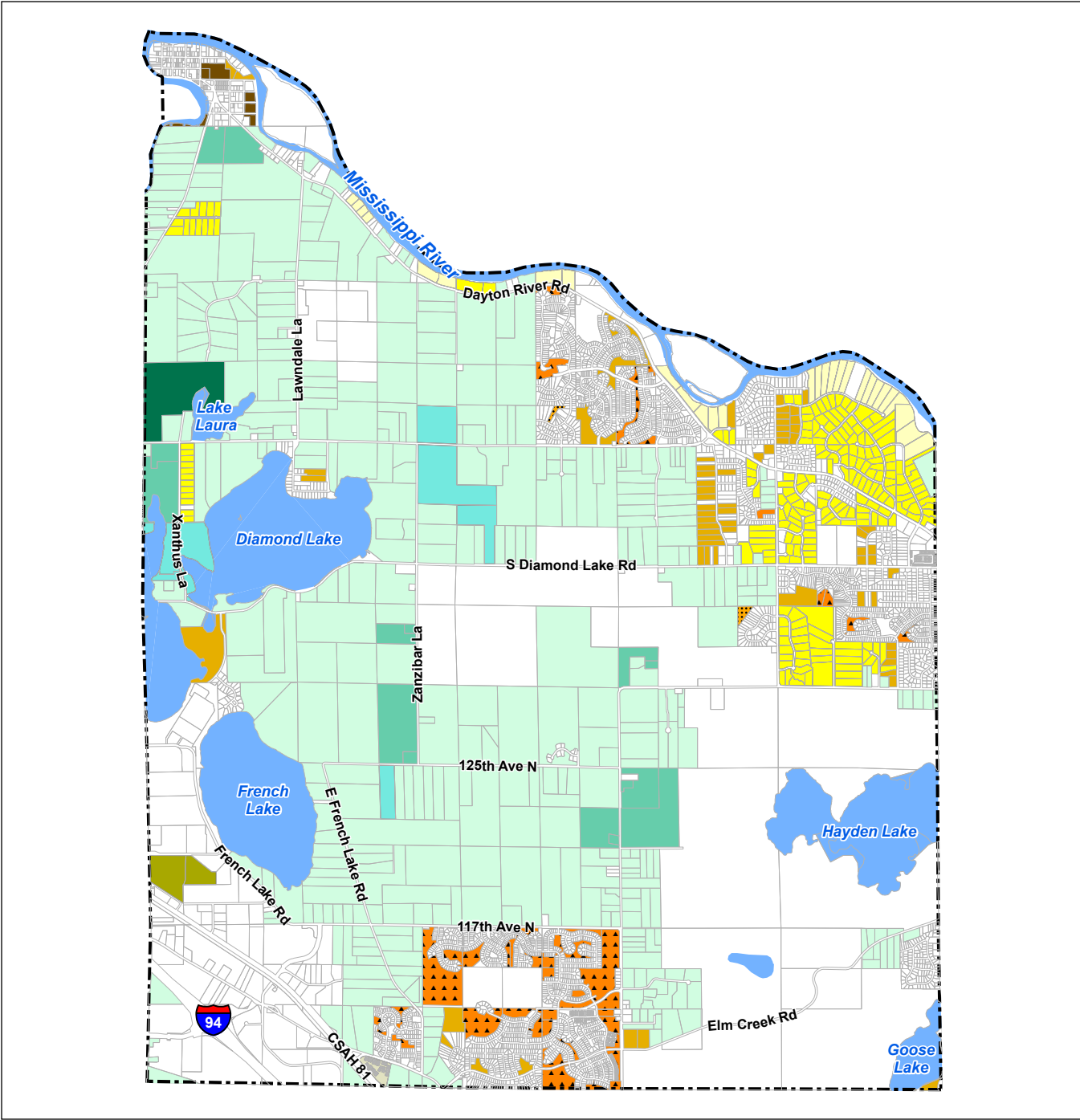
Ayes:

Nays:

Motion passed

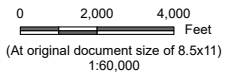
Published by THE PRESS on _____, 2026.

R:\GIS\client_reference_data\United States\MN\Dayton\proj\Planning_Final\Zoning_Landuse\Zoning_Landuse.aprx Revised: 2026-04-30 By: dmerickson



Notes
 1. Coordinate System: NAD 1983 HARN Adj MN Hennepin Feet
 2. Data Sources: Hennepin County, MN
 3. Background: Parcels

- A-1 Agricultural District (392)
- A-2 Agricultural District (15)
- A-3 Agricultural District (1)
- R-1 Single Family District (69)
- R-1A Single Family Residential (2)
- R-2 Single Family District (90,000 Sf, Unsewered) (217)
- R-3 Single Family and Attached Residential (41)
- R-E Single Family District (5 Ac, Unsewered) (30)
- R-M Medium Density Residential District (1)
- R-MH Mobile Home District (2)
- R-O Old Village Residential (6)
- S-A Special Agriculture District (7)



Project Location Dayton, Hennepin Co., MN *Prepared by DME on* 2026-04-30

Client/Project Dayton 193807625

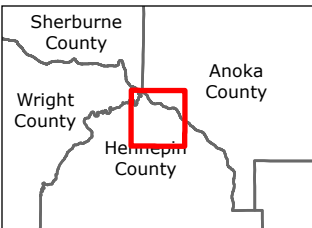
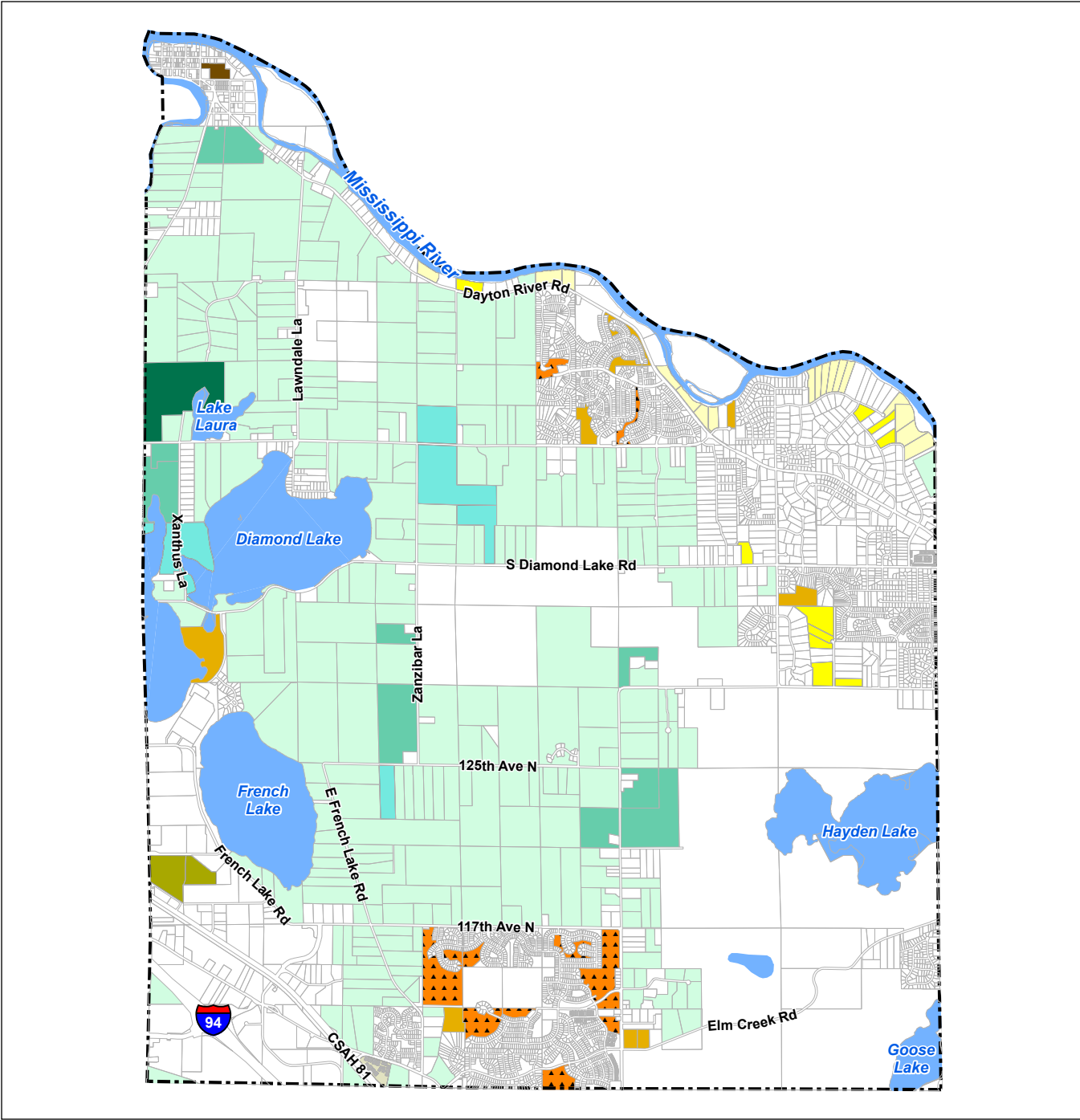
Ordinance Amendment that would allow Shipping Containers on residential and agricultural properties

Figure No. 2

Title
Parcels 2 Acre or Greater, Zoned Agricultural or Residential

Disclaimer: This document has been prepared based on information provided by others as cited in the Notes section. Stantec has not verified the accuracy and/or completeness of this information and shall not be responsible for any errors or omissions which may be incorporated herein as a result. Stantec assumes no responsibility for data supplied in electronic format, and the recipient accepts full responsibility for verifying the accuracy and completeness of the data.

R:\GIS\client_reference_data\United States\MN\Dayton\proj\Planning_Final\Zoning_Landuse\Zoning_Landuse.aprx Revised: 2026-04-30 By: dmerickson



Notes
 1. Coordinate System: NAD 1983 HARN Adj MN Hennepin Feet
 2. Data Sources: Hennepin County, MN
 3. Background: Parcels

- A-1 Agricultural District (278)
- A-2 Agricultural District (10)
- A-3 Agricultural District (1)
- R-1 Single Family District (9)
- R-2 Single Family District (90,000 Sf, Unsewered) (10)
- R-3 Single Family and Attached Residential (11)
- R-E Single Family District (5 Ac, Unsewered) (19)
- R-M Medium Density Residential District (1)
- R-MH Mobile Home District (2)
- R-O Old Village Residential (1)
- S-A Special Agriculture District (6)

0 2,000 4,000 Feet
 (At original document size of 8.5x11)
 1:60,000



Project Location Dayton, Hennepin Co., MN *Prepared by* DME on 2026-04-30

Client/Project Dayton 193807625
 Ordinance Amendment that would allow Shipping Containers on residential and agricultural properties

Figure No. 3

Title
Parcels 5 Acre or Greater, Zoned Agricultural or Residential

From: 612johnsmith@gmail.com
To: [Jon Sevald](#)
Subject: Shipping containers in Dayton
Date: Friday, March 27, 2026 7:04:03 AM

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

****REVISED DRAFT ORDINANCE**

Amending Dayton City Code to Allow Shipping Containers as Accessory Structures on Parcels Over One Acre**

Ordinance No. _____

AN ORDINANCE AMENDING TITLE 12 (ZONING REGULATIONS) OF THE DAYTON CITY CODE TO PERMIT SHIPPING CONTAINERS AS ACCESSORY STRUCTURES ON QUALIFYING RESIDENTIAL AND AGRICULTURAL PROPERTIES AND ESTABLISHING STANDARDS FOR SIZE, PLACEMENT, AND EXTERIOR APPEARANCE.

Section 1. Purpose.

The purpose of this ordinance is to establish clear standards for the use of shipping containers as accessory structures on larger residential and agricultural parcels within the City of Dayton, ensuring compatibility with surrounding properties, maintaining neighborhood character, and protecting public health, safety, and welfare.

Section 2. Applicability.

This ordinance applies to all residential zoning districts within the City of Dayton where accessory structures are permitted, including but not limited to R-A, R-E, and R-1 districts, as well as agricultural parcels legally operating as farms.

Section 3. Definitions.

- **“Shipping Container”** means a standardized, prefabricated metal container originally designed for the transport of goods by ship, rail, or truck, and repurposed for on-site storage or accessory use.
- **“Primary Dwelling”** means the principal residential structure on the parcel.
- **“Finished to Match”** means exterior treatment—including siding, paint color, trim, or cladding—designed to visually harmonize with the primary dwelling in material, color, and architectural character.
- **“Farm”** means a parcel used for bona fide agricultural production, including crop cultivation, livestock raising, or related agricultural operations, as recognized under Dayton City Code and Minnesota Statutes.

Section 4. Permitted Use.

A shipping container may be used as an accessory structure only when all of the following conditions are met:

1. The parcel is **greater than one (1) acre** in size.
2. The container is used solely for **personal residential storage, accessory residential purposes, or agricultural storage on a farm.**
3. The total exterior footprint of the container(s) shall not exceed **1% of the total lot square footage**, except as provided in subsection (6) for farm properties.
4. No more than **one (1)** shipping container is permitted per qualifying residential parcel unless approved through a Conditional Use Permit (CUP).
5. The container must comply with all applicable provisions of the Dayton City Code, including accessory structure standards, building codes, and stormwater regulations.
6. **Farm Exception:**
 - Parcels legally operating as farms may utilize **more than one (1)** shipping container for agricultural storage.
 - All containers on farm properties must be located **as far out of sight from public roads as reasonably possible**, using natural topography, existing structures, or vegetation to minimize visibility.
 - Screening or placement adjustments may be required by the City if visibility from a public road is deemed excessive.

Section 5. Placement Standards.

1. The container must be located **behind the front building line** of the primary dwelling, except where agricultural operations necessitate alternative placement on farm parcels.
2. Minimum setbacks:
 - **Side yard:** 25 feet
 - **Rear yard:** 25 feet or greater if required by the underlying zoning district.
3. The container shall not be placed within any drainage or utility easement, right-of-way, or wetland buffer.
4. The container must be placed on a stable, level surface such as gravel, concrete, or other approved base.

Section 6. Visibility, Screening, and Exterior Finish.

1. If the shipping container is **not visible** from adjacent properties or public rights-of-way, no additional exterior treatment is required.
2. If the container **is visible** from any neighboring property or public right-of-way, the owner must ensure that the container is:
 - **Finished to match** the primary dwelling in siding, color, and trim, **or**
 - Fully screened using fencing, landscaping, or architectural treatments approved by the City.
3. All required exterior finishing or screening must be completed within **90 days** of placement.
4. Farm parcels using multiple containers must still comply with visibility-minimization requirements under Section 4(6).

Section 7. Maintenance.

All shipping containers must be maintained in good condition, free of rust, peeling paint, structural damage, or other deterioration. The City may require repair, screening, or removal if the structure becomes unsafe or visually detrimental.

Section 8. Enforcement and Penalties.

Violations of this ordinance are subject to enforcement under the Dayton City Code, including:

- Written notice of violation,
- Administrative fines,
- Orders to repair, screen, or remove the container, and
- Additional penalties as authorized by state law and local ordinance.

Section 9. Effective Date.

This ordinance shall take effect upon passage and publication as required by law.

From: [Eric Seppelt](#)
To: [Jon Sevald](#)
Subject: Shipping container discussion
Date: Thursday, May 7, 2026 2:54:40 PM

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hi Jon,

I am not able to attend or login for the meeting tonight. I saw the shipping container conversation is being continued. I will not die on this hill since my current neighborhood living situation would prohibit them anyways. I really don't think the effort needs to be made to allow them beyond what they are currently allowed as, temporary storage solutions. The reason it's being discussed in the first place is because properties are in violation of the rule and to my knowledge without any sound justification as to why the rule should be changed to allow them. Drive past a rail yard or coastal shipping yard and that's what you'll see because that is what they are designed for. Because of their intended use the topic that has not come up and was actually brought up to me by my brother who works in supply chain management is the possible internal contamination that may exist either from the products transported in the container or treatments applied to the container to protect what's inside them from rodents, etc. I don't know for sure if resale of these containers regulates this and to allow that potential contamination to be added to Dayton properties seems a bit reckless. If there is a regulatory guideline then I would suggest that be added to any conditions for allowance within the city. Please share this with the commission for their deliberation.

Eric Seppelt

Sent from my iPhone

ITEM:

Response to Curbside Waste EAW

APPLICANT/PRESENTER:

Jon Sevald, Community Development Director

PREPARED BY:

Jon Sevald, Community Development Director
Courtney Bot, Stantec

POLICY DECISION / ACTION TO BE CONSIDERED:

1. Approve submittal of comments to MPCA regarding Curbside EAW
2. Deny submittal of comments to MPCA regarding Curbside EAW

BACKGROUND:

Curbside Waste is a refuse operator with offices and a Transfer Station at 18150 118th Ave in Dayton. Curbside operates other Transfer Stations in Becker and Savage. In 2022, the City Council approved a Conditional Use Permit (CUP) for a *Commercial and Residential Yard Waste Transfer Station*.¹

Curbside would like to accept additional waste types:

- yard waste (currently permitted)
- outsourced mixed municipal solid waste
- construction and demolition waste
- source separated organics
- single stream recycling

The existing Transfer Facility is permitted to process up to 42,000 cubic yards of yard waste per year. This would be expanded to 500,000 cubic yards per year of total wastes within the existing building. This requires a mandatory Environmental Assessment Worksheet (EAW), administered by the MPCA as the Responsible Government Unit (RGU). It will be the MPCA who determines if the EAW is sufficient or if an Environmental Impact Statement (EIS) will be required. The EAW public comment period is May 5, 2026 thru June 5, 2026. The EAW is a 1,067 page document. A summary (37 pages) is attached. The public can submit comments online at <https://mpca.commentinput.com/?id=B847Du935> or by mail; MPCA, Audrey Maass 520 Lafayette Rd N St Paul, MN 55155

Stantec has provided the attached DRAFT comment letter on behalf of the city.

CRITICAL ISSUES:

CUP City Code does not permit Transfer Stations for wastes other than Yard Waste. No matter the outcome of the EAW, Curbside will need to apply to

¹ Resolution 79-2022, *Granting Development Approval of an Industrially Guided Property Including Eave Application Requests for Master Family Investments*

Amend the City Code, and apply for a CUP for a Transfer Station for those waste types.

PLANNING COMMISSION RECOMMENDATION:

N/A

STAFF RECOMMENDATION:

Staff recommends Approval of the attached comment letter.

RELATIONSHIP TO COUNCIL GOALS:

N/A

BUDGET IMPACT:

N/A

ATTACHMENT(S):

DRAFT Comment Letter

EAW summary

May 26, 2026

Audrey Maass
Minnesota Pollution Control Agency
520 Lafayette Road North
Saint Paul, MN 55155

Re: Dayton Transfer Station Environmental Assessment Worksheet (EAW) Comment

Dear Ms. Maass,

The City of Dayton (City) appreciates the opportunity to review and comment on the EAW for the Dayton Transfer Station Project (Project), prepared by Curbside Waste Inc. (Curbside). Upon completing our review, we offer the following.

The Project is located within the City of Dayton. Curbside has been in operation at this location since 2025. Coordination with the City for the original Curbside project at this location as well as the currently proposed Project is outlined in EAW Item 6f. Typical operation hours are Monday through Friday 6:00 AM to 4:30 PM and on those Saturdays following the six major holidays when the holiday occurs on a weekday.

The City acknowledges that while the Project is located in Dayton, the Minnesota Pollution Control Agency (MPCA) is the Responsible Governmental Unit (RGU) per Minn. Rule 4410.4300 Subpart 17 Item C - "For construction or expansion of a mixed municipal solid waste transfer station for 300,000 or more cubic yards per year.

The City of Dayton 2040 Comprehensive Plan identifies the future intended land use for the Project area as Industrial including manufacturing, warehousing, automotive, trucking, office, and other related industrial uses. The City of Dayton Zoning Map identifies the Project area and surrounding areas to the southeast, south, west, and northwest of the Project as I-1 Light Industrial. Planned Unit Development (PUD) is identified for some of I-1 Light Industrial zoned areas to the south of the Project area. Areas to the north, northeast, east, and southeast of the Project Area are zoned A-1 Agricultural District. The Project is deemed compatible with surrounding zoning and land uses. The City understands that Curbside is not proposing a Project or use that will require a zoning change from the existing light industrial use.

The Dayton City Code allows *Commercial and Residential Yard Waste Transfer Station* as a Conditional Use in the I-1 Industrial district and does not permit other types of wastes Curbside proposes. Curbside will be required to apply to amend the Zoning Code to allow other types of wastes to be processed, and apply for a Conditional Use Permit for a Transfer Station to process those wastes.

The City understands the Project will transition from the current waste handling (yard waste) to a broader category of waste handling.

- The Project involves expanding the facility's use from fleet storage/dispatch for 56 Curbside trucks and limited yard waste handling (April to November) to transferring up to 500,000 cubic yards/year of outsourced mixed municipal solid waste, construction and demolition waste, yard waste, source separated organics and single stream recycling.
- With this transition, Curbside acknowledges their ability to receive and transport different waste types and their flexibility to continue collecting and responsibly managing waste materials and expand processing volume to help meet the waste removal needs of residents in the Metropolitan Area in an uncertain future (as described in detail in EAW Item 6d).

As it pertains to the City's review and consideration of the topics and categories outlined in the EAW form, the City provides the following:

- There would be no significant increase in traffic to or from the facility as the current fleet/collection trucks would begin and end their day at the facility as they do today and there would be no additional trips to/from the site by this fleet or by other companies as a result of this change in waste handling.
- Curbside would manage their operations around the seasonality of yard waste, maintaining consistent number of trucks and overall material handling throughout the year.
- The existing footprint would accommodate the Project and not require external construction or building expansion to process more material types or volume of materials. With the facility infrastructure built and operational, no further excavation or grading activities are planned for the Project. Existing impermeable concrete and asphalt surfaces would be used vehicles and equipment. Curbside is not planning for any soil disturbance.
- The current collection and delivery of yard waste, from a combination of Curbside trucks and other company trucks from the surrounding area, results in a temporary collection of yard waste on the facilities "tipping-floor" before it is transferred to semi-trailers and delivered to the Vonco compost site in Becker, MN at a rate of two to three transfer loads per day. With current operations, little to no material is left on tipping floor overnight.
- No changes are planned for domestic water use (connected to City of Dayton municipal water system).
- As it pertains to potential visual impacts and odors:
 - Curbside staff would receive environmental/compliance training regarding the proper disposal of waste.
 - Curbside states that full coverage doors would be installed to provide full overage doors on all six openings to the transfer station.
 - It appears there may have been a typo in the following portion of this sentence in Item 13b – "...and operational plans would aim to strategically deliver materials when wind speeds are high and could blow material outside the building, which will help keep material contained in the transfer station." It is the City's assumption that this was intended to read: "...and operational plans would aim to strategically deliver materials when wind speeds are not high and could blow material outside the building, which will help keep material contained in the transfer station."
 - Curbside staff would walk the perimeter of the property on a daily basis to ensure that all litter is collected and contained.
 - Curbside would install a second barbed wire arm that leans inward instead of outward to capture additional litter in addition to the current chain link fence surrounding the Project.
 - In EAW Item 6b, the last paragraph under "Potential future transfer station operations" it states: "Project approvals/permits to process the other materials (i.e., MSW, C&D, recycling, and organics), Curbside would load received waste material every afternoon (i.e., Monday to Friday or as described above) onto trailers that are enclosed with a tarp for transportation, and transfer material to the final disposal or processing site the following day. Occasionally, Curbside would store material overnight or over a weekend in the transfer station with the doors closed if the material cannot be loaded onto transfer semi-tractor trailers by end of day or by the end of the work week." Later in EAW Item 17c, it states: "Curbside will manage odors by moving material through the transfer station as quickly as possible. For example, most materials will be loaded and removed from the Project Area on a daily or next-day basis. Odor control measures include the six rolling doors, which Curbside can close as needed to limit the escape of odors from the Project to nearby sensitive receptors as well as good housekeeping procedures, limited onsite storage, and regular floor cleanings. There may be occasional temporary storage of waste materials overnight or over a weekend within the transfer station with the doors closed. Curbside will remove waste materials stored overnight or over a weekend on the following operations day."

The City requests that it be clarified if there may be storage of any type exterior to the transfer station either overnight or over weekend.

- There are a number of places in the EAW that speak to the measures Curbside would take to avoid visual impacts (e.g., barb wire system to capture material that may be blown around); however, this same information is not included in the Visual section of the EAW, which would have been recommended by the City.
- As it pertains to noise,
 - While it is described under the EAW Air item, the following is relevant to noise at the facility:
 - Waste truck drivers would be advised to turn off their engines if they are unloading accepted solid waste onto the tipping floor from a trailer, noting that some vehicles such as packer trucks require the engine remain running to dump the material on the tipping floor. Semitrucks engines would only run when the semi-trucks are entering or leaving the transfer station, they would not idle while being loaded with waste materials to be transferred to the final destination for the solid waste.
 - **With the need for truck staging (line to await unload at tipping floor), the City would request Curbside confirm that trucks would not run while awaiting one of the four positions available at the tipping floor.**
 - Regarding the EAW Noise section:
 - The facility is at the eastern end of an established industrial park. There are four residential properties to the east across West French Lake Road. Nearby sensitive receptors include a mobile home park approximately 0.3 miles from the Facility to the west, one residence approximately 150 feet to the northeast, and four residences ranging from approximately 0.1 to 0.2 miles to the southeast.
 - Curbside has not received any noise complaints from the residents since the Facility became operational.
 - During the local permitting process for the facility, conditions were established so noise from the facility did not adversely impact nearby residences. These conditions include limiting operating hours and requiring that maintenance activity be complete indoors.
 - On-site activities are described as not creating prolonged noise disturbances including statement that vehicles that come and go throughout the day do not create excessive noise.
 - Noise associated with maintenance activities, including any cutting, welding, or grinding, is completed inside the maintenance building.
 - The most common noise produced on-site that would be produced is the backup beeper sound from collection vehicles and the front loader or bobcat when in use.
 - Curbside would limit all activities associated with the Project to daytime hours (not overnight) and would be in conformance with state and local noise standards. **The City would request Curbside confirm days and hours of operation.**
 - There will not be any construction-related noise as the Project is already built to handle the requested capacity of waste materials. Curbside is committed to compliance with the noise standards set forth in Minn. Rules, Chapter 7030.0040.
- It is unclear whether EAW Item 21 Cumulative potential effects has been addressed. While the EAW form states that “Preparers can leave this item blank if cumulative potential effects are addressed under the applicable EAW Items”, the City found no mention of cumulative potential effects in the EAW.
- **In EAW Item 6b (Fire suppression and other site-specific requirements), the City requests Curbside work with the Dayton Fire Chief prior to expansion of operations (or prior to any applicable city permits being issued) to ensure the proper design and implementation of a suitable fire suppression system that would provide the additional fire suppression support for the facility including consideration of the Project.**

The City of Dayton welcomes Curbside and the Minnesota Pollution Control Agencies follow-up on any of the questions or comments we have provided in bold/italics. If there are any changes from the statements from the EAW that we have summarized here, the City would request that these be brought to our attention during INSERT MILESTONE.

Respectfully,

Jon Sevald, AICP
Community Development Director
jsevald@daytonmn.gov

December 2022 version

Environmental Assessment Worksheet

This most recent Environmental Assessment Worksheet (EAW) form and guidance documents are available at the Environmental Quality Board's website at: <https://www.eqb.state.mn.us/>. The EAW form provides information about a project that may have the potential for significant environmental effects. Guidance documents provide additional detail and links to resources for completing the EAW form.

Cumulative potential effects can either be addressed under each applicable EAW Item or can be addressed collectively under EAW Item 21.

Note to reviewers: Comments must be submitted to the RGU during the 30-day comment period following notice of the EAW in the *EQB Monitor*. Comments should address the accuracy and completeness of information, potential impacts that warrant further investigation and the need for an EIS.

1. Project title: Dayton Transfer Station

2. Proposer: Curbside Waste Inc.

Contact person: Matt Herman
Title: Chief Operating Officer
Address: 18150 118th Avenue North
City, State, ZIP: Dayton, MN, 55369
Phone: 763-533-2500
Fax:
Email: matt@curbsidewaste.com

3. RGU: Minnesota Pollution Control Agency

Contact person: Audrey Maass
Title: Project Manager
Address: 525 S Lake Ave # 400A
City, State, ZIP: Duluth, MN 55802
Phone: 218-302-6686
Fax:
Email: audrey.maass@state.mn.us

4. Reason for EAW preparation: (check one)

Required:

- EIS Scoping
 Mandatory EAW

Discretionary:

- Citizen petition
 RGU discretion
 Proposer initiated

If EAW or EIS is mandatory give EQB rule category subpart number(s) and name(s):

The Project expansion would exceed the 300,000 cubic yards (CY)/per year or more threshold for a mixed municipal solid waste station per Minn. Rule 4410.4300, Subpart 17, Item C, which states:

"For construction or expansion of a mixed municipal solid waste transfer station for 300,000 or more cubic yards per year, the PCA is the RGU.

5. Project location:

- County: Hennepin
- City/Township: City of Dayton
- PLS Location (¼, ¼, Section, Township, Range): SE ¼, SW ¼, Section 30, Township 120, Range 22
- Watershed (81 major watershed scale): Mississippi River
- GPS Coordinates: 45.169258, -93.509114
- Tax Parcel Number: 3012022430006

At a minimum attach each of the following to the EAW:

- County map showing the general location of the project;
- U.S. Geological Survey 7.5 minute, 1:24,000 scale map indicating project boundaries (photocopy acceptable); and
- Site plans showing all significant project and natural features. Pre-construction site plan and post-construction site plan.
- List of data sources, models, and other resources (from the Item-by-Item Guidance: *Climate Adaptation and Resilience* or other) used for information about current Minnesota climate trends and how climate change is anticipated to affect the general location of the project during the life of the project (as detailed below in item 7. Climate Adaptation and Resilience).

EXHIBITS

1. Average annual temperatures for Hennepin County from 1980 to 2025, January to December.
2. Average annual maximum temperatures for Hennepin County from 1980 to 2025, January to December.
3. Average minimum temperatures for Hennepin County from 1980 to 2025, January to December.
4. Average annual precipitation for Hennepin County from 1980 to 2025, January to December.
5. Average annual PDSI for Hennepin County from 1980 to 2025, January.
6. Average projected annual temperatures for Hennepin County.
7. Annual projected maximum temperatures for Hennepin County.
8. Annual projected minimum temperatures for Hennepin County.
9. Annual projected precipitation for Hennepin County.
10. Projected increase in number of days per year that are expected to exceed 100°F for Hennepin County.

TABLES

1. Daily vehicle operations for the current facility (based on current fleet size).
2. Project magnitude of the facility.
3. Summary of climate variables impact on the project area.
4. Interaction of proposed activities with each climate trend and projection listed in 7a.
5. Cover types.
6. Green infrastructure.
7. Tree canopy.
8. Permits and approvals.
9. Soils in the project area.
10. Public waters inventory basins and watercourses within one mile of the project area.
11. Surrounding wetland features.
12. Water wells within one-quarter mile of the project area.
13. Minnesota Pollution Control Agency What's in My Neighborhood records within one-quarter mile of the project area.
14. Emission categories for carbon footprint.
15. Average maximum number of daily trips.

16. Greenhouse Gas Emissions Summary (CO₂e in short tons per year).
17. Noise Standards.
18. Potential daily vehicle operations for the project (at current fleet size).

APPENDICES

- A. Figures
 1. Project location (USGS Topography)
 2. Project location (Aerial)
 3. Existing land cover
 4. Parks and trails
 5. Farmland classification
 6. Zoning
 7. Soils
 8. Surficial hydrological resources
 9. MN well index
 10. MPCA potentially contaminated sites
- B. Conditional use permit and developers agreement
- C. Wetland delineation report
- D. Phase I Environmental Site Assessment Report
- E. Minnesota Department of Natural Resources Natural Heritage report
- F. Letter response from the Minnesota State Historic Preservation office and outreach attempts to the Minnesota Indian Affairs Council and the Minnesota office of the State Archaeologist
- G. GHG emissions calcs Dayton TS EAW

6. Project description:

- a. Provide the brief project summary to be published in the *EQB Monitor*, (approximately 50 words).

Curbside Waste Inc. intends to transfer up to 500,000 cubic yards/year of outsourced mixed municipal solid waste, construction and demolition waste, yard waste, source separated organics, and single stream recycling at the existing transfer station (Facility) in the City of Dayton at 18150 118th Avenue North. No processing or permanent storage of waste would occur at the transfer station.

- b. Give a complete description of the proposed project and related new construction, including infrastructure needs. If the project is an expansion include a description of the existing Facility. Emphasize: 1) construction, operation methods and features that will cause physical manipulation of the environment or will produce wastes, 2) modifications to existing equipment or industrial processes, 3) significant demolition, removal or remodeling of existing structures, and 4) timing and duration of construction activities.

Current transfer station operations summary and design capacity

Curbside Waste Inc. (Curbside) constructed the Curbside Waste headquarters, office, call center, maintenance shop, and transfer station at 18150 118th Avenue North in the City of Dayton (Project). The Project began construction in 2024 and was operational in April 2025. Project infrastructure encompasses 10.2 acres of the 14.1-acre (Project Area). The remaining 3.9 acres include a 0.5-acre stormwater pond and 3.4-acre wetland.

Curbside is currently managing yard waste from a fleet of yard waste collection trucks and trucks from other companies in the surrounding area. Collection trucks dump yard waste on the tipping-floor that is loaded into semi-trailers for final transportation to a composting facility at a rate of two or three transfer loads per day to the Vonco compost site in Becker, MN. Curbside fills trailers each afternoon and transports them to the compost Facility the next morning. Little, if any, material remains on the tipping floor overnight. The Project’s operating hours are Monday through Friday from 6:00 AM to 4:30 PM. The Project operates on Saturdays following the six major holidays when they occur on a weekday. Curbside may add operating hours on Saturday in the future.

The current transfer station design can accommodate approximately 300 to 400 tons per day of material. Note that the weight of materials varies based on their density. The existing building design can manage yard waste from 30 to 40 collection vehicles per day. Current operations support the use of approximately 10 waste trucks hauling yard waste during the yard waste season (i.e., April to November), which is 25 to 33 percent of the existing potential workload capacity of the transfer station.

Current project vehicle operations summary

Curbside manages a fleet of up to 56 collection vehicles from the Facility. On a typical day, site activities include trucks collecting along commercial and residential trash and recycling routes, and residential organics and residential yard waste routes, as well as roll-off trucks collecting construction and demolition (C&D) debris. All materials other than yard waste are currently delivered to other facilities (including other transfer facilities) and trucks return to the Facility at the end of the day empty most often (with the exception of yard waste). See **Table 1** for the number of trucks that collect and transport each material type to the Project daily.

Table 1. Daily vehicle operations for the current facility (based on current fleet size)

Vehicle by route type	Vehicles per day
Commercial (Front Load)	6 to 8
Residential Trash	26 to 27
Residential Recycling	3 to 5
Residential Yard Waste	9 to 11 (seasonal April to November)
Residential Organics	1 to 2
Roll-Off Construction and Demolition	2 to 3
Semi-Tractor Trailer	6 to 8
Private Vehicles (Staff and Customers)	50 to 80
Total	103 to 144

There would be no significant increase in traffic to or from the Project Area if the Project were permitted to accept non-yard waste materials, as the collection trucks currently start and end their daily activities in the Project Area. Therefore, the collection trucks carrying non-yard waste materials could drop their final load of the day at the Project.

Potential future transfer station operations

The Project would support an increase in the amount of C&D debris, municipal solid waste (MSW), recycling, and source separated organics (food waste), in addition to yard waste currently transferred at the Project, resulting in up to 500,000 cubic yards/year of waste material being transferred through the Facility.

To accommodate the increased transfer of different material types, Curbside would divide the storage areas of the existing transfer station building footprint into separate bunkers for each material type. The existing transfer station building can accommodate the extra materials and volume. The Project would not need external construction or building expansion to process more material types or the volume of materials.

The materials received at the Project would change seasonally. For example, the yard waste season ends in November, as such Curbside would increase delivery of MSW, recycling, or C&D to the transfer station to capitalize on capacity during the winter months. Additionally, Curbside proposes to add the possibility of transferring small amounts of organics (food waste). Curbside would place the organics in a designated roll-off container or dumpster inside the transfer building. The total number of collection trucks and total amount of material transfer would not change.

Following Project approvals/permits to process the other materials (i.e., MSW, C&D, recycling, and organics), Curbside would load received waste material every afternoon (i.e., Monday to Friday or as described above) onto trailers that are enclosed with a tarp for transportation, and transfer material to the final disposal or processing site the following day. Occasionally, Curbside would store material overnight or over a weekend in the transfer station with the doors closed if the material cannot be loaded onto transfer semi-tractor trailers by end of day or by the end of the work week.

Odor and litter control

Curbside will manage odors and litter by moving material through the transfer station as quickly as possible. Curbside will install full coverage doors in all six openings to the transfer station, which will reduce odor and litter concerns. The Project Area is fenced with chain link fencing equipped with an outward facing barbed wire arm on top. The chain link fence and barbed wire will provide containment of litter from the Project. If needed, Curbside would add a second barbed wire arm directed inward instead of outward to capture additional litter. Staff would walk the perimeter of the site on a daily to weekly basis to make sure all litter is collected and contained.

Fire suppression and other site-specific requirements

The transfer station is plumbed for a water-based fire protection system, which Curbside has not designed or installed at this time. To support the Project, Curbside will work with the Dayton Fire Chief and other experts to design and implement a suitable fire suppression system that could provide additional fire suppression support or replace a traditional water-based deluge system.

c. Project magnitude:

Project magnitude of the Facility is provided in **Table 2**.

Table 2. Project magnitude of the facility

Description	Number
Total Project Acreage	14.1
Linear project length	Approximately 1,250 feet
Number and type of residential units	N/A
Residential building area (in square feet)	N/A
Commercial building area (in square feet)	N/A
Industrial building area (in square feet)	Transfer Station: 16,200 Office/Maintenance Building: 15,000
Institutional building area (in square feet)	N/A
Other uses – specify (in square feet)	N/A
Structure height(s)	Transfer Station: 30 feet 2 inches Office Building: 18 feet Maintenance Building: 30 feet 2 inches

- d. Explain the project purpose; if the project will be carried out by a governmental unit, explain the need for the project and identify its beneficiaries.

Curbside is seeking the ability to receive and transfer additional material types at the Project to respond to a changing business climate and ever-changing solid waste and recycling system and regulations. The Metropolitan Area solid waste and recycling systems has faced many challenges and changes since 2022. Changes in the recycling commodity markets, commercial driver shortages, inflation, rising equipment costs and changing regulations have created pressure across the industry. Regulatory requirements to collect residential and commercial organics, future recycling changes from the recently passed Extended Producer Responsibility Legislation (i.e., Packaging Waste and Cost Reduction Act: Minn. Stat. § 115A.144-115A.1463), and the potential closure of the Hennepin County Energy Recovery Center (HERC) will leave residents of the Metropolitan Area facing a long period of uncertainty regarding waste collection. Having the ability to receive and transport different waste types will provide Curbside the flexibility to continue collecting and responsibly managing waste materials and expand processing volume to help meet the waste removal needs of residents in the Metropolitan Area in an uncertain future.

Curbside, a private company, will carry out the Project. The purpose of the transfer station is to provide the Metropolitan Area surrounding the Project with increased capacity to properly dispose of MSW, C&D waste, yard waste, single stream recyclables, and organics. Curbside's primary goal is to use the existing transfer station to its design capacity to help state and local governments meet their current and future sustainability goals regarding waste disposal.

- e. Are future stages of this development including development on any other property planned or likely to happen? Yes No

If yes, briefly describe future stages, relationship to present project, timeline and plans for environmental review.

- f. Is this project a subsequent stage of an earlier project? Yes No

If yes, briefly describe the past development, timeline and any past environmental review.

Curbside presented a concept plan (application) to the City of Dayton in 2020 and again in late 2021 for the construction of the Project. The Dayton planning commission and city council provided favorable and constructive comments to the concept plan, and the application was amended to reflect the comments provided by the Dayton planning commission and city council.

In November 2022, Curbside received approval from the City of Dayton for a Conditional Use Permit (CUP)/Interim Use Permit (IUP) for a residential/commercial yard waste transfer station and a Developer's Agreement with the City of Dayton in March 2023 (Appendix B). Curbside began construction in 2024, and the transfer station became operational in April 2025. In April 2025, Curbside applied to the Minnesota Pollution Control Agency (MPCA) to accept construction and demolition waste, municipal solid waste, and single-stream recycling, which triggered development of this EAW, an Ordinance Amendment, and a Conditional Use Permit.

7. Climate adaptation and resilience:

- a. Describe the climate trends in the general location of the project (see guidance: *Climate Adaptation and Resilience*) and how climate change is anticipated to affect that location during the life of the project.

Based on historical climate data from the Minnesota Department of Natural Resources (MNDNR) Minnesota Climate Trends Resource for Hennepin County from 1980 – 2025,¹ there were upward trends for five climatic variables including average temperatures, maximum temperatures, minimum temperatures, and average annual precipitation, and Palmer Drought Severity Index (PDSI). The PDSI monitors drought events and predicts drought severity, with greater values indicating drier conditions. Minnesota may experience changes in the following climatic variables because of climate change (**Exhibits 1 through 5**):

- Average temperatures have increased by 0.49°F per decade;
- Maximum temperatures have increased by 0.46°F per decade;
- Minimum temperatures have increased by 0.51°F per decade;
- Precipitation has decreased by 0.01 inches per decade; and
- PDSI has decreased by 0.1 index variables per decade for the month of January.

Exhibit 1. Average annual temperatures for Hennepin County from 1980 to 2025, January to December

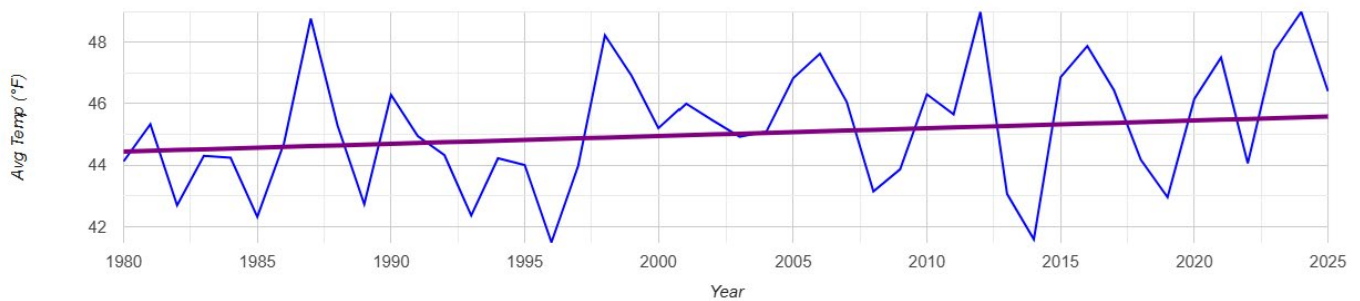


Exhibit 2. Average annual maximum temperatures for Hennepin County from 1980 to 2025, January to December

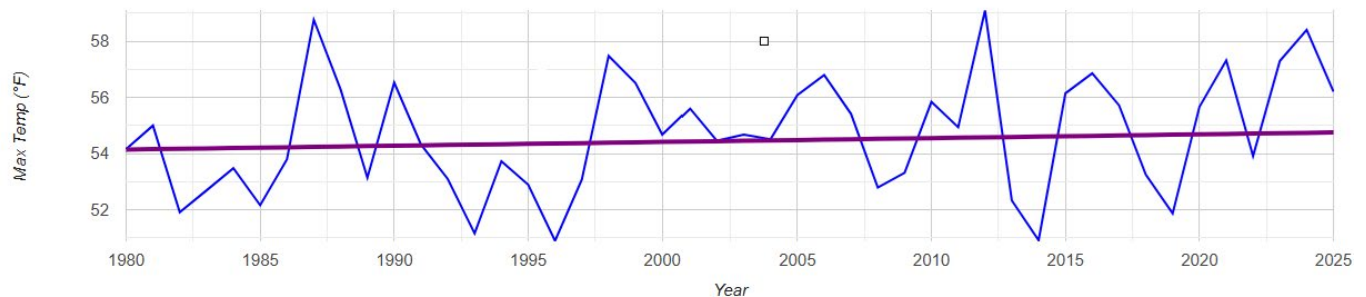
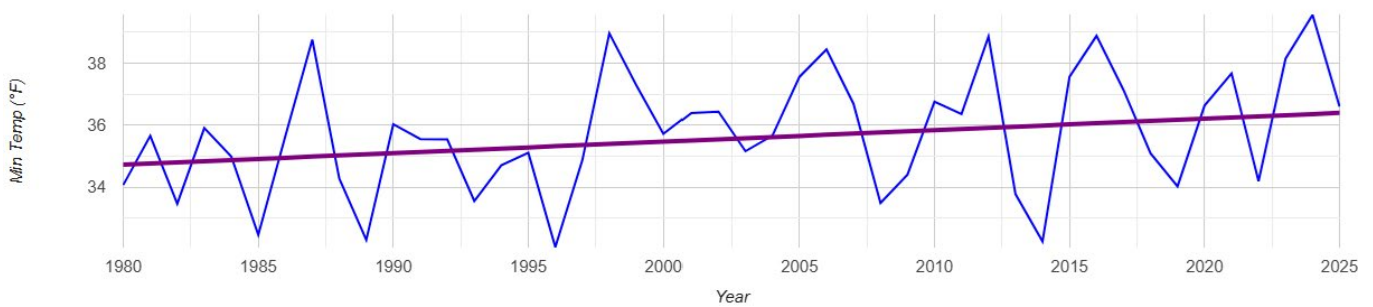


Exhibit 3. Average annual minimum temperatures for Hennepin County from 1980 to 2025, January to December



¹ Minnesota Department of Natural Resources. 2026. Minnesota Climate Trends. Available at: <https://arcgis.dnr.state.mn.us/ewr/climatetrends/>

Exhibit 4. Average annual precipitation for Hennepin County from 1980 to 2025, January to December

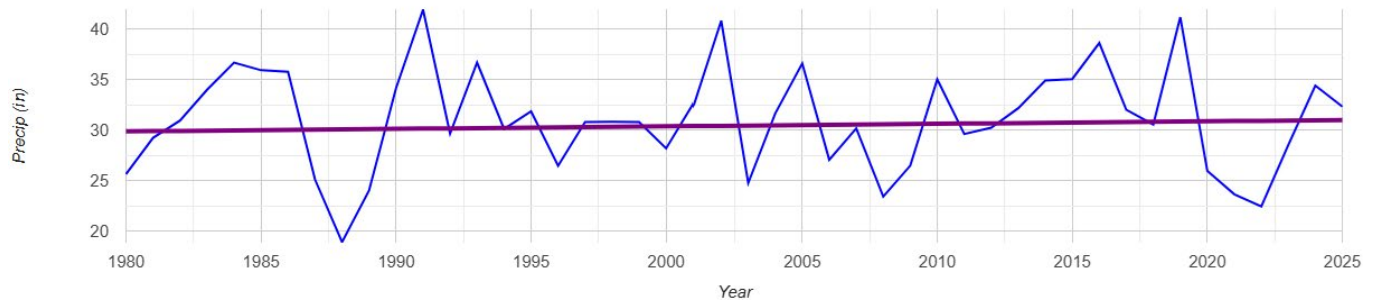
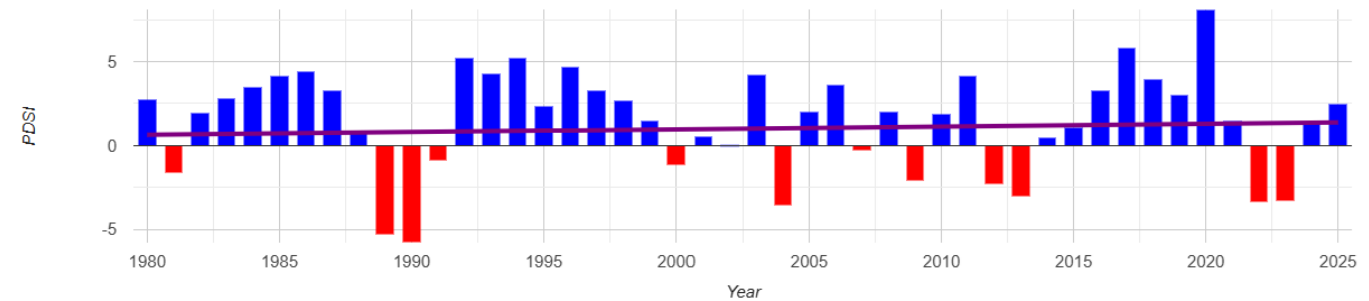


Exhibit 5. Average annual PDSI for Hennepin County from 1980 to 2025, January



The Minnesota Climate Explorer tool was used to summarize future annual climate projections for Hennepin County using historic data for four climatic variables including average temperature, maximum temperature, minimum temperature, and precipitation.² The model mean, depicted in blue, illustrates the average of all models included in the analysis. Best-case late-century scenarios were used for projection purposes, as it is likely that Project infrastructure will need to be updated by the turn of the century. Trends for projected climatic variables are presented in **Exhibits 6** through **9** and model means are summarized as follows:

- Annual Projected Average Temperatures: Under the best-case late-century scenario (RCP 4.5), average annual temperatures could reach 47.4°F to 55.0°F (mean 51.3°F) by 2099;
- Annual Projected Maximum Temperatures: Under the best-case late-century scenario (RCP 4.5) annual maximum temperatures could reach 53.9°F to 62.0°F (mean 57.9°F) by 2099;
- Annual Projected Minimum Temperatures: Under the best-case late-century scenario (RCP 4.5) annual minimum temperatures could reach 41.2°F to 48.5°F (mean 44.9°F) by 2099; and
- Annual Projected Precipitation: Under the best-case late-century scenario (RCP 4.5) annual precipitation could reach 17.0 to 70.8 inches (mean 32.9 inches) by 2099.

² Minnesota Department of Natural Resources. 2025. Minnesota Climate Explorer. Available at: <https://climate-explorer.dnr.state.mn.us/main/historical>

Exhibit 6. Average projected annual temperatures for Hennepin County.

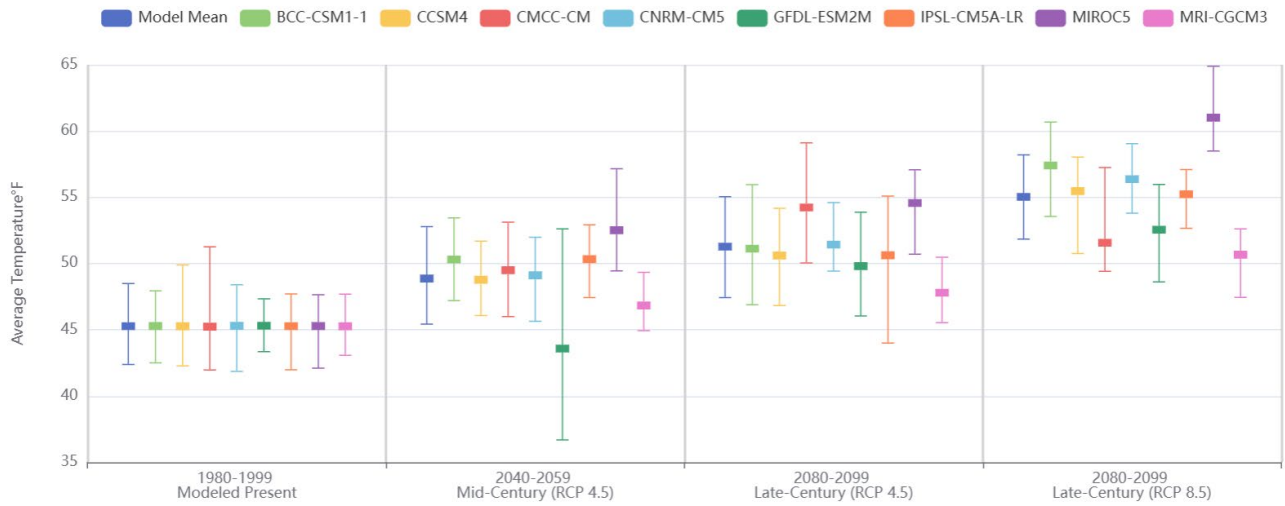


Exhibit 7. Annual projected maximum temperatures for Hennepin County.

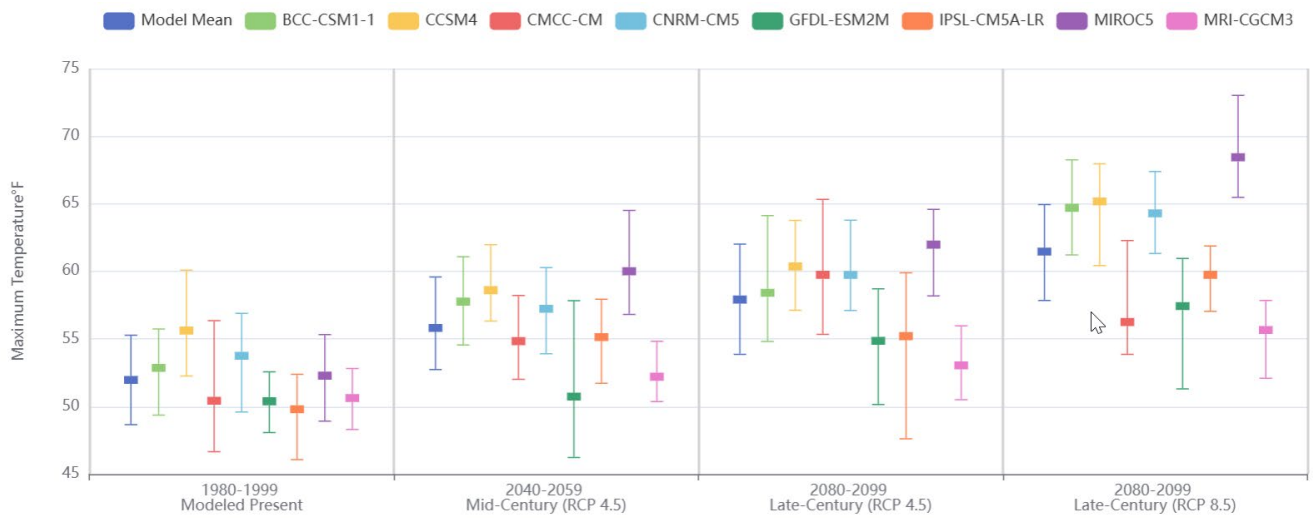


Exhibit 8. Annual projected minimum temperatures for Hennepin County.

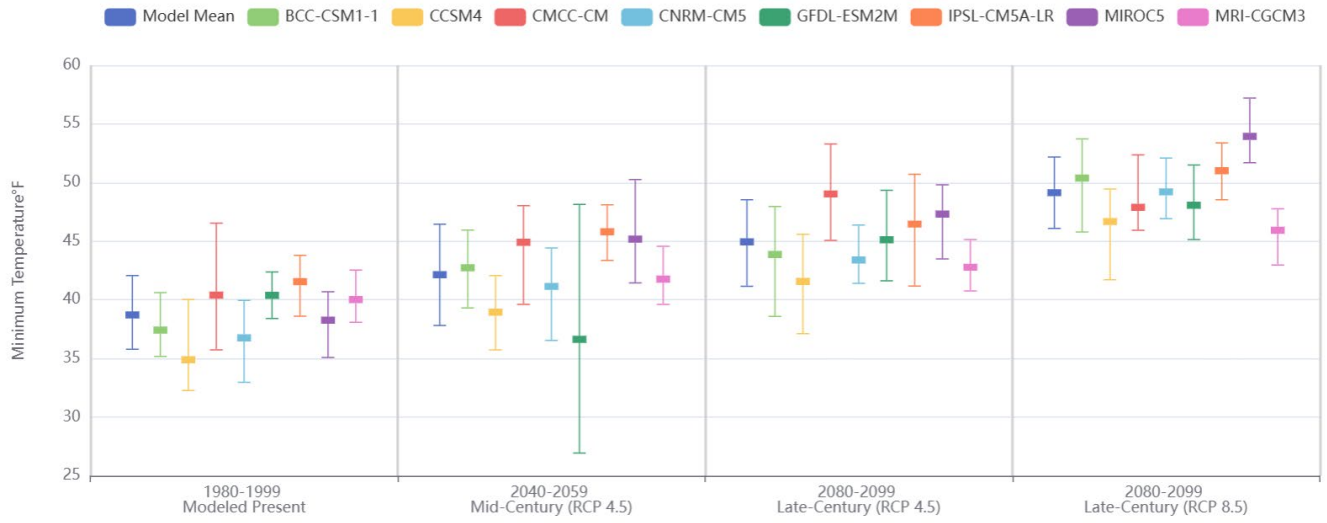
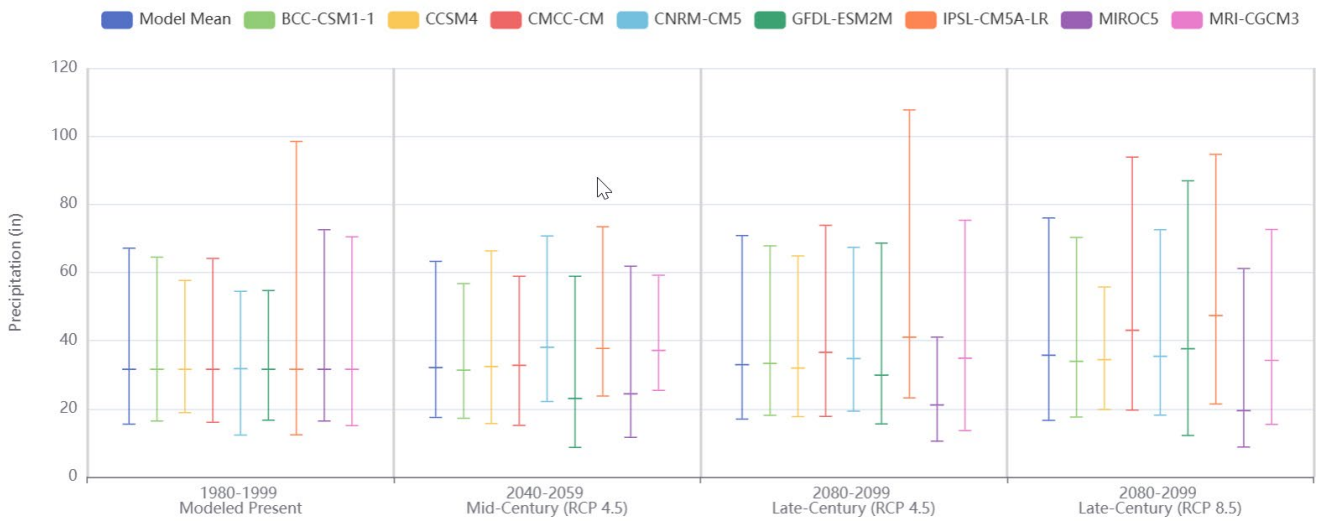
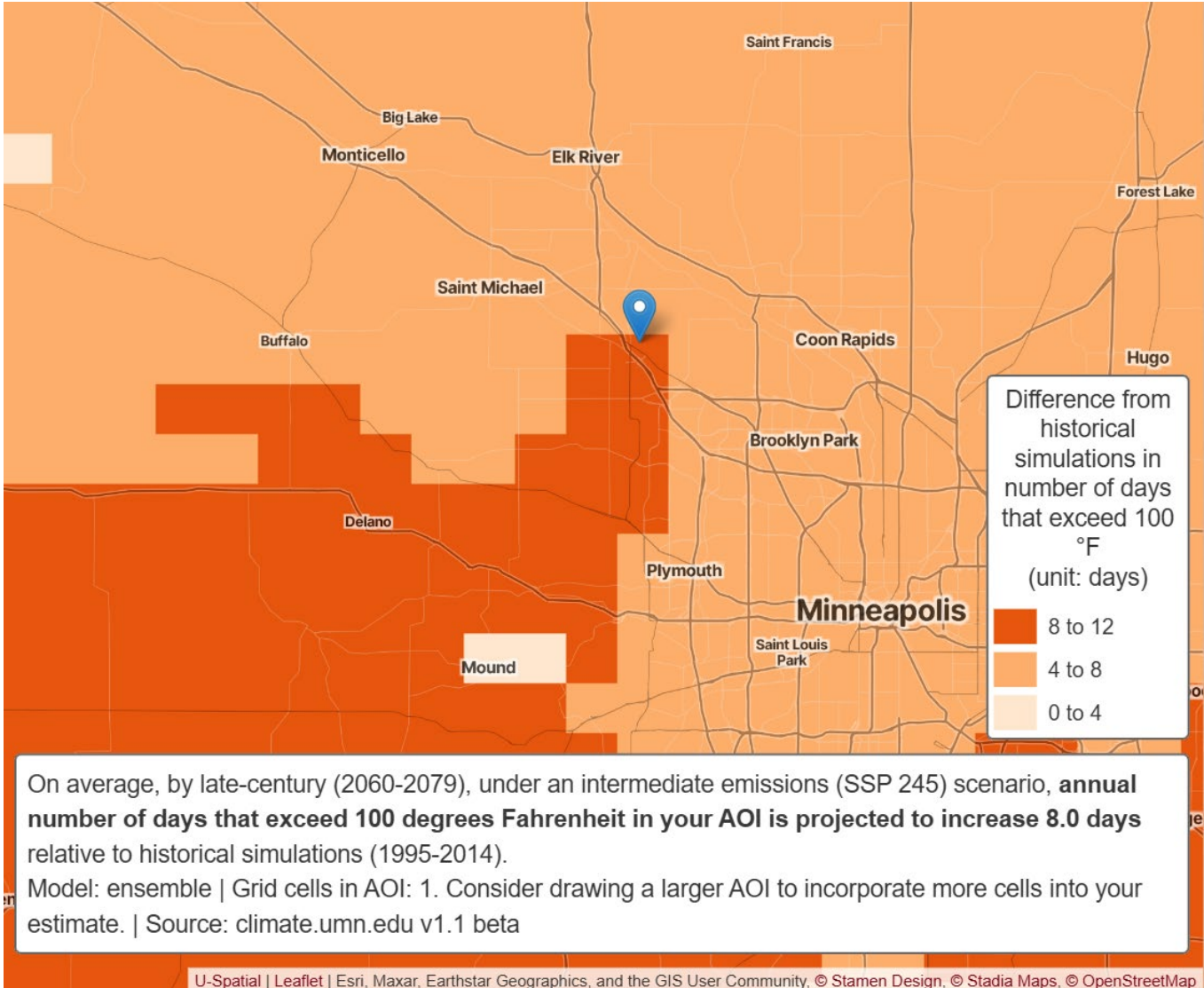


Exhibit 9. Annual projected precipitation for Hennepin County.



Based on the University of Minnesota’s Minnesota Climate Mapping and Analysis Tools, the number of days projected to exceed 100°F per year is expected to increase by 8 days per year by late-century using an intermediate-emissions scenario (**Exhibit 10**).³

Exhibit 10. Projected increase in number of days per year that are expected to exceed 100°F for Hennepin County



Summaries of the impact of climate variables on the Project Area are provided in **Table 3**. Late-century scenarios were used for projection purposes in **Table 3**, as it is likely that Project infrastructure will need to be updated by the turn of the century.

³ University of Minnesota. No date. Minnesota Climate Mapping and Analysis Tools. Available at: https://app.climate.umn.edu/?output_type=numDif&scenario=ssp370_2060-2079&model=ensemble&variable=tmax-degF&time_frame=yearly&aoi=p%7EMN_outline%7E0#intro_pane

Table 3. Summary of climate variables impact on the project area

Climate variables	Trends and projections	Location impact
Average annual temperature increasing	<p>Average temperatures have increased by 0.49°F per decade from 1980 to 2025. Projections indicate an increase in average annual temperatures for</p> <ul style="list-style-type: none"> • Best-case late-century scenario: mean 51.3°F 	<p>Increased air conditioning demands would reduce fuel efficiency of staff and customer vehicles, and waste collection and transportation vehicles. Air conditioning-related energy usage for the office building during periods of warm weather would increase and may place stress on the electrical grid. Curbside would reduce furnace-related energy usage during periods of cool/cold weather.</p>
Average precipitation increasing; Heavier, more damaging rains (occurring on an hourly/daily scale)	<p>Precipitation has decreased by 0.01 inches per decade from 1980 to 2025. Projections indicate an increase in average annual precipitation for</p> <ul style="list-style-type: none"> • Best-case late-century scenario: mean 32.9 inches 	<p>Increased runoff from average rain increasing and heavier, more damaging rains would be directed from the Facility to the 0.5-acre stormwater pond in the Project Area. Curbside built the 0.5-acre stormwater pond to accommodate average and heavier rainfall events.</p>
Cold weather warming	<p>Minimum temperatures have increased by 0.51°F per decade from 1980 to 2025. Projections indicate an increase in annual minimum temperatures for</p> <ul style="list-style-type: none"> • Best-case late century scenario: mean 44.9°F 	<p>Cold weather warming may result in decreased snow cover which could reduce the need for snow removal at the Facility and reduce spring flooding from snowmelt during early spring months.</p>
Risk of heat waves increasing (exceeding 100°F)	<p>By 2060, the annual number of days above 100°F is projected to increase by 8 compared to the 1995 to 2014 timeframe.</p>	<p>Increased air conditioning demands would reduce fuel efficiency of staff and customer vehicles, and waste collection and transportation vehicles. Air conditioning-related energy usage for the office building during periods of warm weather would increase and may place stress on the electrical grid.</p>
Risk of drought increasing	<p>Palmer Drought Severity Index (PDSI) has decreased by 0.1 index variables per decade from 1980 to 2025. Lower PDSI values correlate with wetter conditions.</p>	<p>An reduced risk of drought may negatively impact plant growth and survival around the stormwater pond and wetland within the Project Area.</p>

- b. For each Resource Category in the table below: Describe how the project's proposed activities and how the project's design will interact with those climate trends. Describe proposed adaptations to address the project effects identified.

Climate and adaptation considerations related to the Project are summarized in **Table 4**. The Facility's existing infrastructure is built to the size and scale necessary to process the increase in waste material volume and type for the proposed Project. No new infrastructure will be built for the proposed Project, nor will any changes to existing land cover occur. As such, the interaction among climate trends and the proposed Project's increase in waste management volume and type is the sole Project component evaluated in **Table 4**.

Table 4: Interaction of proposed activities with each climate trend and projection listed in 7a.

Resource category	Climate variables	Project components	Potential environmental effects <ul style="list-style-type: none"> • Identify climate change risks & vulnerabilities. • Identify long-term impacts that climate conditions pose to proposed activities. 	Adaptation strategies (with applicable timeframe – construction to end of expected lifespan)
Project design	Average annual temperature increasing	Increase in volume and types of non-hazardous waste materials (i.e., yard waste, municipal solid waste, construction and demolition, recycling, and organics) transferred through the Project Area.	Environmental impact not foreseen with interaction between an increase in waste material volume or types and an increase in average annual temperature.	None proposed. Most waste materials will be moved within 48 hours (i.e., duration of a weekend) and the transfer station floor will be cleared every seven days per the Minnesota Administrative Rules. Therefore, waste materials will not be impacted by an increase in annual temperature.
	Average annual precipitation increasing	Increase in volume and types of non-hazardous waste materials (i.e., yard waste, municipal solid waste, construction and demolition, recycling, and organics) transferred through the Project Area.	Environmental impact not foreseen with interaction between an increase in waste material volume or types and an increase in average annual precipitation.	None proposed. Curbside will temporarily store waste materials in the transfer station and will be loaded into semi-trailers that will be securely covered during transportation to their destination. Therefore, increases to waste material volume and type not be impacted by an increase in annual precipitation.
	Cold weather warming	Increase in volume and types of non-hazardous waste materials (i.e., yard waste, municipal solid waste, construction and demolition, recycling, and organics) transferred through the Project Area.	Environmental impacts are not foreseen with interaction between an increase in waste material volume or types and cold weather warming.	None proposed. Most waste materials will be moved within 48 hours (i.e., duration of a weekend) and the transfer station floor will be cleared every seven days per the Minnesota Administrative Rules. Therefore, waste materials will not be impacted by cold weather warming.
	Heavier, more damaging rains	Increase in volume and types of non-hazardous waste materials (i.e., yard waste, municipal solid waste, construction and demolition, recycling, and organics) transferred through the Project Area.	Environmental impacts are not foreseen with interaction between an increase in waste material volume or types and heavier, more damaging rains.	None proposed. Waste materials will be temporarily stored in a building and will be loaded into semi-trailers that will be securely covered during transportation to their destination. The transfer station roof and trailer covers will protect all waste materials from rain contact. Therefore, an increase in waste material volume and type will not be impacted by heavier, more damaging rains.
	Risk of heat waves increasing	Increase in volume and types of non-hazardous waste materials (i.e., yard	Environmental impacts are not foreseen with interaction between an increase in waste material volume or types and risk of heat waves increasing.	None proposed. Most waste materials will be moved within 48 hours (i.e., duration of a weekend) and the transfer station floor will be cleared every seven days

Resource category	Climate variables	Project components	Potential environmental effects <ul style="list-style-type: none"> • Identify climate change risks & vulnerabilities. • Identify long-term impacts that climate conditions pose to proposed activities. 	Adaptation strategies (with applicable timeframe – construction to end of expected lifespan)
		waste, municipal solid waste, construction and demolition, recycling, and organics) transferred through the Project Area.		per the Minnesota Administrative Rules. Therefore, waste material will not be impacted by risk of heat waves increasing.
	Risk of drought increasing	Increase in volume and types of non-hazardous waste materials (i.e., yard waste, municipal solid waste, construction and demolition, recycling, and organics) transferred through the Project Area.	Environmental impacts are not foreseen with interaction between an increase in waste material volume or types and increased risk of drought.	None proposed. Most waste materials will be moved within 48 hours (i.e., duration of a weekend) and the transfer station floor will be cleared every seven days per the Minnesota Administrative Rules. Therefore, waste material will not be impacted by an increase in the risk of drought.
Land Use	Impacts are addressed in Item 10. (No land use change associated with the project.)			
Water Resources	Impacts are addressed in Item 12.			
Contamination/ Hazardous Materials/ Wastes	Impacts are addressed in Item 13.			
Fish, wildlife, plant communities, and sensitive ecological resources (rare features)	Impacts are addressed in Item 14.			

8. Cover types: Estimate the acreage of the site with each of the following cover types before and after development:

Most of the 14.1-acre Project Area is comprised of impervious surfaces (44 percent of the Project Area [6.1 acres]), followed by lawn/landscaping (29 percent [4.1 acres]), wetlands (24 percent [3.4 acres]), and a stormwater pond (3 percent [0.5 acres]) (Table 5) (Figure 3, Appendix A). The Project will not require or involve any changes to existing cover types as the existing infrastructure was designed to process and transfer the volume of waste material requested pending approvals/permits.

There will be no changes to existing green infrastructure due to the Project. The existing 0.5-acre stormwater retention pond will remain (Table 6). The Project will not involve any tree removal or tree planting. The 62 trees planted when the transfer station and supporting infrastructure was constructed will remain (Table 7).

Table 5. Cover Types

Cover types	Before(acres)	After (acres)
Wetlands and shallow lakes (<2 meters deep)	3.4	3.4
Deep lakes (>2 meters deep)	0	0
Wooded/forest	0	0
Rivers/streams	0	0
Brush/Grassland	0	0
Cropland	0	0
Livestock rangeland/pastureland	0	0
Lawn/landscaping	4.1	4.1
Green infrastructure TOTAL (from table below*)	0	0
Impervious surface	6.1	6.1
Stormwater Pond (wet sedimentation basin)	0.5	0.5
Other (describe)	0	0
TOTAL	14.1	14.1

Table 6. Green infrastructure

Green infrastructure	Before (acreage)	After (acreage)
Constructed infiltration systems (infiltration basins/infiltration trenches/ rainwater gardens/bioretenion areas without underdrains/swales with impermeable checkdams)	0.5	0.5
Constructed tree trenches and tree boxes	0	0
Constructed wetlands	0	0
Constructed green roofs	0	0
Constructed permeable pavements	0	0
Other (describe)	0	0
TOTAL	0.5	0.5

Table 7. Tree canopy

Trees	Percent	Number
Percent tree canopy removed or number of mature trees removed during development	0	0
Number of new trees planted	62	0

9. Permits and approvals required: List all known local, state and federal permits, approvals, certifications and financial assistance for the project. Include modifications of any existing permits, governmental review of plans and all direct and indirect forms of public financial assistance including bond guarantees, Tax Increment Financing and infrastructure. *All of these final decisions are prohibited until all appropriate environmental review has been completed. See Minnesota Rules, Chapter 4410.3100.*

A list of permits or approvals secured for the Facility and Project are provided in **Table 8**.

Table 8. Permits and approvals

Unit of Government	Type of application	Status
Minnesota Pollution Control Agency (MPCA) MPCA	National Pollutant Discharge Elimination System (NPDES) Permit	Granted on May 10, 2023. Permit Number: C00066674.
	Permit-by-Rule	Approved May 5, 2025 PBR001457
	Solid Waste Permit	To be applied for.
State Historic Preservation Office Review	Archaeological /Historic Review	Phase 1 Archeological Survey Final Report published on May 17, 2021.
City of Dayton	Development of an Industrially Guided Property including Eleven Application Requests for Master Family Investments	Approved and adopted on 11/22/2022. Resolution Number: 79-2022.
	Permit-By-Rule Notification Form	Acknowledged
	Wetland Conservation Act (Boundary Approval) Notice of Decision	Notice of City of Dayton approval of a WCA Application for Wetland Boundary and Type was sent on 9/15/2020.
	Granting Final Plat Approval of an Industrially Guided Property for Master Real Estate 2	Approved and adopted on 2/28/2023. Resolution Number 06-2023.
	Non-Residential New Construction	Approved on 11/6/2023. Permit Number: P2023-01034
	Non-Residential Plumbing Permit	Approved on 11/3/2023. Permit Number: P2023-01038
	Water Connection Permit	Approved on 6/10/2024. Permit Number: P2024-00500

Cumulative potential effects may be considered and addressed in response to individual EAW Item Nos.10-20, or the RGU can address all cumulative potential effects in response to EAW Item No.22. If addressing cumulative effect under individual items, make sure to include information requested in EAW Item No. 21.

10. Land use:

a. Describe:

- i. Existing land use of the site as well as areas adjacent to and near the site, including parks and open space, cemeteries, trails, prime or unique farmlands.

Curbside constructed a transfer station and associated infrastructure (10.2 acres), the remaining land use includes a 3.4-acre wetland and 0.5-acre stormwater pond. Accordingly, existing land use is predominately industrial.

Surrounding land use includes undeveloped areas (forested), industrial lots, commercial, and scattered homesteads. A residential mobile home park (Dayton Park) is approximately 0.3 miles to the west. There are no designated parks, open space, cemeteries, or trails within or immediately adjacent to the Project Area (**Figure 4, Appendix A**). Although there were/are areas of prime farmland, farmland of statewide importance, and prime farmland if drained within the Project Area, the Project will not require construction of new or modified infrastructure (**Figure 5, Appendix A**).

- ii. Plans. Describe planned land use as identified in comprehensive plan (if available) and any other applicable plan for land use, water, or resources management by a local, regional, state, or federal agency.

According to the City of Dayton 2040 Comprehensive Plan, future intended land use for the Project Area is industrial including manufacturing, warehousing, automotive, trucking, office, and other related industrial uses. No other planned land use, water use, or resource management were identified for the Project Area.

- iii. Zoning, including special districts or overlays such as shoreland, floodplain, wild and scenic rivers, critical area, agricultural preserves, etc.

The City of Dayton zoning map (updated January 24, 2025) identified the Project Area as I-1 Light Industrial District (**Figure 6, Appendix A**). The Project Area is not within any shoreland, floodplain, wild or scenic river, agricultural preserve, or other critical area.

- iv. If any critical facilities (i.e. facilities necessary for public health and safety, those storing hazardous materials, or those with housing occupants who may be insufficiently mobile) are proposed in floodplain areas and other areas identified as at risk for localized flooding, describe the risk potential considering changing precipitation and event intensity.

Not applicable.

- ### b. Discuss the project's compatibility with nearby land uses, zoning, and plans listed in Item 9a above, concentrating on implications for environmental effects.

The City of Dayton 2040 Master Plan zones the Project Area and surrounding areas to the southeast, south, west, and northwest of the Project as I-1 Light Industrial (**Figure 6, Appendix A**). Some of I-1 Light Industrial zoned areas to the south of the Project Area are also identified as being zoned for Planned Unit Development (PUD) (**Figure 6, Appendix A**). The areas north, northeast, east, and southeast of the Project Area are zoned A-1 Agricultural District (**Figure 6, Appendix A**). Therefore, the Project is compatible with surrounding zoning and land uses. The Project will not require a zoning change from the existing light industrial use.

- ### c. Identify measures incorporated into the proposed project to mitigate any potential incompatibility as discussed in Item 10b above and any risk potential.

Not applicable.

11. Geology, soils and topography/land forms:

- ### a. Geology - Describe the geology underlying the project area and identify and map any susceptible geologic features such as sinkholes, shallow limestone formations, unconfined/shallow aquifers,

or karst conditions. Discuss any limitations of these features for the project and any effects the project could have on these features. Identify any project designs or mitigation measures to address effects to geologic features.

The Project Area is underlain by Pleistocene aged Heiberg Member till, part of the New Ulm Formation, which consist of unsorted loam to clay loam with scattered pebbles and cobbles, and occasional boulders.⁴ The unconsolidated sediments are underlain by late Cambrian aged bedrock of the Tunnel City Group with most of the site underlain by the Lone Rock Formation, which consists of interbedded sandstone and siltstone and ranges in thickness from 85 to 145 feet.⁵ Additionally, the southwest portion of the Project Area is underlain by the Mazomanie Formation, which consists of cross-stratified quartzose sandstone with interbedded dolomitic sandstone up to 50 feet thick. There are no sinkholes, shallow limestone formations, unconfined/shallow aquifers, or karst formations within the Project Area. The Project will not require or involve any new construction or modifications and as such will not introduce any subsurface impacts that could influence geology. Therefore, no Project-related limitations or effects are expected, and Curbside proposes no mitigation measures.

- b. Soils and topography - Describe the soils on the site, giving NRCS (SCS) classifications and descriptions, including limitations of soils. Describe topography, any special site conditions relating to erosion potential, soil stability or other soils limitations, such as steep slopes, highly permeable soils. Provide estimated volume and acreage of soil excavation and/or grading. Discuss impacts from project activities (distinguish between construction and operational activities) related to soils and topography. Identify measures during and after project construction to address soil limitations including stabilization, soil corrections or other measures. Erosion/sedimentation control related to stormwater runoff should be addressed in response to Item 12.b.ii.

Topography within the Project Area is relatively flat, ranging from 920 to 930 feet above mean sea level.⁶ According to the National Resources Conservation Service (NRCS) Web Soil Survey, five soil types were mapped within the Project Area (**Table 9**) (**Figure 7, Appendix A**).

Table 9. Soils in the project area

Map unit symbol	Map unit name	Erosion hazard	Soil drainage classification	Hydric	Acres	Percent of project area
L22C2	Lester loam, 6 to 10 percent slopes, moderately eroded	Moderate	Well drained	No	5.5	39.0
L44A	Nessel loam, 1 to 3 percent slopes	Slight	Moderately well drained	No	3.3	23.4
L49A	Klossner soils, depressional, 0 to 1 percent slopes	Slight	Very poorly drained	Yes	2.5	17.7
L23A	Cordova loam, 0 to 2 percent slopes	Slight	Poorly drained	Yes	1.6	11.3
L45A	Dundas-Cordova complex, 0 to 3 percent slopes	Slight	Somewhat poorly drained	No	1.2	8.5
Total	--	--	--	--	14.1	99.9* Does not equal 100.0 due to rounding

⁴ Berthold, A. 2018. *Surficial Geology. County Atlas Series Atlas C-45, Part A. Hennepin County Plate 3.*

⁵ Retzler, A. 2018. *Bedrock Geology. County Atlas Series Atlas C-45, Part A. Hennepin County Plate 2.*

⁶ Minnesota Department of Natural Resources. 2025. MnTOPO. Available at: <https://mntopo.dnr.state.mn.us/>

Soils in the Project Area range from very poorly drained to well drained. NCRS erosion hazard ratings are classifications used to evaluate the potential for soil erosion within an area. The ratings are categorized as slight, moderate, severe, or very severe. The soils underlying the Project Area have slight to moderate erosion hazard ratings, indicating that high rates of erosion are unlikely under ordinary climatic conditions and with the implementation of erosion control best management practices.

As the Facility infrastructure is built and operational, no further excavation or grading activities are needed. All vehicles and equipment will use existing impermeable concrete and asphalt surfaces. Curbside does not anticipate any soil disturbance; therefore, soil integrity in the Project Area would be maintained.

- NOTE: For silica sand projects, the EAW must include a hydrogeologic investigation assessing the potential groundwater and surface water effects and geologic conditions that could create an increased risk of potentially significant effects on groundwater and surface water. Descriptions of water resources and potential effects from the project in EAW Item 12 must be consistent with the geology, soils and topography/land forms and potential effects described in EAW Item 11.

12. Water resources:

a. Describe surface water and groundwater features on or near the site in a.i. and a.ii. below.

- i. Surface water - lakes, streams, wetlands, intermittent channels, and county/judicial ditches. Include any special designations such as public waters, shoreland classification and floodway/floodplain, trout stream/lake, wildlife lakes, migratory waterfowl feeding/resting lake, and outstanding resource value water. Include the presence of aquatic invasive species and the water quality impairments or special designations listed on the current MPCA 303d Impaired Waters List that are within 1 mile of the project. Include DNR Public Waters Inventory number(s), if any.

There are no lakes, streams, intermittent channels, county/judicial ditches, or special designation surficial water features within or immediately adjacent to the Project Area. There is an impaired stream (Diamond Creek: Public Waters Inventory Number 27013a) with total maximum daily loads (TMDLs) for dissolved oxygen, *Escherichia coli*, and fish bioassessments⁷ and six public water basins within one mile of the Project Area⁸ based on the MPCA's 2024 list (**Table 10**) (**Figure 8, Appendix A**). The six public water basins include French Lake (MNDNR Division of Waters Lake Number 27012700) and five unnamed public water wetlands.

⁷ Minnesota Pollution Control Agency. 2024. *Impaired Waters: final 2024*. Available at:

<https://mpca.maps.arcgis.com/apps/webappviewer/index.html?id=fafc5a12d2fd4b16bc95bb535d09ae82>

⁸ Minnesota Department of Natural Resources. 2025. *Public Waters (PW) Basin and Watercourse Delineations*. GIS data available at:

https://www.dnr.state.mn.us/waters/watermgmt_section/pwi/maps_metro.html

Table 10. Public waters inventory basins and watercourses within one mile of the project area

Public waters inventory basin/watercourse name	Public waters inventory number	Minnesota Department of Natural Resources division of waters Lake number	Approximate distance and direction from project area
Unnamed Public Water Wetland	--	27030700	0.8 miles southwest
Unnamed Public Water Wetland	--	27024000	0.7 miles southeast
Unnamed Public Water Wetland	--	27023800	0.7 miles southeast
Unnamed Public Water Wetland	--	27024100	0.9 miles southeast
Unnamed Public Water Wetland	--	27023900	0.6 miles southeast
Diamond Creek	27013a	--	0.9 miles northeast

A wetland delineation conducted on June 11, 2020, identified three wetlands comprising approximately 3.2 acres within the Project Area (**Table 11**) (**Figure 8, Appendix A**) (**Appendix C**). Note that the wetland delineation did not include the far eastern portion of the Project Area or the additional 0.2 acres of wetland (3.4 acres total). The 3-acre wetland (see **Table 11**) extends into the portion of the Project Area not included in the wetland delineation. Also, there is a stormwater pond that is approximately 0.5-acres in size that was recently created in the portion of the Project Area that was not included in the wetland delineation.

Table 11. Surrounding Wetland Features

Wetland ID	Circular 39 designation	Cowardin attribute	Total acreage	Impact acreage	Impact duration
Wetland 1	Wet Meadow/Shallow Marsh	PEMB/Cd	3.00	0	Impacts to this wetland will be avoided entirely.
Wetland 2	Wet Meadow	PEM1Bf	0.08	0	Impacts to this wetland will be avoided entirely.
Wetland 3	Seasonally Flooded Basin	PEM1A	0.07	0	Impacts to this wetland will be avoided entirely.

- ii. Groundwater – aquifers, springs, seeps. Include: 1) depth to groundwater; 2) if project is within a MDH wellhead protection area; 3) identification of any onsite and/or nearby wells, including unique numbers and well logs if available. If there are no wells known on site or nearby, explain the methodology used to determine this.

The Project is within the Elm Creek Watershed Management District. There are no springs or seeps within the Project Area. Per the Minnesota Hydrogeology Atlas,⁹ depth to groundwater in the area ranges from 0 to 10 feet below ground surface. The general flow of groundwater in the region surrounding the Project Area is southwest to northeast, generally trending towards French Lake (MNDNR Division of Waters Lake Number 27012700).

Per the MDH Source Water Protection Web Map Viewer,¹⁰ the Facility is not within any Drinking Water Supply Management Areas or Wellhead Protection Areas. There are no wells within the Project Area. There are 13 wells within 0.25 miles of the Project Area (**Table 12**) (**Figure 9, Appendix A**).¹¹ The Facility is connected to the Maple Grove municipal water system.

⁹ Minnesota Department of Natural Resources. 2015. *Pollution Sensitivity of the Bedrock Surface, Minnesota Hydrogeology Atlas series HG-01*. Available at: https://files.dnr.state.mn.us/waters/groundwater_section/mapping/mha/hq01_meta.html#ordering

¹⁰ Minnesota Department of Health. No date. *Source Water Protection Web Map Viewer*. Available at: <https://experience.arcgis.com/experience/14825b159b2e4dc686736d98e39ebce7>

¹¹ Minnesota Department of Health. 2019. *Minnesota Well Index (MWI)*. Available at: [Minnesota Well Index \(MWI\) - Resources - Minnesota Geospatial Commons](https://www.mn.gov/Minnesota-Well-Index-(MWI)-Resources-Minnesota-Geospatial-Commons)

Table 12. Water wells within one-quarter mile of the project area

Well identification number	Well name
00470624	Codema Inc.
00555243	Olson Company
00743427	VSI Construction
00183911	Peterson, Earl A.
00492238	Christian Builders #549
00513686	Boggs, Mrs. Don
00179023	Knutson, Jerry
00410668	Spaanum, Thurber
00500596	Kahlstorf, Carol
00408653	Boggs, Donald
00523944	Witcher Const New Office
00745303	Boggs, Bob
00762358	French Lake Golf

b. Describe effects from project activities on water resources and measures to minimize or mitigate the effects in Item b.i. through Item b.iv. below.

i. Wastewater - For each of the following, describe the sources, quantities and composition of all sanitary, municipal/domestic and industrial wastewater produced or treated at the site.

- 1) If the wastewater discharge is to a publicly owned treatment Facility, identify any pretreatment measures and the ability of the Facility to handle the added water and waste loadings, including any effects on, or required expansion of, municipal wastewater infrastructure.

Facility wastewater is discharged to the Maple Grove municipal sewer system. No pre-treatment measures are necessary for the wastewater. Facility-related wastewater production is limited to that used by a few Curbside staff and some customers. Changes or expansions to the Maple Grove municipal wastewater infrastructure are not necessary for the Project. Curbside does not have a transfer station floor cleaning protocol currently. However, they do have a small floor sweeper that may be used to clean the transfer station floor. Curbside would dispose of water from the floor sweeper down a municipal drain at the wash bay and pass through the oil/water separator before entering the wastewater line.

- 2) If the wastewater discharge is to a subsurface sewage treatment systems (SSTS), describe the system used, the design flow, and suitability of site conditions for such a system. If septic systems are part of the project, describe the availability of septage disposal options within the region to handle the ongoing amounts generated as a result of the project. Consider the effects of current Minnesota climate trends and anticipated changes in rainfall frequency, intensity and amount with this discussion.

Not applicable.

- 3) If the wastewater discharge is to surface water, identify the wastewater treatment methods and identify discharge points and proposed effluent limitations to mitigate impacts. Discuss any effects to surface or groundwater from wastewater discharges, taking into consideration how current Minnesota climate trends and anticipated climate change in the general location of the project may influence the effects.

Not applicable.

- ii. Stormwater - Describe changes in surface hydrology resulting from change of land cover. Describe the routes and receiving water bodies for runoff from the project site (major downstream water bodies as well as the immediate receiving waters). Discuss environmental effects from stormwater discharges on receiving waters post construction including how the project will affect runoff volume, discharge rate and change in pollutants. Consider the effects of current Minnesota climate trends and anticipated changes in rainfall frequency, intensity and amount with this discussion. For projects requiring NPDES/SDS Construction Stormwater permit coverage, state the total number of acres that will be disturbed by the project and describe the stormwater pollution prevention plan (SWPPP), including specific best management practices to address soil erosion and sedimentation during and after project construction. Discuss permanent stormwater management plans, including methods of achieving volume reduction to restore or maintain the natural hydrology of the site using green infrastructure practices or other stormwater management practices. Identify any receiving waters that have construction-related water impairments or are classified as special as defined in the Construction Stormwater permit. Describe additional requirements for special and/or impaired waters.

No new construction or changes in the amount of impervious surface will occur for this Project, therefore surface hydrology should not change from existing conditions. If climate change results in an increase in average annual precipitation amounts, the existing stormwater pond infrastructure should have capacity to manage the increase in volume. The actual volume of 2.45 acre-feet is 0.66 acre-feet more than the required volume of 1.79 acre-feet based on 1,800 cubic feet per acre draining to the pond. The actual abstracted volume of the stormwater pond is 2.37 acre-feet which is 0.02 acre-feet more than the required abstracted volume. As the actual capacity of the stormwater pond is overbuilt, it should be able to manage potential increases in stormwater volume that may result from climate change.

- iii. Water appropriation - Describe if the project proposes to appropriate surface or groundwater (including dewatering). Describe the source, quantity, duration, use and purpose of the water use and if a DNR water appropriation permit is required. Describe any well abandonment. If connecting to an existing municipal water supply, identify the wells to be used as a water source and any effects on, or required expansion of, municipal water infrastructure. Discuss environmental effects from water appropriation, including an assessment of the water resources available for appropriation. Discuss how the proposed water use is resilient in the event of changes in total precipitation, large precipitation events, drought, increased temperatures, variable surface water flows and elevations, and longer growing seasons. Identify any measures to avoid, minimize, or mitigate environmental effects from the water appropriation. Describe contingency plans should the appropriation volume increase beyond infrastructure capacity or water supply for the project diminish in quantity or quality, such as reuse of water, connections with another water source, or emergency connections.

The Facility is connected to Dayton's municipal water system. The Project will not result in a change to the water supply connection or water usage from existing conditions.

- iv. Surface Waters

- a) Wetlands - Describe any anticipated physical effects or alterations to wetland features such as draining, filling, permanent inundation, dredging and vegetative removal. Discuss direct and indirect environmental effects from physical modification of wetlands, including the anticipated effects that any proposed wetland alterations may have to the host watershed, taking into consideration how

current Minnesota climate trends and anticipated climate change in the general location of the project may influence the effects. Identify measures to avoid (e.g., available alternatives that were considered), minimize, or mitigate environmental effects to wetlands. Discuss whether any required compensatory wetland mitigation for unavoidable wetland impacts will occur in the same minor or major watershed and identify those probable locations.

Curbside has already constructed the Facility; therefore no new construction or modifications are necessary for the Project. As such, the Project will not drain, fill, inundate, dredge, or remove vegetation from wetland areas. The Project will not impact the 3.2-acre wetland within the Project Area. Additionally, the presence of an existing berm and fence surrounding the Project Area would contain yard waste and minimize or avoid litter contamination of nearby wetlands. Curbside does not anticipate any wetland impacts; therefore, Curbside is not proposing mitigation.

- b) Other surface waters- Describe any anticipated physical effects or alterations to surface water features (lakes, streams, ponds, intermittent channels, county/judicial ditches) such as draining, filling, permanent inundation, dredging, diking, stream diversion, impoundment, aquatic plant removal and riparian alteration. Discuss direct and indirect environmental effects from physical modification of water features, taking into consideration how current Minnesota climate trends and anticipated climate change in the general location of the project may influence the effects. Identify measures to avoid, minimize, or mitigate environmental effects to surface water features, including in-water Best Management Practices that are proposed to avoid or minimize turbidity/sedimentation while physically altering the water features. Discuss how the project will change the number or type of watercraft on any water body, including current and projected watercraft usage.

The Project will not drain, fill, inundate, dredge, dike, divert, impound, or remove aquatic vegetation from surface water features. The Project will not require any construction activity. The Facility has an existing stormwater pond and fencing to minimize or avoid direct or indirect effects on surface waters and avoided entirely during the operation of the Project. Curbside does not anticipate any surface water impacts; therefore, no mitigation is proposed.

13. Contamination/hazardous materials/wastes:

- a. Pre-project site conditions - Describe existing contamination or potential environmental hazards on or in close proximity to the project site such as soil or ground water contamination, abandoned dumps, closed landfills, existing or abandoned storage tanks, and hazardous liquid or gas pipelines. Discuss any potential environmental effects from pre-project site conditions that would be caused or exacerbated by project construction and operation. Identify measures to avoid, minimize or mitigate adverse effects from existing contamination or potential environmental hazards. Include development of a Contingency Plan or Response Action Plan.

Based on review of the MPCA's What's in My Neighborhood (WIMN) database,¹² the Facility has a solid waste, permit by rule (Site ID 263142) and there were 12 sites identified within one-quarter mile of the Project Area (**Table 13**) (**Figure 10, Appendix A**).

¹² Minnesota Pollution Control Agency. 2025. *What's in My Neighborhood*. Available at: <https://mpca.maps.arcgis.com/apps/webappviewer/index.html>

Table 13. Minnesota Pollution Control Agency What's in My Neighborhood Records within One-Quarter Mile of the Project Area

Site ID	Site name	MPCA program	Status	Location
263142	Dayton Transfer Station 18150 118 th Avenue North Dayton, MN 55369	Solid Waste, Permit by Rule (PBR001457)	Active	This is the Facility.
23427	JE Dunn Construction Co. 11771 Troy Lane North Dayton, MN 55369	Aboveground Storage Tank (AST) (MN0000998260). Hazardous Waste, Very small quantity generator (TS0122335)	Active 300-gallon AST storing used oil (Installed 4/15/1993). Active hazardous waste generator of automotive cleaning solvents for years 1985, 2008, 2009, 2010, 2011, 2013, 2014, 2016, 2017, 2018, 2019, 2020, 2021, 2023, and 2024.	Approximately 0.19 miles southwest of the Project Area.
252244	Dayton 94 and Troy Lane Extension Dayton, MN 55369	Construction Stormwater Permit (C00064146)	Inactive (Permit issued 6/21/2022 and terminated 10/24/2024)	Approximately 0.18 miles south of the Project Area.
255461	MTL Troy Lane Addition Troy Lane North Dayton, MN 55369	Construction Stormwater Permit (C00066674)	Inactive (Permit issued 5/10/2023 and terminated 7/18/2023)	Approximately 0.16 miles west of the Project Area.
23047	Prokart Indoors - Maple Grove 11700 Troy Lane North Dayton, MN 55369-9279	Hazardous Waste - Minimal quantity generator (MND985771435)	Active hazardous waste generator of cleaning solvents/non-chlorinated solvents for years 2014, 2016, 2017, 2018, 2019, 2020, 2021, 2023, 2024 and 2025.	Approximately 0.21 miles southwest of the Project Area.
252977	West French Lake Road 17400 117th Street West and north to 12100 West French Lake Road	Construction Stormwater Permit (C00064714)	Active (Permit issued 8/12/2022)	Approximately 60 feet northeast of the Project Area.
185775	VSI Construction 11751 Troy Lane Dayton, MN 55369	Brownfields, Petroleum Brownfield (PB4443)	Inactive (Site closed 10/31/2013)	Approximately 0.2 miles southwest of the Project Area.
231561	Spears Manufacturing 12700 West French Lake Road Dayton, MN 55327	Construction Stormwater Permit (C00054458)	Inactive (Permit issued 7/23/2019 and terminated 8/20/2021)	Approximately 0.17 miles northeast of the Project Area.

Site ID	Site name	MPCA program	Status	Location
31973	Codema LLC 11790 Troy Lane North Suite A Maple Grove, MN 55369-4705	Hazardous Waste - Very small quantity generator (MND985745645)	Active hazardous waste generator of washing solvents and waste paint for years 2008, 2009, 2010, 2011, 2013, 2014, 2016, 2017, 2018, 2019, 2020, 2021, 2023, 2024 and 2025.	Approximately 0.18 miles southwest of the Project Area.
186781	Dayton Park Dump Dayton, MN 55369	Site Assessment (SA0007676)	Inactive (Site closed 1/11/2000)	Approximately 0.19 miles northwest of the Project Area.
216587	Elevation Coating Warehouse 11790 Troy Lane North Suite B Maple Grove, MN 55369-4734	Hazardous Waste (MNS000312136)	Inactive. The last reported hazardous waste generator license application was registered 9/10/2024. This site was a previous hazardous waste generator of waste paint/paint thinner for years 2017, 2018, 2019, 2020, 2021, 2023 and 2024.	Approximately 0.18 miles southwest of the Project Area.
254163	West French Lake Road West French Lake Road Dayton, MN 55327	Construction Stormwater Permit (C00065683)	Inactive (Permit issued 1/18/2023 and terminated 9/23/2024)	Approximately 60 feet northeast of the Project Area.
252760	Dayton Fields 121st and French Lake Road West Dayton, MN 55369	Construction Stormwater Permit (C00064484)	Active (Permit issued 7/22/2022)	Approximately 0.14 miles northwest of the Project Area.

Braun Intertec Corporation completed a Phase I Environmental Site Assessment (ESA) on May 16, 2023, prior to construction of the Facility. The results of the Phase I ESA are provided in Appendix D. As the Project will not require construction, Curbside does not anticipate that potentially contaminated sites within the vicinity of the Project Area will be exposed or exacerbated.

- b. Project related generation/storage of solid wastes - Describe solid wastes generated/stored during construction and/or operation of the project. Indicate method of disposal. Discuss potential environmental effects from solid waste handling, storage and disposal. Identify measures to avoid, minimize or mitigate adverse effects from the generation/storage of solid waste including source reduction and recycling.

Curbside would generate minimal solid waste at the transfer station during operation. Specifically, MSW and single-stream recyclables may be generated by Curbside staff throughout the work week (i.e., single-use plastic or aluminum cans).

Potential environmental effects include contamination from improper disposal/littering. Trash and recycling bins would be co-located and available in multiple locations for Curbside staff and customers to properly dispose of items while on-site. Curbside staff would receive environmental/compliance training regarding the proper disposal of waste. Curbside will install full coverage doors on all six openings to the transfer station, and operational plans would aim to strategically deliver materials when wind speeds are high and could blow material outside the building, which will help keep material contained in the transfer station. Curbside staff would walk the perimeter of the site on a daily to weekly basis to make sure all litter is collected and contained. If needed, Curbside would add a second barbed wire arm that leans inward instead of outward to capture additional litter in addition to the current chain link fence surrounding the Project. There may be occasional temporary storage of waste materials overnight or over a weekend within the transfer station with the doors closed. Curbside will remove waste materials stored overnight or over a weekend on the following operations day. Curbside will dispose of all waste materials at an appropriate permitted Facility. Curbside will comply with Minnesota Administrative Rule 7035.2870 Subp. 5H, which requires that transfer station floors be cleared every seven days.

- c. Project related use/storage of hazardous materials - Describe chemicals/hazardous materials used/stored during construction and/or operation of the project including method of storage. Indicate the number, location and size of any new above or below ground tanks to store petroleum or other materials. Indicate the number, location, size and age of existing tanks on the property that the project will use. Discuss potential environmental effects from accidental spill or release of hazardous materials. Identify measures to avoid, minimize or mitigate adverse effects from the use/storage of chemicals/hazardous materials including source reduction and recycling. Include development of a spill prevention plan.

Curbside will not use or store any chemicals or hazardous materials at the Project Area during operation. No new above or below ground tanks to store petroleum or other materials would be constructed or used during operation. Spills resulting from non-hazardous liquid wastes would be addressed by the trench drains and spill kits stored onsite as needed.

- d. Project related generation/storage of hazardous wastes - Describe hazardous wastes generated/stored during construction and/or operation of the project. Indicate method of disposal. Discuss potential environmental effects from hazardous waste handling, storage, and disposal. Identify measures to avoid, minimize or mitigate adverse effects from the generation/storage of hazardous waste including source reduction and recycling

Curbside will not generate, accept, or store any hazardous waste in the Project Area during operation. Curbside will not accept any hazardous solid waste for temporary storage or for transfer to the Project Area. Customers attempting to dispose of hazardous solid waste at the Facility would be informed of the waste acceptance policies and directed to nearby approved hazardous waste drop-off sites or other resources.

14. Fish, wildlife, plant communities, and sensitive ecological resources (rare features):

- a. Describe fish and wildlife resources as well as habitats and vegetation on or in near the site.

Most of the Project Area (72 percent) is either unvegetated or heavily manicured (i.e., mowed lawn). The remaining 28 percent of the Project Area is comprised of wetlands fringed by cattails and some adjacent herbaceous upland areas interspersed with shrubs and small trees.

Fauna

Wildlife species that may use the developed portion of the Project Area include the non-native house mouse (*Mus musculus*) and house sparrow (*Passer domesticus*). Wildlife species that may use the wetland and stormwater pond portions of the Project Area include the northern leopard frog (*Lithobates pipiens*), American toad (*Anaxyrus americanus*) and other amphibians, mallard (*Anas platyrhynchos*) and other waterfowl species, greater egret (*Ardea alba*), painted (*Chrysemys picta*) and snapping (*Chelydra serpentina*) turtles, passerines (i.e., song sparrow [*Melospiza melodia*] and red-winged blackbird [*Agelaius phoeniceus*]), and common mammalian species such as the eastern cottontail (*Sylvilagus floridanus*) and eastern meadow vole (*Microtus drummondii*). The wetlands within the Project Area may provide habitat for fish species such as the central mudminnow (*Umbra limi*).

- b. Describe rare features such as state-listed (endangered, threatened or special concern) species, native plant communities, Minnesota Biological Survey Sites of Biodiversity Significance, and other sensitive ecological resources on or within close proximity to the site. Provide the license agreement number (LA- **2025-001**) and/or correspondence number (MCE **2025-00682**) from which the data were obtained and attach the Natural Heritage Review letter from the DNR. Indicate if any additional habitat or species survey work has been conducted within the site and describe the results.

There are no Minnesota Biological Survey (MBS) sites of biodiversity significance or MNDNR native plant communities within the Project Area or within one mile of the Project Area. French Lake (ID No. 27012700) 0.1 miles northeast of the Project Area is categorized as a lake of high biological significance.¹³ There are no old growth stands or calcareous fens within the Project Area or within one mile of the Project Area.¹⁴

State-Listed Species

An official Natural Heritage Report was received from the MNDNR on October 22, 2025. As the Project will not require new construction or modifications to existing land uses, the MNDNR determined that the Project will not negatively affect any known occurrences of rare features (**Appendix E**).

Federally Listed

Based on the USFWS Information of Planning and Consultation (IPaC) report, the experimental, non-essential population of whooping crane (*Grus americana*), proposed endangered salamander mussel (*Simpsonaias ambigua*), and proposed threatened monarch butterfly (*Danaus plexippus*) and western regal fritillary (*Argynnis idalia occidentalis*) may occur within the Project Area or surrounding region. Whooping cranes use a variety of habitats including marshes, wetlands, pastures, flooded agricultural fields, but they avoid areas with tall structures (i.e., trees and buildings) and areas near roads. Salamander mussels inhabit fast-flowing rivers and streams. Monarch caterpillars only occur on milkweed species (*Asclepias* spp.) and adult monarchs will forage on a variety of flowering plants in herbaceous upland or wetland areas. Western regal fritillary caterpillars only occur in areas with abundant violets (*Viola* spp.) in tallgrass prairies and adults forage on a variety of flowering

¹³ Minnesota Department of Natural Resources. 2024. Lakes – Biological Significance. Available at: https://mnatlas.org/gis-tool/?id=k_0010

¹⁴ Minnesota Department of Natural Resources. 2024. Forestry – Inventory: Old Growth. Available at: <https://mnatlas.org/resources/forestry-inventory-old-growth/>

plants in tallgrass prairies. USFWS designated critical habitats were not mapped within the Project Area or within one mile of the Project Area.

Curbside has not conducted any additional habitat assessments or rare species surveys.

- c. Discuss how the identified fish, wildlife, plant communities, rare features and ecosystems may be affected by the project including how current Minnesota climate trends and anticipated climate change in the general location of the project may influence the effects. Include a discussion on introduction and spread of invasive species from the project construction and operation. Separately discuss effects to known threatened and endangered species.

The Project will not require new construction or modifications to existing infrastructure. As such, Curbside does not expect any impacts to fish, wildlife, plant communities, rare features, or ecosystems within or near the Project Area. The Project proposes to increase the volume and types of waste materials transported to and from the existing transfer station building and would not alter existing or surrounding land cover or habitat. Additionally, the presence of an existing berm and fence surrounding the Facility will contain waste materials and prevent contamination of nearby natural areas.

Curbside does not expect any impacts to MBS sites of biodiversity significance or MNDNR native plant communities as there are none within the Project Area or within one mile of the Project Area. There is no suitable habitat for the whooping crane, salamander mussel, western regal fritillary, or red saltwort within the Project Area; therefore, Curbside expects no impacts to these species. Although there is potentially suitable habitat for the common gallinule and monarch butterfly in or near the wetland portion of the Project Area, Curbside does not expect the Project to impact the undeveloped areas.

Climate trends for Hennepin County suggest a projected increase in temperatures and precipitation over the coming decades. The projected changes in climate metrics could, over time, facilitate changes in plant communities in the wetland portion of the Project Area and in non-developed areas in the surrounding region. Most of the land cover within the Project Area is developed (i.e., impervious) and does not provide suitable habitat for fish, wildlife, or threatened or endangered species. Curbside does not expect the Project to contribute to climate change-related impacts to wildlife or their habitats in the Project Area or surrounding region.

Curbside mitigates the spread of invasive plant species by cleaning collection trucks regularly in their on-site wash bay. The water from cleaning collection trucks does not enter storm sewers. Some vehicle washing is done in the wash bay which connects to the sanitary system after the wash-water passes through an oil water separator. Occasionally, vehicles are washed outside by a third-party company. When vehicles are washed outside, the wash-water is reclaimed by the third-party company before disposing down the sanitary sewer system. The third-party company covers the storm drain in the primary vehicle parking area before any vehicles are washed, the water from vehicle washing is reclaimed into the truck washing vehicle or pumped to the wash bay floor drain to avoid any wash water going into the storm drain.

- d. Identify measures that will be taken to avoid, minimize, or mitigate the adverse effects to fish, wildlife, plant communities, ecosystems, and sensitive ecological resources.

Project impacts (i.e., waste debris) would be contained to the Project Area with the existing fence and use of the doors that will be constructed on the transfer station and would not impart adverse effects to surrounding fish, wildlife, plant communities, ecosystems, or sensitive ecological resources. Note that the fence may impact movements of medium to large sized mammals and larger turtles. The Project will not require direct impacts to existing land cover or changes to existing infrastructure. Accordingly, Curbside is not proposing any mitigation measures.

15. Historic properties:

Describe any historic structures, archeological sites, and/or traditional cultural properties on or in close proximity to the site. Include: 1) historic designations, 2) known artifact areas, and 3) architectural features. Attach letter

received from the State Historic Preservation Office (SHPO). Discuss any anticipated effects to historic properties during project construction and operation. Identify measures that will be taken to avoid, minimize, or mitigate adverse effects to historic properties.

HDR conducted a literature review to identify recorded cultural resources within the Project Area and within 0.25 miles of the Project Area using the Minnesota Office of the State Archaeologist (OSA) Portal, the SHPO Minnesota Statewide Historic Inventory Portal (MnSHIP), the Minnesota State Historic Sites Network (MS 138.662), the State Register of Historic Places (138.664), and the National Park Service National Register of Historic Places (NRHP) web map.^{15, 16, 17}

Prior to construction of the Facility in 2024, the Project Area was surveyed for archaeological sites in 2021 by Nienow Cultural Consultants. No archaeological sites were identified within the Facility footprint during the 2021 survey. The nearest archaeological site, 21HE0547 (identified during the 2021 survey), is 500 feet from the Project Area. The site consists of one precontact isolated flake. This site has not been formally evaluated for NRHP eligibility. As the site is 500 feet from the Project Area and the Project would not include ground disturbance, the Project would not impact this site. There are no other archaeological sites within 0.25 miles of the Project Area.

The literature review identified no previously inventoried historic structures on the MnSHIP portal and no historic properties on the State Register of Historic Places, State Historic Sites Network, nor eligible for or listed in the NRHP within 0.25 miles of the Project Area. SHPO concurred with the determination that no archaeological resources will be affected by the Project; and that no properties listed in the National or State Registers of Historic Places or within the Historic Sites Network will be affected by the Project on October 14, 2025 (**Appendix F**). Project review requests were made to the OSA and the Minnesota Indian Affairs Council (MIAC) via email on September 29, November 26, and December 30, 2025. Responses have not been received from OSA or MIAC at the time this EAW was prepared (**Appendix F**).

16. Visual:

Describe any scenic views or vistas on or near the project site. Describe any project related visual effects such as vapor plumes or glare from intense lights. Discuss the potential visual effects from the project. Identify any measures to avoid, minimize, or mitigate visual effects.

The Great River Road is the nearest scenic byway, which is more than 4.1 miles north of the Project Area. Viewers at ground level can typically see three miles to the horizon on unobstructed terrain. However, the transfer station and associated infrastructure are already built and no expansions or modifications to the existing infrastructure will occur. Other industrial facilities, agricultural lands, and some single-family residences surround the Project Area. Agricultural and residential land uses border the Facility to the north and east and industrial uses border to the southwest. The Project would be consistent with the surrounding industrial and agricultural uses and therefore no additional visual impacts (including increases to traffic) would be incurred because of increased waste transfer capacity. The Project would be consistent with the surrounding industrial and agricultural uses.

¹⁵ Minnesota Department of Administration – State Historic Preservation Office. Minnesota Statewide Historic Inventory Portal. Available at: <https://mnship.qisdata.mn.gov/>

¹⁶ National Park Service. National Register of Historic Places. 2020. Available at: <https://www.nps.gov/maps/full.html?mapId=7ad17cc9-b808-4ff8-a2f9-a99909164466>

¹⁷ Minnesota Department of Administration – State Archaeologist. No date. Office of the State Archaeologist (OSA) Portal. Available at: <https://osaportal.qisdata.mn.gov/>

17. Air:

- a. Stationary source emissions - Describe the type, sources, quantities and compositions of any emissions from stationary sources such as boilers or exhaust stacks. Include any hazardous air pollutants, criteria pollutants. Discuss effects to air quality including any sensitive receptors, human health or applicable regulatory criteria. Include a discussion of any methods used assess the project's effect on air quality and the results of that assessment. Identify pollution control equipment and other measures that will be taken to avoid, minimize, or mitigate adverse effects from stationary source emissions.

No stationary source emissions exist in the Project Area. No hazardous air pollutants would be emitted as no hazardous solid waste is accepted at the Facility nor would it be accepted at the Project. The Curbside anticipates minimal impact to surrounding air quality due to the Project and does not require an air permit. Therefore, no pollution control equipment is required to be installed or in operation at the Project.

- b. Vehicle emissions - Describe the effect of the project's traffic generation on air emissions. Discuss the project's vehicle-related emissions effect on air quality. Identify measures (e.g. traffic operational improvements, diesel idling minimization plan) that will be taken to minimize or mitigate vehicle-related emissions.

There would be no increase in traffic to or from the Project Area as the collection trucks currently start and end their daily activities at the Facility. Therefore, the collection trucks carrying non-yard waste materials could drop their final load of the day at the Project. Curbside anticipates that other vehicles to visit the Project Area include private passenger vehicles for workers and customers, two front loaders, and semi-trucks with open trailers.

To minimize vehicle-related emissions and maintain a safe working environment, Curbside will ask drivers to turn off their engines if they are unloading accepted solid wastes onto the tipping floor from a trailer. Note that some vehicles such as packer trucks require the engine remain running to dump the material on the tipping floor. Semi-trucks engines would only run when the semi-trucks are entering or leaving the transfer station, they would not idle while being loaded with waste materials to be transferred to the final destination for the solid waste.

Only four vehicles can safely unload on the tipping floor simultaneously, which would limit the concentration of vehicle emissions in the Project Area at any given time. Trucks line up along the east/southeast side of the Facility as they stage to move through the building.

- c. Dust and odors - Describe sources, characteristics, duration, quantities, and intensity of dust and odors generated during project construction and operation. (Fugitive dust may be discussed under item 17a). Discuss the effect of dust and odors in the vicinity of the project including nearby sensitive receptors and quality of life. Identify measures that will be taken to minimize or mitigate the effects of dust and odors.

The transfer station and associated infrastructure areas are paved or concrete, so dust emissions during operation would be limited to that of the accepted waste materials. The effect of dust in the vicinity of the Project would be minimal. Dust control measures include the six rolling doors which Curbside can close as needed to prevent the escape of dust generated from the waste materials being unloaded and moved on the tipping floor.

Odors from accepted waste materials are possible. The effect of odor in the vicinity of the Project may impact quality of life if not appropriately managed. Curbside will manage odors by moving material through the transfer station as quickly as possible. For example, most materials will be loaded and removed from the Project Area on a daily or next-day basis. Odor control measures include the six rolling doors, which Curbside can close as needed to limit the escape of odors from the Project to nearby sensitive receptors as well as good housekeeping procedures, limited onsite storage, and regular floor cleanings. There may be occasional temporary storage of waste materials overnight or over a weekend within the transfer station with the doors closed. Curbside will remove waste materials stored overnight or over a weekend on the following operations day. Curbside will

comply with Minnesota Administrative Rule 7035.2870 Subp. 5H, which requires that transfer station floors be cleared every seven days.

18. Greenhouse Gas (GHG) Emissions/Carbon Footprint

- a. GHG Quantification: For all proposed projects, provide quantification and discussion of project GHG emissions. Include additional rows in the tables as necessary to provide project-specific emission sources. Describe the methods used to quantify emissions. If calculation methods are not readily available to quantify GHG emissions for a source, describe the process used to come to that conclusion and any GHG emission sources not included in the total calculation.

As the Project will not involve any construction, only emissions from the operations phase are provided here (Table 14). Also, the waste materials transferred through the Project would not remain onsite long enough to contribute to Project greenhouse gas emissions. Note that the values provided throughout Item 18 below are based on the existing operations for the Facility, and that the proposed Project is expected to produce comparable levels of greenhouse gas emissions. Therefore, waste material-related greenhouse gas emissions are not expected because of the Project, and were not added here.

Table 14. Emission Categories for Carbon Footprint

Category	Scope	Project Phase	Type of Emissions
Direct Emissions	Scope 1	Operations	Combustion (Mobile Sources)
	Scope 1	Operations	Combustion (Stationary Sources)
Indirect Emissions	Scope 2	Operations	Off-Site Electricity
	Scope 3	Operations	Off-Site Waste Management

Operational Emissions – Mobile Sources

The average number of daily trips for the Project are provided in Table 15.

Table 15. Average Maximum Number of Daily Trips

Projected Vehicle Types	Trips/Day
Commercial	64
Personal	80
Total	144

For the Project, it was estimated that daily trips take place 307 days per year (i.e., not including Sundays or the six main holidays). It was estimated that daily trips for the 11 commercial vehicles that are used on a seasonal basis (i.e., residential yard waste collection occurs from April to November) would take place 204 days per year. The commute of workers, customers, and commercial vehicles was estimated at 30 miles daily. The total estimate of miles traveled by personal vehicles annually is 736,800 (80 vehicles x 30-mile commute x 307 days). The total estimate of miles traveled by commercial vehicles annually is 555,450 ([53 vehicles x 30-mile commute x 307 days] + [11 vehicles x 30-mile commute x 204 days]).

Gas mileage for personal vehicles was estimated at 24.7 miles per gallon.¹⁸ Gas mileage estimates for semi-tractor trailers was 6.0 miles per gallon and for all other commercial vehicles was 4.5 miles per gallon.¹⁹ GHG

¹⁸ U.S. Department of Transportation – Bureau of Transportation Statistics. 2025. Average Fuel Efficiency of U.S. Light-Duty Vehicles. Available at: <https://www.bts.gov/content/average-fuel-efficiency-us-light-duty-vehicles>

¹⁹ U.S. Department of Energy. 2024. FOTW #1342, May 13, 2024: Average On-Road Fuel Economy for Medium/Heavy Trucks Ranged from 4.0 mpg to 10.1 mpg in 2021. Available at: <https://www.energy.gov/eere/vehicles/articles/fotw-1342-may-13-2024-average-road-fuel-economy-mediumheavy-trucks-ranged-40>

emissions associated with Project-related trips were calculated at 1,480.4 short tons of carbon dioxide equivalent per year using the Emission Factors Hub.²⁰ Curbside does not have any compressed natural gas powered vehicles in their fleet.

Operational Emissions – Stationary Combustion

Based on the observed natural gas usage from the previous calendar year, approximately 16,750 therms or 97.7 short tons of carbon dioxide equivalent are used annually. Natural gas combustion GHG emissions were calculated using the Greenhouse Gas Equivalencies Calculator.²¹

Operational Emissions – Offsite Electricity Production

Based on estimates from observed offsite electricity usage, approximately 120,000 kilowatt hours or 52.8 short tons of carbon dioxide equivalent are used annually. Offsite electricity GHG emissions were calculated using the Greenhouse Gas Equivalencies Calculator.²²

Operational Emissions – Waste Management

GHG emissions for Project-related waste management were determined based on how the waste is handled and by using waste generation estimates. Annual waste generation rates were calculated using a daily waste generation estimate of 75 pounds based on current generation amounts for 307 days (i.e., not including Sundays or the six main holidays). The Project-related waste generation rate is estimated at 11.5 short tons per year. GHG emissions associated with Project-related waste management was calculated at 7.3 short tons of carbon dioxide equivalent per year.

Summary

Total GHG emission estimates were calculated at 1,638 short tons of carbon dioxide equivalent per year (Table 16).

Table 16. Greenhouse Gas Emissions Summary (CO2e in short tons per year)

Scope	Source	Project Emissions (CO2e)
Direct Emissions		
Scope 1	Operations (Combustion - Mobile Sources)	1,480.4
Scope 1	Operations (Combustion – Stationary Sources)	97.7
Indirect Emissions		
Scope 2	Operations (Off-Site Electricity)	52.8
Scope 3	Operations (Off-Site Waste Management)	7.3
Total		1,638.2

b. GHG Assessment

- i. Describe any mitigation considered to reduce the project’s GHG emissions.

The following activities were implemented when the Facility was built to help mitigate the Facility’s GHG emissions. These activities will be continued for the Project:

²⁰ U.S. Environmental Protection Agency. 2025. GHG Emission Factors Hub. Available at: <https://www.epa.gov/climateleadership/ghg-emission-factors-hub>

²¹ U.S. Environmental Protection Agency. 2025. Greenhouse Gas Equivalencies Calculator. Available at: <https://www.epa.gov/energy/greenhouse-gas-equivalencies-calculator>

²² U.S. Environmental Protection Agency. 2025. Greenhouse Gas Equivalencies Calculator. Available at: <https://www.epa.gov/energy/greenhouse-gas-equivalencies-calculator>

Maintaining all existing trees.

- Using energy-efficient lighting in buildings and the parking lot.
- Using energy-efficient appliances, heating, and air conditioning systems.
- Using programmable thermostats.
- Continue exploration of using electric/hybrid vehicles.
 - ii. Describe and quantify reductions from selected mitigation, if proposed to reduce the project's GHG emissions. Explain why the selected mitigation was preferred.

Curbside is committed to continuing the implementation of GHG emission reducing efforts through energy efficient lighting, appliances, heating and air conditioning systems, maintaining all existing trees, and using programmable thermostats. These mitigation measures were selected as they are currently being implemented. They are also considering options to include hybrid or electric vehicles in their fleet of collection trucks to reduce mobile source combustion.

- iii. Quantify the proposed projects predicted net lifetime GHG emissions (total tons/#of years) and how those predicted emissions may affect achievement of the Minnesota Next Generation Energy Act goals and/or other more stringent state or local GHG reduction goals.

As there is no designed end to Project operations, Curbside used a 24-year lifetime to estimate net lifetime GHG emissions to align with the 2050 milestone of the Minnesota Next Generation Energy Act. Estimated GHG emissions for the next 24 years are 42,552 short tons of carbon dioxide equivalent. Note that the GHG emission estimates would be produced by the Facility and are not considered additional emissions resulting from the Project. Waste materials will be onsite briefly (i.e., typically for less than 48 hours) and will not contribute to GHG emissions in a measurable way. After the waste materials are transported to their final off-site destinations, they are estimated to contribute 728 pounds of carbon dioxide equivalent per short ton for conventional landfill disposal and 1,400 to 2,399 pounds of carbon dioxide equivalent per short ton for incineration.^{23,24} The Project's GHG emissions would have a minimal effect on the State's or local area's GHG reduction goals. Therefore, Curbside proposes no further mitigation.

19. Noise

Describe sources, characteristics, duration, quantities, and intensity of noise generated during project construction and operation. Discuss the effect of noise in the vicinity of the project including 1) existing noise levels/sources in the area, 2) nearby sensitive receptors, 3) conformance to state noise standards, and 4) quality of life. Identify measures that will be taken to minimize or mitigate the effects of noise.

Noise Area Classifications (NAC) for Minn. Rules, Chapter 7030.0040 are categorized by the type of land use activities at a location and the sensitivity of those activities to noise. Residential- and institutional-type land use activities, including homes, churches, camping and picnicking areas, and hotels, are in NAC-1. Commercial-type land use activities, such as transit terminals, retail and business services, are included in NAC-2. Industrial-type land use activities, such as mining activities, are included in NAC-3 and would comprise the entirety of the proposed Project activities. MPCA noise standards are listed below in Table 17.

²³ Johnke, B. et al. No date. Emissions from waste incineration. In: *Good Practice Guidance and Uncertainty Management in National Greenhouse Gas Inventories*. Pp. 1-14. Available at: https://www.ipcc-nggip.iges.or.jp/public/gp/bgp/5_3_Waste_Incineration.pdf

²⁴ Manfredi, S. et al. 2009. Landfilling of waste: accounting of greenhouse gases and global warming contributions. *Waste Management and Research* 27(8): 825-836. Available at: <https://pubmed.ncbi.nlm.nih.gov/19808732/>

Table 17. Noise Standards

Noise Area Classification	Daytime		Nighttime	
	L ₅₀	L ₁₀	L ₅₀	L ₁₀
1: Residential-type Land Use Activities	60	65	50	55
2: Retail-type Land Use Activities	65	70	65	70
3: Manufacturing-type Land Use Activities	75	80	75	80

The Facility is at the eastern end of an established industrial park. There are four residential properties to the east across West French Lake Road. Curbside has not received any noise complaints from the residences since the Facility became operational. During the local permitting process for the Facility, conditions were established so noise from the Facility did not adversely impact nearby residences. These conditions include limiting operating hours and requiring that maintenance activity be complete indoors. For example, the east-facing maintenance building doors are not opened before 7:00 am, which reduces the level of noise outside of the building. Furthermore, on-site activities do not create prolonged noise disturbances. Vehicles that come and go throughout the day do not create excessive noise. Noise associated with maintenance activities, including any cutting, welding, or grinding, is completed inside the maintenance building. The most common noise produced on-site that would be produced is the backup beeper sound from collection vehicles and the front loader or bobcat when in use. Existing noise levels in the Project Area are influenced by traffic from nearby roads, occasional freight trains, and operation of the existing Facility. Nearby sensitive receptors include a mobile home park approximately 0.3 miles from the Facility to the west, one residence approximately 150 feet to the northeast, and four residences ranging from approximately 0.1 to 0.2 miles to the southeast. Curbside would limit all activities associated with the Project to daytime hours (not overnight) and would be in conformance with state and local noise standards. There will not be any construction-related noise as the Project is already built to handle the requested capacity of waste materials. Curbside is committed to continue complying with the noise standards set forth in Minn. Rules, Chapter 7030.0040.

20. Transportation

- a. Describe traffic-related aspects of project construction and operation. Include: 1) existing and proposed additional parking spaces, 2) estimated total average daily traffic generated, 3) estimated maximum peak hour traffic generated and time of occurrence, 4) indicate source of trip generation rates used in the estimates, and 5) availability of transit and/or other alternative transportation modes.

There are currently 30 striped truck parking spaces and 54 passenger vehicle parking spaces (including two handicap spaces) in the parking lot. Additional vehicle parking takes place in an orderly fashion across the site. Curbside does not plan any additional parking spaces for the Project. The Project's operating hours are generally Monday to Friday during daytime hours (not overnight). The Project operates on Saturdays following the six major holidays when they occur on a weekday. Operating hours may be extended to include all Saturdays in the future. Curbside manages a fleet of approximately 60 collection vehicles to and from the Project Area on a typical day of operation (Table 18). Peak traffic times are from 4 AM to 7 AM and 2 PM to 5 PM. Project-related activities typically include trucks collecting waste materials along commercial and residential trash and recycling routes, residential organics and residential yard waste routes, and roll-off trucks collecting C&D debris. Curbside then loads waste materials onto trailers of semi-tractor trailers, covered, and transferred to a public or private disposal Facility in the greater metropolitan area. Curbside does not expect any changes to the number of collection vehicles or collection vehicle trips from the Project.

Curbside expects customers to follow traffic flow signage, unload their waste materials on the tipping floor or in roll-off container dumpsters, and then exit the Project Area. The Project Area will only be accessible to customers

during operating hours. Curbside will shut the doors to the transfer station, and the entrances/exits will be gated and locked outside of operating hours.

Table 18. Potential Daily Vehicle Operations for the Project (at current fleet size)

Vehicle/Route Type	Vehicles Per Day
Commercial (Front Load)	6 to 8
Residential Trash	26 to 27
Residential Recycling	3 to 5
Residential Yard Waste	9 to 11 (seasonal April to November)
Residential Organics	1 to 2
Roll-Off Construction and Demolition	2 to 3
Semi-Tractor Trailer	6 to 8
Private Vehicles (Staff and Customers)	50 to 80
Total	103 to 144

- b. Discuss the effect on traffic congestion on affected roads and describe any traffic improvements necessary. The analysis must discuss the project’s impact on the regional transportation system. *If the peak hour traffic generated exceeds 250 vehicles or the total daily trips exceeds 2,500, a traffic impact study must be prepared as part of the EAW.* Use the format and procedures described in the Minnesota Department of Transportation’s Access Management Manual, Chapter 5 (available at: <http://www.dot.state.mn.us/accessmanagement/resources.html>) or a similar local guidance,

The total number of daily trips including Curbside vehicles and private vehicles are not expected to exceed 144 vehicles. Peak traffic times are in the early morning and early afternoon. Curbside does not expect traffic volume to significantly change from the existing traffic conditions due to the Project, as the same number of fleet and staff vehicles currently travel to the Facility. Therefore, Curbside does not expect any effects on traffic congestion on affected roads.

- c. Identify measures that will be taken to minimize or mitigate project related transportation effects.

Curbside does not expect changes in traffic volume or peak times from the Project as collection trucks, semi-tractor trailers, and personal vehicles of Curbside staff and customers currently travel to and from the Project Area. Curbside is not proposing any traffic improvements at this time.

21. Cumulative potential effects: (Preparers can leave this item blank if cumulative potential effects are addressed under the applicable EAW Items)

- a. Describe the geographic scales and timeframes of the project related environmental effects that could combine with other environmental effects resulting in cumulative potential effects.
- b. Describe any reasonably foreseeable future projects (for which a basis of expectation has been laid) that may interact with environmental effects of the proposed project within the geographic scales and timeframes identified above.
- c. Discuss the nature of the cumulative potential effects and summarize any other available information relevant to determining whether there is potential for significant environmental effects due to these cumulative effects.

22. Other potential environmental effects: If the project may cause any additional environmental effects not addressed by items 1 to 19, describe the effects here, discuss the how the environment will be affected, and identify measures that will be taken to minimize and mitigate these effects.

Curbside does not anticipate any other potential environmental effects due to the Project that are not identified in items 1 through 19.

RGU CERTIFICATION. *(The Environmental Quality Board will only accept **SIGNED** Environmental Assessment Worksheets for public notice in the EQB Monitor.)*

I hereby certify that:

- The information contained in this document is accurate and complete to the best of my knowledge.
- The EAW describes the complete project; there are no other projects, stages or components other than those described in this document, which are related to the project as connected actions or phased actions, as defined at Minnesota Rules, parts 4410.0200, subparts 9c and 60, respectively.
- Copies of this EAW are being sent to the entire EQB distribution list.

Signature: *Katrina Hapka*

Date: April 16, 2026

This document has been electronically signed.

Katrina Hapka
Supervisor
Environmental review Unit
Resource Management and Assistance Division

ITEM:

Consideration of Letter of Support for Regional Solicitation

PREPARED BY:

Zach Doud, City Administrator

POLICY DECISION / ACTION TO BE CONSIDERED:

Consider Reviewing the Letter of Support for Regional Solicitation

BACKGROUND:

City Staff was approached by Hennepin County to provide a letter of support for the 2026 Regional Solicitation funding. This funding would be utilized to construct a bikeway along CSAH 12 (Dayton River Road) from the East Boundary to N Diamond Lake Rd.

The current project does not include any bikeway or multi-modal transportation and County Staff have heard during their study of the corridor that multi-modal transportation is being requested from users and residents alike. Based on this information, the County is requesting a letter of support for a \$5.5M Regional Solicitation Grant Ask from the federal government.

The current cost participation for the project will adjust with any multi-modal transportation being added onto the project and we would look to alleviate as much of the dollars onto other governmental agencies (like Three Rivers Park District) who would greatly benefit from this type of addition to the rehabilitation project on CSAH 12 already planned in 2029.

Cost breakdown as of today is \$0 for the City with no multi-modal trail completed, however if multi-modal trail were to be constructed, the rough estimate would be a \$3.5M ask from the City which is noted on the attachments. If the grant were to be awarded with that multi-modal trail then the ask would be \$2.5M. City Staff inquired on how that math worked on the project and there were notes from the County that it would be separated out differently if it were to be awarded so the math is not correct above. It is anticipated that a contribution of \$1M would be asked from the City, if the math of the trail and contribution lines up correctly, which the City would again look to partner with another government agency to contribute.

CRITICAL ISSUES:

There are no outstanding issues.

RELATIONSHIP TO COUNCIL GOALS:

Maintain and Enhance the Natural and Rural Community Connection

RECOMMENDATION:

If there can be a minimal output of work by the City of Dayton for this grant money and it would remain the same contribution as originally anticipated then it is staff recommendation for approval.

ATTACHMENT(S):

Cost Estimate
Letter from Hennepin County
Letter of Support Example

CSAH 12 – CP 2250200 Anticipated Financial Obligations

Without federal funds

Section 7 Anticipated Financial Obligations					
	%	Federal	Hennepin County	Dayton	Total
CSL DESIGN ¹	12%	\$ -	\$ 500,000	\$ 223,000	\$ 723,000
ROW		\$ -	\$ 615,000	\$ 615,000	\$ 1,230,000
CRN		\$ -	\$ 9,410,000	\$ 1,860,000	\$ 11,270,000
CSL C/A ²	10%	\$ -	\$ 900,000	\$ 186,000	\$ 1,086,000
CGY		\$ -	\$ 2,822,000	\$ 558,000	\$ 3,380,000
Totals:		\$ -	\$ 14,247,000	\$ 3,442,000	\$ 17,689,000
Key					
¹ Actual percentage for Design Engineering to be determined during project development					
² Actual percentage for Construction Administration to be determined during project development					
Fund 10	Financing within the county's Operating Budget				
Fund 53	Financing within the county's Capital Budget				

With federal funds

Section 7 Anticipated Financial Obligations					
	%	Federal	Hennepin County	Dayton	Total
CSL DESIGN ¹	12%	\$ -	\$ 500,000	\$ 223,000	\$ 723,000
ROW		\$ -	\$ 615,000	\$ 615,000	\$ 1,230,000
CRN		\$ 5,500,000	\$ 4,818,000	\$ 952,000	\$ 11,270,000
CSL C/A ²	10%	\$ -	\$ 900,000	\$ 186,000	\$ 1,086,000
CGY		\$ -	\$ 2,822,000	\$ 558,000	\$ 3,380,000
Totals:		\$ 5,500,000	\$ 9,655,000	\$ 2,534,000	\$ 17,689,000
Key					
¹ Actual percentage for Design Engineering to be determined during project development					
² Actual percentage for Construction Administration to be determined during project development					
Fund 10	Financing within the county's Operating Budget				
Fund 53	Financing within the county's Capital Budget				

HENNEPIN COUNTY
MINNESOTA

May 13, 2026

Zach Doud
City Administrator
City of Dayton
12260 S Diamond Lake Road
Dayton, MN 55327

Re: Support for 2026 Regional Solicitation Application:
CSAH 12 (Dayton River Road) Regional Bikeway Project

Dear Mr. Doud:

As part of the Metropolitan Council's 2026 Regional Solicitation, Hennepin County is submitting an application to seek federal funding to construct a bikeway as part of the county's CSAH 12 (Dayton River Road) rehabilitation project from approximately 700' South of Balsam Lane to 500' South of CSAH 144 (N Diamond Lake Road) in the City of Dayton. Federal funding through this solicitation is available for program years 2030 and 2031.

The project will involve the construction of a bicycle facility along CSAH 12 (Dayton River Road) from approximately 700' South of Balsam Lane to 500' South of CSAH 144 (N Diamond Lake Road). The bikeway identified along this corridor is part of Three Rivers Park District's West Mississippi River Regional Trail network and the larger US Bicycle Route 45, which extends parallel to the Mississippi River from Itasca State Park to the state of Louisiana. Note that this bicycle facility will be one element within a larger rehabilitation project of the existing roadway that is anticipated to include, but not limited to, the following elements: new pavement, as well as curb and stormwater structures adjacent to the proposed multi-use trail facility. In addition, Complete & Green Streets strategies (such as boulevards, curb extensions, and raised medians) will be considered to improve accessibility, mobility, and safety along and across the corridor.

Due to this project's location in the City of Dayton, we would appreciate a letter of support from the City of Dayton acknowledging that the city is aware of this project and that the city will likely be required to cost participate in the project as outlined in the Hennepin County Cost Participation and Maintenance Policies. Specific details regarding cost participation and maintenance responsibilities are anticipated to be determined during the design phase as project development is advanced.

If you agree to support this proposed project, please send a PDF letter via email addressed to:

Carla Stueve, P.E.
Director and County Highway Engineer
Hennepin County Transportation Project Delivery
1600 Prairie Drive
Medina, MN 55340

You may email the electronic version of the letter to me at Emily.Buell@hennepin.us. I have attached a letter template that you may use or modify as you see fit.

Hennepin County appreciates the opportunity to partner with the City of Dayton on this important transportation improvement project. Given an application deadline of June 25, 2026 we would appreciate your support letter by June 5, 2026. If you have any questions, please contact me at (612) 543-1963 or at Emily.Buell@hennepin.us.

Sincerely,



Emily Buell
Transportation Project Delivery – Capital Programming

Cc: Carla Stueve, P.E. – Director and County Highway Engineer
Jason Pieper, P.E. - Transportation Project Delivery – Capital Programming Manager

Date

Carla Stueve, P.E.
Director and County Highway Engineer
Hennepin County Transportation Project Delivery
1600 Prairie Drive
Medina, MN 55340

Dear Ms. Stueve:

The City of Dayton hereby expresses its support for Hennepin County's 2026 Regional Solicitation federal funding application for the construction of a bicycle facility along CSAH 12 (Dayton River Road) from approximately 700' South of Balsam Lane to 500' South of CSAH 144 (N Diamond Lake Road) in the City of Dayton.

The bikeway identified along this corridor is part of Three Rivers Park District's West Mississippi River Regional Trail network and the larger US Bicycle Route 45, which extends parallel to the Mississippi River from Itasca State Park to the state of Louisiana. The city understands that this bicycle facility will be one element within a larger rehabilitation project of the existing roadway that is anticipated to include, but not limited to, the following elements: new pavement, as well as curb and stormwater structures adjacent to the proposed multi-use trail facility. In addition, Complete & Green Streets strategies (such as boulevards, curb extensions, and raised medians) will be considered to improve accessibility, mobility, and safety along and across the corridor.

The City of Dayton acknowledges that Hennepin County is pursuing federal funding to construct a bicycle facility along CSAH 12 (Dayton River Road) and that the city will likely be required to cost participate in the project as outlined in the Hennepin County Cost Participation and Maintenance Policies. Specific details regarding participation and maintenance responsibilities are anticipated to be determined during the design process as project development is advanced.

Thank you for making us aware of this application and project, and for the opportunity to provide support. The City of Dayton looks forward to working with you on this project.